

KAMPLAN 2004

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MAPS:

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1	Generalized Land Use Map
2	Parks and Schools
3	Hazard Lands
4	Environmentally Sensitive Areas
5	Major Road Network

<u>Bylaw No.</u>	<u>Date of Adoption</u>
5-1-2146	July 26, 2005
5-1-2260	November 29, 2005
5-1-2264	February 28, 2006
5-1-2273	March 28, 2006
5-1-2281	April 25, 2006
5-1-2319	January 16, 2007
5-1-2321	January 16, 2007
5-1-2301	February 27, 2007
5-1-2346	May 29, 2007
5-1-2316	June 12, 2007
5-1-2356	August 14, 2007
5-1-2386	March 11, 2008
5-1-2398	June 24, 2008
5-1-2399	June 24, 2008
5-1-2416	September 30, 2008
5-1-2426	November 18, 2008
5-1-2441	March 24, 2009
5-1-2501	January 26, 2010
5-1-2518	June 8, 2010
5-1-2529	August 31, 2010
5-1-2535	August 31, 2010
5-1-2517	November 2, 2010
5-1-2491	November 2, 2010
5-1-2563	March 15, 2011
5-1-2566	March 30, 2011
5-1-2587	May 31, 2011
5-1-2589	June 28, 2011
5-1-2460	August 30, 2011
5-1-2572	November 15, 2011
5-1-2608	November 15, 2011
5-1-2614	January 31, 2012
5-1-2618	February 14, 2012
5-1-2630	May 15, 2012
5-1-2635	June 10, 2014
5-1-2639	August 21, 2012
5-1-2652	September 11, 2012
5-1-2640	June 25, 2013
5-1-2661	April 9, 2013
5-1-2672	June 25, 2013
5-1-2680	May 14, 2013
5-1-2713	January 28, 2014
5-1-2725	June 24, 2014
5-1-2731	September 9, 2014
5-1-2729	November 25, 2014
5-1-2794	April 5, 2016

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Advisory Planning Committee

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- ▶ Councillor Joe Leong
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INTRODUCTION

The *Local Government Act*, Part 26, provides the legislative framework under which KAMPLAN 2004: The Official Community Plan is to be prepared. It outlines the general purpose of a community plan and lists both the required and optional contents of the plan.

Section 875 (1) of the *Local Government Act* states that, "An Official Community Plan is statement of objectives and policies to guide decisions on planning and land use management within the area covered by the plan, respecting the purposes of local government."

Section 877 of the Act outlines the required content as follows:

"A community plan must include statements and map designations for the area covered by the plan respecting the following:

1. The approximate location, amount, type, and density of residential development required to meet anticipated housing needs for a period of at least five years;
2. The approximate location, amount, and type of present and proposed commercial, industrial, institutional, agricultural, recreational, and public utility land uses;
3. The approximate location and area of sand and gravel deposits that are suitable for sand and gravel extraction;
4. Restrictions on the use of land that is subject to hazardous conditions or that is environmentally sensitive to development;
5. The approximate location and phasing of any major road, sewer, and water systems;
6. The approximate location and type of present and proposed public facilities including schools, parks, and waste treatment and disposal sites; and
7. Housing policies of the local government respecting affordable housing, rental housing, and special needs housing."

Section 878 outlines optional policy statements that a local government may include in the community plan as follows:

1. Policies of the local government relating to social needs, social well-being, and social development;
2. A regional context statement consistent with the rest of the community plan as it relates to regional, social, economic, and environmental objectives;
3. Policies of the local government respecting the maintenance and enhancement of farming in an area designated for agricultural use in the community plan; and
4. Policies of the local government relating to the preservation, protection, restoration, and enhancement of the natural environment, its ecosystems, and biological diversity."

KAMPLAN 2004 PROCESS

KAMPLAN 2004: The Official Community Plan was prepared through an extensive process of community consultation, including direction provided by an Advisory Planning Committee, a Technical Committee, and frequent meetings with major stakeholders in the community, including neighbourhood associations, the Business Improvement Associations, the Thompson Rivers University, and others. The KAMPLAN Advisory Committee was composed of residents from throughout the City, as well as representatives from the development and real estate industries. The Technical Committee was composed of City staff who provided continuing and timely input on City initiatives such as the Tournament Capital of Canada Facility Expansion Program, the water treatment plant, the Liquid Waste Management Plan, and Safer City. Stakeholder input, such as that provided by the Business Improvement Associations or the non-profit sector, provided specific feedback on issues relevant to each stakeholder group. The chronology of the KAMPLAN 2004 process is outlined on the following table.

Based on the input received through the KamTalk community conference and the public open houses conducted in May and June of 2002, the KAMPLAN Advisory Committee developed a community vision that would provide the guiding principles to KAMPLAN 2004.

November 2001	<ul style="list-style-type: none">• KamTalk Community Conference.
February 2002	<ul style="list-style-type: none">• Formation of KAMPLAN Advisory Committee.
March 2002	<ul style="list-style-type: none">• First meeting of KAMPLAN Advisory Committee.• KamTalk proceedings published.
April 2002	<ul style="list-style-type: none">• Issue identification with KAMPLAN Advisory Committee.• Technical Committee review of issues.
May/June 2002	<ul style="list-style-type: none">• Public open houses at eight locations throughout the City.
Summer 2002	<ul style="list-style-type: none">• Review of secondary plans.• Technical analysis of demographic and development trends.
October/November 2002	<ul style="list-style-type: none">• Development of Kamloops Vision Statement with KAMPLAN Advisory Committee.• Presentation on KAMPLAN to non-profit sector.
December 2002	<ul style="list-style-type: none">• Preparation of policy framework.

Introduction

January/February 2003	<ul style="list-style-type: none">• Review of policy framework by KAMPLAN Advisory Committee and Technical Committee.
March-June 2003	<ul style="list-style-type: none">• Draft KAMPLAN 2004 prepared.
Summer 2003	<ul style="list-style-type: none">• Review of draft KAMPLAN 2004 by KAMPLAN Advisory Committee and Technical Committee.
October 2003	<ul style="list-style-type: none">• Second draft of KAMPLAN 2004 prepared and reviewed by committees.
December 2003	<ul style="list-style-type: none">• Public open houses to present KAMPLAN 2004.
January/April 2004	<ul style="list-style-type: none">• Workshop on KAMPLAN with City Council to discuss major changes.• April 27th first reading by City Council.
May/December 2004	<ul style="list-style-type: none">• Forward document to TNRD, Agricultural Land Commission, and other agencies.• Revisions.
May/June 2005	<ul style="list-style-type: none">• Second Reading.• Public Hearing.• Adoption.

(5-1-2535)

SUSTAINABLE KAMLOOPS PLAN: FOUNDATIONS FOR SUSTAINABILITY

The purpose of "Sustainable Kamloops Plan: Foundations for Sustainability" is to adopt strategies and targets to reduce greenhouse gas emissions in accordance with the *Local Government (Green Communities) Statutes Act*. The intent of the legislation is to facilitate more sustainable initiatives within BC municipalities by considering climate issues and energy conservation approaches. Through this legislation, the *Local Government Act* requires each municipality to establish targets, policies, and actions for the reduction of greenhouse gas (GHG) emissions and include these in their Official Community Plans.

For instance, reduction targets for the City of Kamloops are included in the "Sustainable Kamloops Plan: Foundations for Sustainability" and include:

- Reducing community-wide GHG emissions by 40% below 2007 levels by 2020;
- Reducing residential-based GHG emissions to 0.9 tonnes/capita by 2020;
- Reducing transportation-related GHG emissions to 2.4 tonnes/capita by 2020;
- Increasing alternative transportation to 30% of all trips;
- Reducing GHG emissions from municipal corporate operations to 4,600 tonnes by 2020; and
- Achieving municipal corporate carbon neutrality by 2012.

Additionally, the Sustainable Kamloops Plan is a vision to address sustainability topics at the local level. The key sustainability components identified in the "Sustainable Kamloops Plan: Foundations for Sustainability" are as follows:

- | | |
|-------------------------------|-------------------------------|
| • Transportation | • Recreation |
| • Climate Change | • Natural Environment |
| • Energy | • Food Security |
| • Air | • Arts, Culture, and Heritage |
| • Water Use Efficiency | • Community Safety |
| • Drinkwater Water Quality | • Economic Development |
| • Stormwater | • Health and Wellness |
| • Wastewater | • Education |
| • Solid Waste | |



Refer to the Sustainability and Environmental Services Division for more information, or to the City of Kamloops website to view "Sustainable Kamloops Plan: Foundations for Sustainability".

SECTION I: REGIONAL CONTEXT STATEMENT

The Growth Strategies Act and the Local Government Act provide a mechanism to link local community plans with regional plans.

Section 866 of the Local Government Act requires that where a Regional District has prepared a Regional Growth Strategy, the Official Community Plan (OCP) must include a Regional Context Statement that is accepted by the board of the regional district. The regional context statement must identify the relationship between matters contained in the regional growth strategy and the OCP, and how the OCP is consistent with the regional growth strategy.

The City of Kamloops and the Thompson-Nicola Regional District (TNRD) have had a long history of co-operating on matters relating to growth management.

In the 1980s, the City and the TNRD established a Fringe Area Policy to deal with development applications on the fringes of the City. The Fringe Area Policy allows City officials to comment on development applications within five kilometres of municipal boundaries. The policy provides the City with a mechanism to comment on developments that have the potential to generate demands for extensions to municipal infrastructure at some point in the future.

Beginning in 1995, the City participated with the TNRD and other government agencies in the development of a Regional Growth Strategy (RGS), which was adopted by the Board of the Thompson-Nicola Regional District as Bylaw 1767 in May of 2000. The RGS is not a land use plan, but rather a strategy for achieving a sustainable future for the region. The RGS provides a framework for:

- taking action on development and settlement issues;
- coordinating efficient land use, public facilities, services, finances, and resources over the next 20 years;
- encouraging a variety of economic opportunities, land use choices, and quality of life attributes in an affordable and efficient manner; and
- ensuring our environment and natural amenities are protected and conserved.

The RGS provides specific policies to manage human settlement, transportation and energy, economic development, environmental protection and open space, and cultural heritage issues.

Most recently, the City and the Village of Chase have participated with the TNRD on the preparation of the South Thompson Settlement Strategy (2002). The strategy seeks to manage growth in the South Thompson corridor by directing growth to existing urban areas or rural residential clusters, preserving the agricultural integrity of the valley, and minimizing the impacts of development on the environment, particularly water quality.

The City of Kamloops is the largest urban area within the regional district. The City's population of approximately 80,000 (estimate 2001) represents 65% of the region's population. This percentage has increased slowly since 1986, when the City's portion of the region's population was only 63%. The decrease in primary and manufacturing activity

which tends to affect the rural areas of the TNRD more than the urban centres, coupled with an increase in service sector jobs which tend to be concentrated in Kamloops, will likely increase this percentage in the future. The RGS notes that mining closures, forest industry consolidations, and variable agricultural commodity prices have all had their impact on the TNRD. The wildfires of 2003, the loss of the Tolko mill in Barriere, and the spread of the Mountain Pine beetle will also have an impact on growth in the TNRD. These and the projected closure of Highland Valley Copper in 2009 will contribute to the concentration of population in Kamloops.

The RGS anticipates an annual growth rate of 1.5%, with the regional population growing from 120,000 in 2000 to 160,000 by 2015. While the Official Community Plan uses the slightly more conservative growth projection of 1.25%, the City is in agreement with the RGS that the majority of growth will gravitate to the larger municipalities.

1.0 REGIONAL GROWTH STRATEGY

GOAL

- To create a balance among human settlement, economic development, and environmental conservation such that development does not significantly limit the options of future generations.

Regional Growth Policies

- 1.1 The policies of the Official Community Plan are consistent with the policies contained in the Regional Growth Strategy.
- 1.2 Kamloops is the regional centre for the Thompson-Nicola Regional District. While recognizing that economic development in the region will benefit Kamloops, the City will not support development which may require future service extensions.

Human Settlement

The RGS encourages urban communities to:

- develop in a compact, cost-effective, sustainable manner;
- maintain and enhance the community's quality of life;
- establish a commercial base to provide employment and serve the needs of residents and visitors;
- encourage a high standard of development;
- ensure a range of housing types capable of meeting the needs of residents;
- protect and enhance the quality of the natural environment;
- encourage mixed use, higher densities, and pedestrian friendly developments; and
- minimize impacts on adjacent resource uses.

- 1.3 The Official Community Plan supports the Human Settlement policies contained in the RGS.

The RGS also recommends a review of the Fringe Area Policy Paper to ensure that growth management policies and procedures reflect the RGS. Through its participation in the South Thompson Settlement Strategy (2002), the City has recently reviewed its fringe policy in the South Thompson Valley, which has experienced continuing development pressure over the years.

There has been little or no development pressure in the North Thompson, Cherry Creek, or Kamloops South fringe areas. Reviewing policies in these areas is of a lower priority and should be undertaken in response to development pressure.

- 1.4 The City of Kamloops will work with the TNRD to develop implementation strategies in relation to the newly adopted *South Thompson Settlement Strategy (2002)*.
- 1.5 The current fringe area policy agreement between the Thompson-Nicola Regional District and the City shall remain in place for the term of this plan for those parts of the fringe area outside the South Thompson Valley.
- 1.6 The City's support for future major development proposals at its periphery, in particular those providing permanent residential development, will be contingent upon developing a cost sharing/equity strategy for City services to ensure that Kamloops taxpayers will not have to bear additional burdens. While recognizing that economic development in the region will benefit Kamloops, the City will not support development which may require future service extensions.
- 1.7 The City will support and will co-operate with the Kamloops Indian Band (KIB) in its endeavour to develop its lands as a method of ensuring economic self-sufficiency and of accommodating future growth. The City recognizes that the development of the KIB lands in a cost-efficient and orderly manner supports and complements the City's and the region's overall growth management strategy.

Transportation and Energy

The RGS recognizes major transportation corridors for existing functions and potential expansion. The Official Community Plan recognizes the Trans Canada, Coquihalla, and Yellowhead Highways, as well as both national railways. It also contains policies recognizing the preferred alignment for the Trans Canada Highway from the east end of Valleyview to just east of the Peterson Creek Bridge along the Juniper bench. It is expected that this highway bypass will not be built for the next 10 to 15 years due to budget constraints.

As noted in the RGS, the Official Community Plan recognizes the importance of air transportation service and encourages the potential for expansion of facilities and service at Kamloops Airport.

- 1.8 The City supports increased development opportunities at the airport in accordance with the recently completed the *Kamloops Airport Area Land Use and Development Plan (2000)*.

The RGS encourages compact community design in order to save transportation and energy costs and to provide more opportunity for walking, cycling, and other forms of transportation. One of the fundamental principles of the Official Community Plan is the integration of land use and transportation planning to reduce vehicle trips and total miles travelled through the TravelSmart planning process. Recent work by the City on the *Bicycle Master Plan (2000)*, the *Pedestrian Master Plan (2002)*, the Rivers Trail, and the *Transit Master Plan (2000)* all support residents using alternate modes of transportation.

- 1.9 The City encourages intensification and infill over peripheral development to ensure that municipal services are provided in an efficient and cost-effective manner.
- 1.10 The City supports the use of alternate modes of transportation including transit, bicycles, and walking as encouraged through the TravelSmart planning process.

Economic Development

The RGS seeks to broaden the economic base through diversification and expansion. Kamloops has one of the most diversified economies in the TNRD and is not as dependent on resource based industries as other centres. Growth of the Thompson Rivers University and in the service sector is expected to continue through the time frame of this plan. In addition, Kamloops is expected to continue as the regional centre for health care and government services.

- 1.11 The City supports diversification and expansion of the local economy by supporting appropriate development proposals through a business friendly development approvals process, by continually reviewing regulations and procedures to ensure red tape is minimized, by using City-owned land to create development opportunities, and by participating in the region's economic development agencies and initiatives.
- 1.12 The City will support regional tourism development that will enhance Kamloops and the region as a tourist destination area, including the following initiatives:
 - Sun Peaks Resort development,
 - Rocky Mountaineer Railtours,
 - Six Mile Ranch proposal, and
 - Kamloops Indian Band proposals.

Environmental Protection

The RGS seeks to protect and enhance the environment through the use of stewardship principles. The Official Community Plan includes policies that support the City's Environmental Management System, which include annual environmental performance audit reports, an environmental indicators program, and is guided by an Environmental Performance Advisory Committee. The City is a participant in the federal government's Partners for Climate Protection program and has completed Milestone 2 of the program. The City participated with the TNRD on the Regional Solid Waste Management Plan, which includes recycling initiatives to divert waste from the landfill. The City is constructing a water treatment plant and is nearing completion of a Liquid Waste Management Plan, which will ensure that downstream water quality is protected. The Official Community Plan also contains policies and mapping of environmentally sensitive areas within City boundaries. The Official Community Plan recognizes the constraints placed on development by environmental factors and, in particular, contains extensive policies related to the silt bluff hazard lands.

- 1.13 The City supports the region's vision of a sustainable future which balances human settlement and economic development with the need for environmental conservation.
- 1.14 The City will oppose development which contributes to a potential degradation of the City's supply of potable water.
- 1.15 The City will work with the Kamloops Indian Band and the Thompson-Nicola Regional District to minimize development and growth in grassland areas. To be aware that each request reduces the overall grassland ecosystem to reduce fragmented development and to increase preservation protection, restoration, and enhancement of the natural areas so as to retain ecosystems and biological diversity.

Open Space and Cultural Heritage

The RGS policies in relation to open space focus on the allocation of Crown land and the need for open space amenities on a region wide basis. The Official Community Plan supports these goals for open space and cultural heritage, but focuses on the parks and recreational needs for Kamloops residents and the use of open space for the natural drainage of storm water. The Parks and Recreation Master Plan and the Cultural Strategic Plan contain specific policies about the City's parks system and our cultural and heritage resources.

- 1.16 The City supports the provision of parks and recreational opportunities and the preservation, celebration, and interpretation of our cultural heritage.

Co-operation and Process

- 1.17 The City of Kamloops will continue to co-operate with the TNRD and other government agencies and First Nations to address growth management issues as they arise and to plan for the future in a collaborative manner.

Regional Context Statement

- 1.18 The current fringe area policy agreement between the TNRD and the City of Kamloops shall remain in place for the term of this plan for those parts of the fringe area outside the South Thompson Valley.

SECTION II: GROWTH MANAGEMENT

The City's growth management approach emphasizes infill and the intensification of land use in order to make efficient use of existing infrastructure and reduce environmental and financial costs of growth. Senior governments are encouraging municipalities to grow and develop in a more sustainable manner, conserving land, energy, and other resources, and reducing the impact on the environment. In principle, this approach is widely supported by the public, but in practice, is often difficult to achieve due to neighbourhood opposition to increased densities in existing neighbourhoods and reluctance to use transit. For the term of this plan, the City will continue to pursue its long-term goal of a more compact, efficient, and sustainable community.

Since 1997, the City has integrated land use and transportation planning [*TravelSmart (1999)*] as part of the OCP process to assist in developing a growth strategy that will achieve community goals for sustainable growth, quality of life, mobility, and cost-effectiveness. *TravelSmart (1999)* evaluated a wide variety of land use scenarios to see if altering current trends will produce measurable improvements. Each scenario concentrated development in one or more sectors of the City to determine where growth could be accommodated to make the best use of the existing transportation network and reduce the costs of future improvements. These scenarios were repeated as part of the KAMPLAN 2003 process and confirmed the preferred distribution of growth outlined in previous Official Community Plans.

Starting with a base population of 80,000 (2001), land use scenarios were analyzed at the 100,000 and 120,000 population horizons. Using a projected growth rate of 1.25%, it is anticipated that the population may reach 100,000 by the year 2021 and 120,000 by 2,036. Currently there is sufficient land designated for development to accommodate a population of 100,000 (vacant lots and zoned land). In addition, there are a number of proposed development parcels which could accommodate a population well in excess of 120,000.

The results show that equivalent long-term growth in all sectors of the City will cause significant long-term costs for road network improvements or, alternatively, high levels of congestion in key locations. *TravelSmart (1999)* results indicate the need for growth limitations in Batchelor Heights and Juniper (Northwest and Southeast Sectors) and managed growth in the Central Core (Downtown/Tranquille) and Southwest.

Growth To 100,000 (2021)

Growth to a population level of 100,000 can be supported relatively easily since sufficient land is already zoned and the major infrastructure is available. The following table shows the distribution of this growth if no additional lands were zoned and if all existing development parcels achieved full build-out. This includes a modest amount of redevelopment within the Central Core (Downtown/Tranquille).

GROWTH DISTRIBUTION - 100,000 (2021)			
Area	Existing in 2003	Growth	% of Growth
Northwest	27,950	4,150	21%
Northeast	4,100	3,300	17%
Southwest	20,550	7,150	36%
Southeast	12,100	1,500	7%
Central Core	15,300	3,900	19%
Total	80,000	20,000	100%

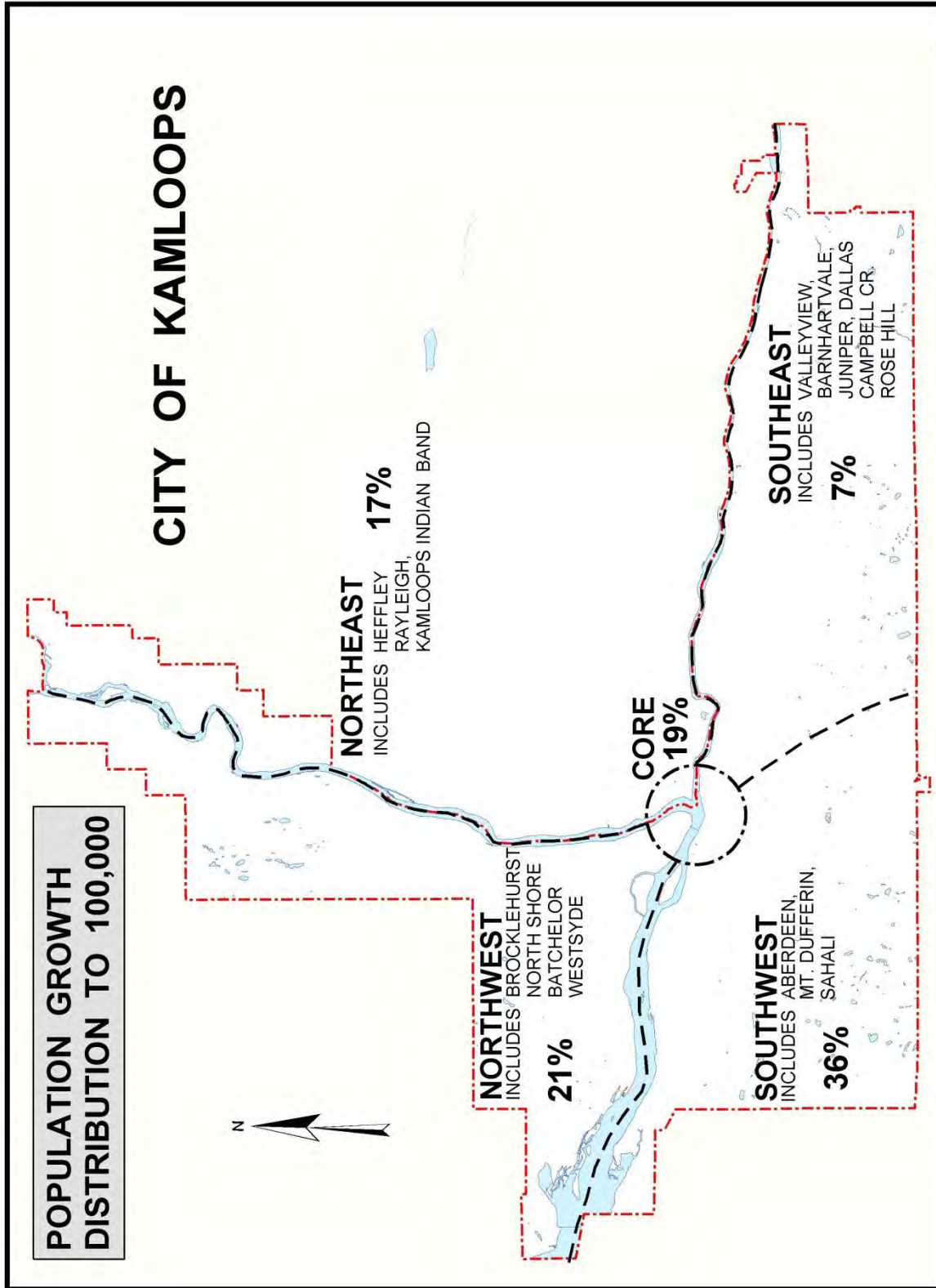
Basic road network improvements required to accommodate this population include:

- Columbia Street - road widening between 3rd and 6th Avenues (\$2 million)
- Hillside Drive Extension - west of Home Depot to Summit Drive (\$8.5 million)

Other infrastructure improvements required include:

- Water - North Shore upgrade (\$6 - \$12 million)
 - intake/supply improvements (\$5 - \$10 million)
- Sewer - system upgrading (\$3 - \$5 million)
 - effluent treatment upgrading - partial biological nutrient removal (BNR) plant (\$25 - \$30 million)

Within individual development areas, there will be additional site-specific requirements such as drainage, local roads, water/sewer mains, and reservoirs which are provided by the developer at the time of development, as needed.



Growth on the Kamloops Indian Band lands (Northeast Sector) may also affect the timing of infrastructure improvements. The Red Bridge is nearing the end of its useful life (ten to fifteen years) and will need to be replaced. The preferred design crosses from the Kamloops Indian Band lands over the South Thompson River to connect to Lorne Street (estimated cost \$25 million).

Growth to 120,000 (2036)

Growth from 100,000 to 120,000, however, will require significant investment in new infrastructure. While areas have been identified in all sectors to accommodate this level of population (Special Development Areas), the distribution of this growth will have an impact on the costs and the consequences to the community. A comparative analysis of development costs by growth sector indicated that road costs were roughly three times higher than all other servicing costs combined and therefore should be considered the most critical factor.

The need to construct the 6th Avenue extension from downtown to the Sahali neighbourhood, estimated at \$20 million, will be required prior to the 120,000 population horizon to alleviate congestion on the Columbia Street corridor, regardless of the growth scenario evaluated.

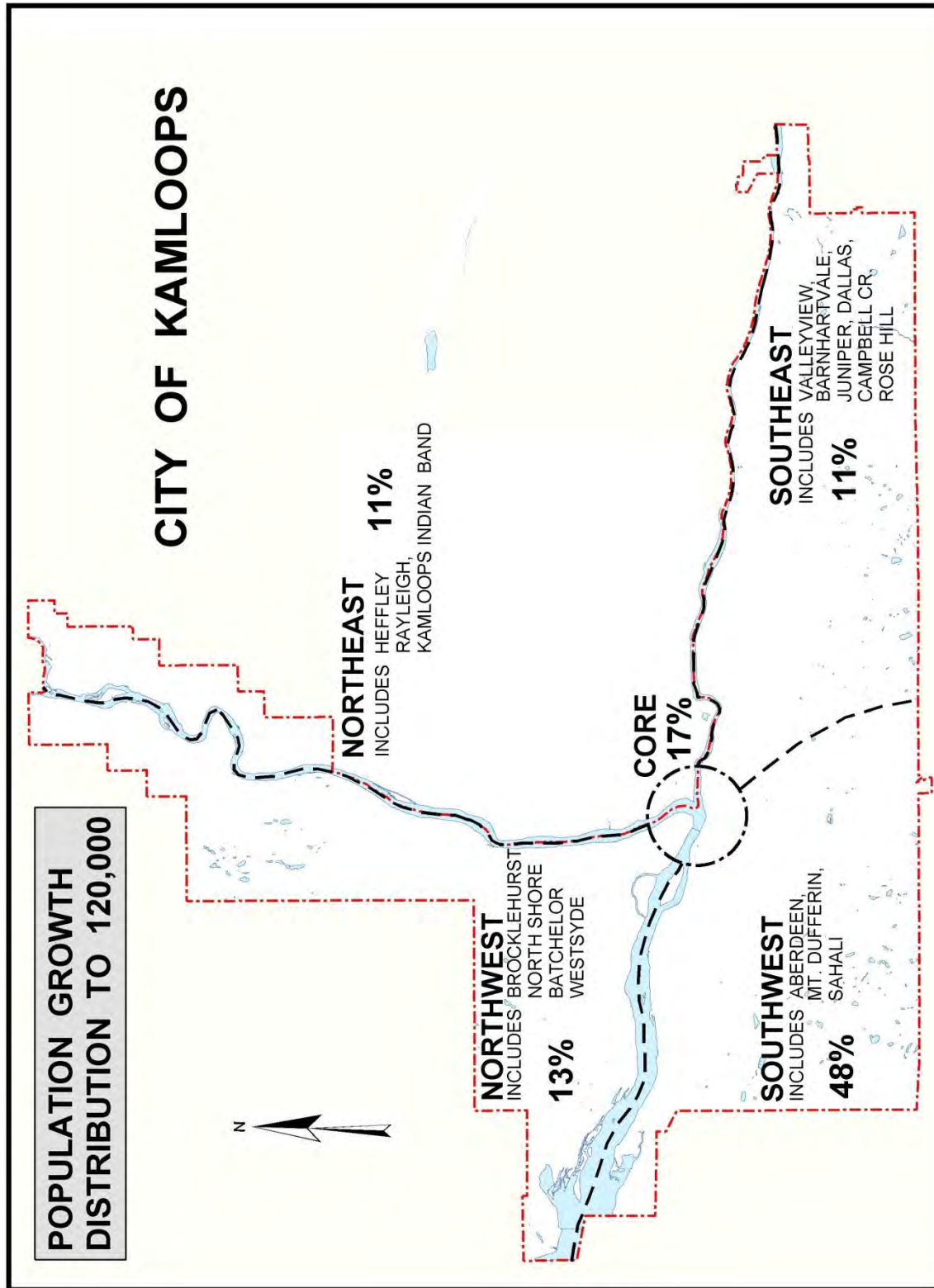
Growth in the Northwest Sector will lead to increased congestion on the Overlander Bridge and will advance the need for construction of the Singh Street bridge and approaches, estimated to cost between \$50 million and \$70 million. Directing growth as shown in the growth distribution will defer the need for this bridge to beyond the 120,000 population horizon. The City will continue to protect the bridge corridor and approaches to keep this option open.

The recommended growth distribution showed that even with no improvements to the existing system, changing the pattern of growth could reduce the expected levels of congestion and improve network performance. For example, as the population grows from 80,000 to 120,000 (50% increase), if the growth follows historical patterns, there would be a doubling of the *vehicle hours*. Directing growth as shown on the following table would result in an increase in *vehicle hours* of only 50%, or about equal to the growth in population. This would mean that the increase in vehicle use during the afternoon peak hour could be reduced by 50% through managing growth alone and with no road network improvements. Since *vehicle hours* may be considered a direct indicator of congestion and mobility, managing growth results in less increase in congestion than historical or unmanaged growth.

GROWTH DISTRIBUTION - 120,000 (2036)			
Area	Existing in 2003	Growth	% of Growth
Northwest	27,950	5,000	13%
Northeast	4,100	4,400	11%
Southwest	20,550	19,200	48%
Southeast	12,100	4,600	11%
Central Core	15,300	6,800	17%
Total	80,000	40,000	100%

Other network performance indicators give similar, if less dramatic, results. Average network speed is 7% higher, while the total number of kilometres travelled during the peak hour is reduced by 17%. Managing growth also offers shorter travel times between key locations with and without network improvements. *TravelSmart (1999)* maximizes efficiency in the existing road network and minimizes the costs of transportation improvements needed to maintain reasonably acceptable levels of mobility.

TravelSmart (1999) also supports the OCP policies for encouraging mixed use development to balance the peak load on streets and encourage alternative modes of travel.



Recommended Growth Management Approach

The City will adhere to the following Guiding Principles:

- Infill, intensification, mixed use, and redevelopment will be encouraged, particularly within the town centres and neighbourhood centres.
- Existing zoning commitments for development will be honoured, and services will be upgraded on a planned and phased basis to meet demands.
- The *TravelSmart (1999)* recommended growth distribution will be used as the general guideline for considering levels of development within the Special Development Areas.
- *TravelSmart (1999)* results will be monitored to determine the success of land use adjustments, travel demand management techniques, and network improvements on travel behaviour, with a major review as the population nears the 100,000 level.
- Where development proposals are proposed which do not reflect the City's planned phasing, all required infrastructure improvements, including major off-site upgrades, will be the responsibility of the developer.

1.0 COMMUNITY GROWTH

GOAL

- To develop in a manner which will lead to a more compact, sustainable, and efficient land use form while maintaining and enhancing the community's liveability.

Community Growth Policies

1.1 The City's land use pattern, accompanying service structure, and support for new for new development initiatives will be based on several general growth management principles. These include the following:

1.1.1 The City will support development opportunities where:

- infill or redevelopment of existing serviced land is proposed,
- public services and utilities can be provided in an efficient and cost-effective manner, and
- development is to occur in designated Special Development areas.

1.1.2 The City will encourage higher density land use activities to be clustered in nodes where accessibility, services, and amenities are provided and along major transit routes. Land use density will be highest in and adjacent to the City Centre and in the Tranquille Market area.

- 1.1.3 The City will promote compact community techniques such as urban villages, pedestrian pockets, mixed use development, and well defined neighbourhood centres in specific locations, including (by priority):
- in or adjacent to the City Centre,
 - in or adjacent to the Tranquille Market Street,
 - as part of the "Main Street" area on McGill Road,
 - in conjunction with existing neighbourhood centres, including Westsyde, Brocklehurst, and Valleyview, and
 - in new or developing neighbourhoods.
- 1.2 The City will discourage the following development:
- intensification or expansion of rural or peripheral areas,
 - peripheral expansion requiring upgrading of existing service infrastructure, and
 - urban expansion into the ALR.
- 1.2.1 The City will prohibit or restrict development in designated hazard land areas, depending on the degree of risk associated with the hazard and the ability to mitigate any potential impacts.
- 1.3 The City will use the provision of municipal services and utilities as a means of supporting the land use policies and community goals outlined in this plan.
- 1.4 The City will require the preparation of comprehensive development plans for all new development areas. The comprehensive development plans should address the following issues:
- municipal infrastructure requirements,
 - need for schools, parks, and community facilities,
 - public input into the planning process,
 - commercial and institutional needs,
 - residential mix and density, and
 - connectivity to adjacent neighbourhoods.

2.0 NEIGHBOURHOOD GROWTH

Over the years, the City has undertaken a number of neighbourhood and sector plans to provide more detailed analysis and policy guidance within certain areas of the City.

(5-1-2398)

In the core area, both the *City Centre Plan (2005)* and the *North Shore Neighbourhood Plan (2008)* strongly encourage mixed use development (commercial and residential) which provides increased employment opportunities and an increased market for goods and services in the downtown and North Shore areas. Mixed use also reduces dependence on the automobile by balancing the peak load on streets and encouraging alternate modes of travel. The recommended growth distribution will see 10% to 17% of the City's growth in the core area. Both the *City Centre Plan (2005)* and the *North Shore Revitalization Strategy and Concept Plan (1995)* need to be reviewed and updated within the term of this plan.

In the Northwest Sector, three neighbourhood plans will direct growth: the *Westsyde Neighbourhood Plan (1994)*, the *Batchelor Hills Land Use Plan (1997)*, and the *Airport Area Land Use and Development Plan (2000)*.

The *Westsyde Neighbourhood Plan (1994)* focused on infill and intensification opportunities. At the time of the plan, only limited multi-family sites and only a modest increase in the base zoning of the neighbourhood (from a minimum lot size of 10,000 sq. ft. to a minimum lot size of 7,000 sq. ft.) were identified. A recent rezoning application indicates that residents may now be willing to consider further intensification.

The *Batchelor Hills Land Use Plan (1997)* will guide development along the bench running north from Batchelor Hills to McQueen Road and Alpine Terrace. The plan identifies up to 950 residential units able to support a population of about 2,500 people. These lands have been rezoned to allow development to proceed. In the very long term, the Singh Street Bridge and other network improvements may allow much greater levels of development. In the short term, the single access and intersection with Ord and Westsyde Roads may prove to be critical limiting factors.

The *Airport Land Use Plan (2000)* proposes that the lands closest to the existing residential neighbourhood be used for housing, lands along Tranquille Road be used for service commercial uses, and lands on the Airport itself be used for light industrial. A population of 500 - 600 would be suitable in about 280 residential units. These lands were rezoned in 2001 to encourage development of the Airport. Major development of the airport lands may require further upgrading of Ord and Tranquille Roads. The Airport should continue to be supported as an employment base as well as a critical municipal service.

Additionally, in 2004, a unique neighbourhood plan, the *McDonald Park Neighbourhood Plan*, was completed as a result of a joint effort between the University of British Columbia's School of Community and Regional Planning and the City of Kamloops. The Plan does not specify direction for growth, but rather identifies ways in which the McDonald Park neighbourhood can be maintained and enhanced as a livable and sustainable community by focusing on identity, neighbourhood safety, design, beautification, and transportation connectivity.

In the Southeast Sector, the *Southeast Sector Plan (1992)* was recently reviewed in the context of the *South Thompson Settlement Strategy (2002)*, which was a joint planning strategy conducted by the City of Kamloops, the Village of Chase, and the Thompson-Nicola Regional District. The *South Thompson Settlement Strategy* confirmed that the development policies contained in the *Southeast Sector Plan* are still valid and that further rural residential development is not supported except in those parts of the Barnhartvale neighbourhood with sufficient servicing capacity.

A neighbourhood plan for the Juniper and Rose Hill neighbourhoods was initiated in 1997, but put on hold to allow the Ministry of Transportation to conduct a public process examining options for the future of the East Trans Canada Highway corridor. With the 2001 announcement that a bypass along the Valleyview bench is the preferred alignment, it is recommended that this neighbourhood plan be reactivated.

In the Southwest Sector, development in the Aberdeen neighbourhood has been guided by the *Aberdeen Hills General Development Plan (1982)* for the past two decades. Given other developments in this sector, it is recommended that this plan be reviewed and updated within the term of this plan.

Among the decisions that may affect the outcome of a revised *Southwest Sector Plan* is the 1994 decision to proceed with the Pineview Valley development. This development was originally proposed to contain up to 1,200 residential units at densities approximately 50% greater than any other neighbourhood in Kamloops. A weaker real estate market in the late 1990s has led to that projection being reduced to 900 residential units supporting a population of approximately 2,200 people.

In 1996, the City adopted the *Mt. Dufferin Land Use Plan (1996)* which led to the creation of Kenna Cartwright Park. The plan also identified up to 600 residential units to round out the Mt. Dufferin neighbourhood. The first phase of development is currently under construction adjacent to Dufferin Elementary School.

The most recent neighbourhood plan in the Southwest Sector is the *McGill Corridor/Southgate Concept Plan (2002)* which seeks to create a mixed use "Main Street" along the south side of McGill Road, a technology park in much of Southgate, and an arterial commercial corridor along Notre Dame Drive.

GOALS

- To maintain and develop integrated, sustainable, and liveable neighbourhoods.
- To develop more compact and cost-effective neighbourhoods.

Neighbourhood Growth Policies

- 2.1 The City will encourage the sensitive integration of higher density and mixed use development in new and existing neighbourhoods. The design of this type of development is critical to ensure sensitive integration, particularly within existing neighbourhoods. Development Permit Area designation may be used. The evaluation of new projects will be determined on the following principles/guidelines:

- 2.1.1 A street environment that increases pedestrian activity, such as placing buildings closer to the street line, providing multiple storefronts, and awnings or canopies over the sidewalk, should be promoted.
- 2.1.2 New development in existing neighbourhoods should respect, or be consistent with, established neighbourhood standards, while recognizing that a mix and diversity of land use types should be encouraged and that neighbourhoods change over time. These standards can be addressed through the following design elements:
 - scale and size,
 - landscaping,
 - siting and setbacks,
 - building height,
 - exterior finish, and
 - rooflines.
- 2.1.3 An appropriate balance of built form and open space (including public roads, parks, and open space) should be provided.
- 2.1.4 A public network of streets, sidewalks, boulevards, walkways, and trails that integrates the proposed development with the existing neighbourhood pattern should be encouraged.
- 2.1.5 Play areas, parks, and recreational facilities that provide focal points for activities should be promoted.
- 2.1.6 The development should not excessively alter or modify the natural landscape or environment.
- 2.2 The City will encourage higher densities and mixed use clusters in neighbourhoods, particularly in areas of greatest accessibility, along transit routes or adjacent to major services and amenities such as schools, shopping, recreation facilities, or places of worship.
- 2.3 The City will encourage the establishment or evolution of liveable neighbourhoods having the following qualities or characteristics:
 - 2.3.1 Neighbourhoods should have identifiable centres or focal points which may be exhibited in forms such as commercial areas, community centres, places of worship, or amenity areas.
 - 2.3.2 Neighbourhoods should provide a balanced mix of activities including housing, shopping, employment opportunities, schools, places of worship, and recreational space.
 - 2.3.3 Neighbourhoods should have parks or school fields, including passive and active space, that provide for a range of recreational opportunities and purposes.

- 2.3.4 Neighbourhoods should have a mix and choice of housing opportunities. The City will encourage a minimum 65%:35% split of single to multiple family as an appropriate mix. New multiple family development should be limited in size to avoid creating large concentrations of a single housing type. The City will encourage the maximum size of low- and medium-density multiple family development to be limited to 50 units per development cell as a guideline for ensuring sensitive integration.
- 2.3.5 The Development Permit areas for multiple family development may be considered to ensure integration.
- 2.3.6 Neighbourhoods should promote community interaction among all residents. The City will discourage the development of large gated communities which promote isolation from the neighbourhoods in which they are situated.
- 2.4 The City will support the continuation of home-based businesses within neighbourhoods and will review the home-based business regulations to consider the following:
- increasing the range of uses or services that may be permitted;
 - limiting the size of the use in the residential dwelling; and
 - limiting the level of traffic that may be generated by the home-based business.
- 2.5 The City may consider utilizing unique or alternative local street and servicing standards in new development areas within the context of an overall neighbourhood or development plan.
- 2.6 The City may consider traffic-calming measures in existing neighbourhoods.
- 2.7 The following neighbourhood plans need to be prepared or reviewed and updated within the term of this plan (by 2008):
- *City Centre Plan (2005),*
 - *North Shore Revitalization Strategy (1994),*
 - *Juniper Rose Hill Neighbourhood Plan, and*
 - *Southwest Sector Plan (1992).*

3.0 SPECIAL DEVELOPMENT AREAS

(5-1-2398)

Special Development Areas are lands proposed to be developed at some future date in order to accommodate population growth between the 100,000 and 120,000 population horizons and to accommodate development that requires specialized design and sensitive integration considerations. Some of the lands are zoned to reflect current or historical uses, others are not zoned specifically for development, but are the subject of active development interest and are logical extensions of existing subdivisions. In most cases, individual land use, servicing, traffic and design studies will be required prior to development approvals.

GOALS

- To reserve sufficient lands to accommodate growth to the year 2036 or a population horizon of 120,000.
- To ensure appropriate design and sensitive integration considerations are made when introducing new development into existing neighbourhoods.

(5-1-2398)

Special Development Area Policies

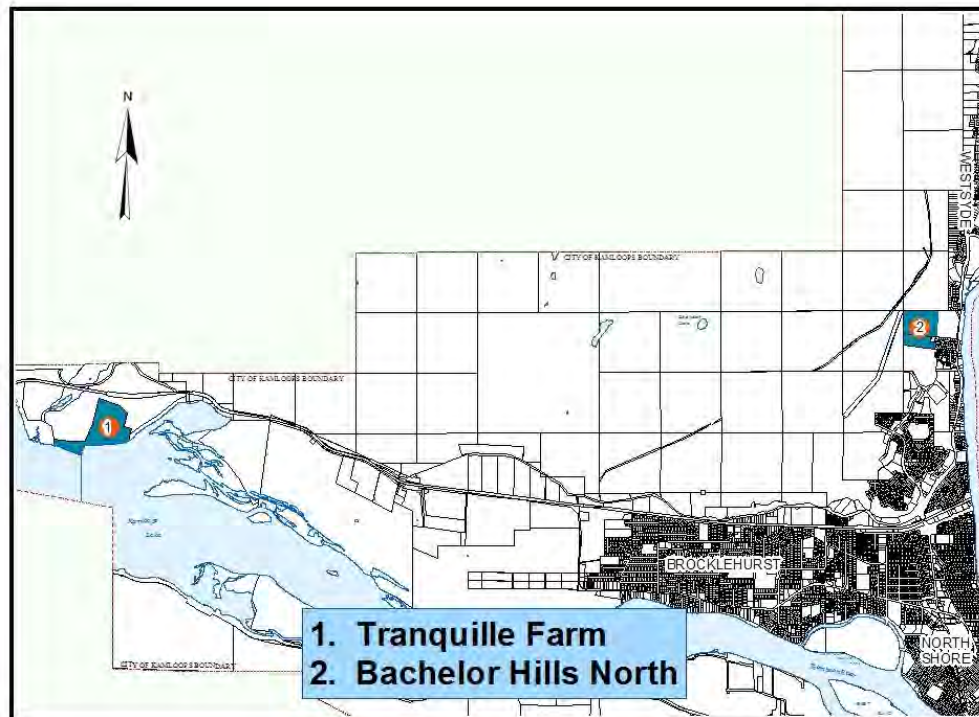
3.1 A number of **Special Development Areas** (SDAs) are shown on Map 1. These identify areas which may be considered for development within the term of this plan and which require special study and evaluation prior to designating specific land uses. These areas will be evaluated individually on the basis of consistency with the policies contained in this plan. Land use compatibility will be important, but the most significant criterion will be cost-effectiveness. Development cost charges may be used to achieve cost-effectiveness. Alternative financing mechanisms may be utilized in accordance with the *Local Government Act*.

Each of the SDAs has development constraints associated with it which may affect the timing and scale of development. It is anticipated that the individual evaluations will outline recommended strategies for addressing any development issues.

3.2 The following lands are identified as SDAs:

Northwest Sector

- Batchelor Hills - The preferred development scenario identified for the area recommends the population of approximately 2,500 on the benchlands. At the 120,000 population mark the Singh Street bridge and other road network improvements may allow much greater levels of development in this area. In the short-term, the single access road and intersections at 8th Street, Ord Road, and Westsyde Road continues to be a limiting factor to additional residential growth in this neighbourhood.

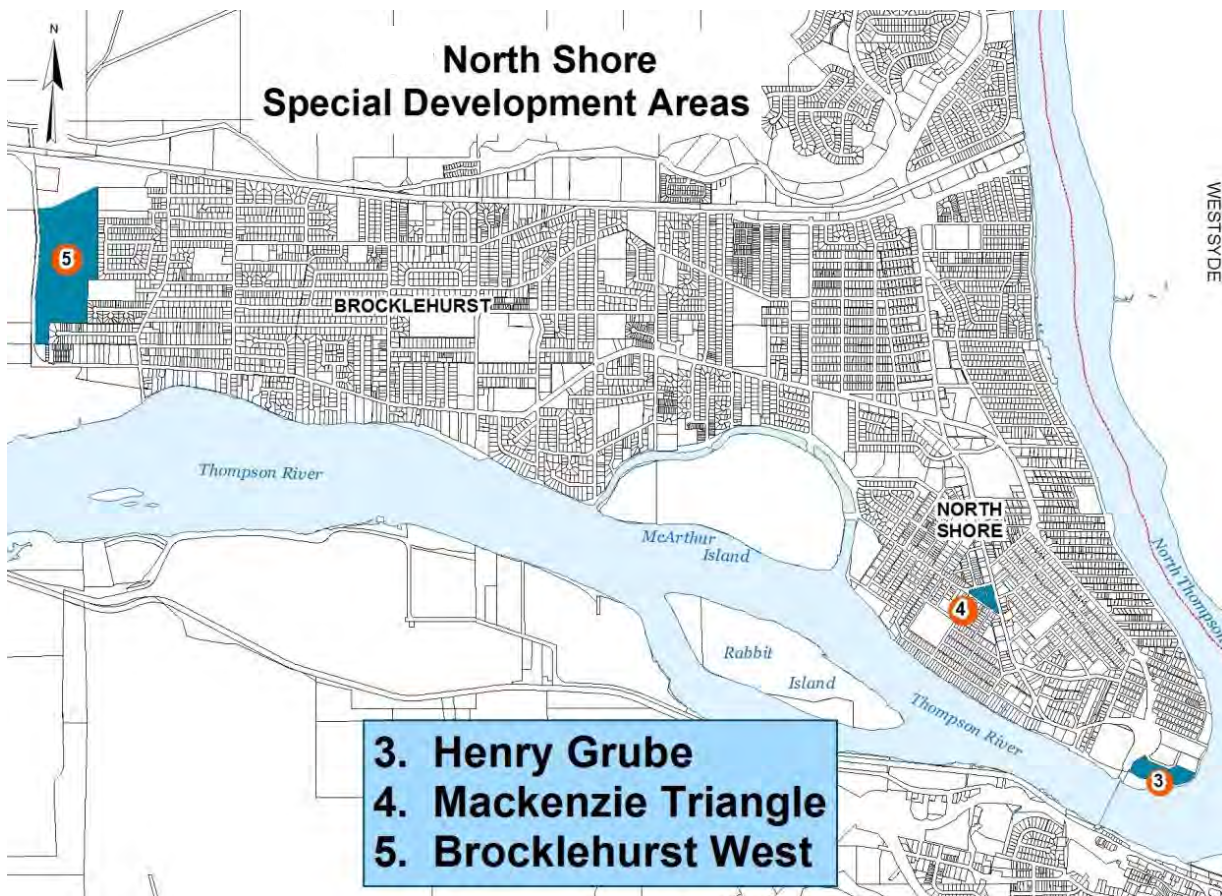


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- Henry Grube - located along one of the City's greatest assets, the Thompson River, the Henry Grube site is a potential location for redevelopment on the North Shore. Prior to any development approvals, a comprehensive development review is required which must include: details on how the water's edge will continue to be used by the public; what types of land uses will be appropriate in the area given traffic and circulation constraints, keeping in mind sensitive integration into the existing residential neighbourhood; details about traffic impacts through a traffic impact assessment; and details about how the greater community can maintain access to the uses of the recreational space and Rivers Trail currently available on this site.
- MacKenzie Triangle - located along and adjacent to the Tranquille Commercial District, the MacKenzie Triangle requires a comprehensive development review prior to development approval that includes: details on how development will support the use of Kamloops' Spirit Square; on the type of intensive residential development that can take place, combined with commercial uses; and the potential for the use of a portion of the laneway to be incorporated into development of the area.

- Brocklehurst West - located within the Airport Entry Corridor and comprising the largest undeveloped greenfield site within the Brocklehurst neighbourhood, this location requires a comprehensive development review prior to development approval addressing the following: incorporating a mix of residential, commercial and light industrial, airport-oriented uses, with any new residential uses being oriented adjacent to the existing residential areas; including a minimum of a 10 m buffer strip or a roadway, between any residential and industrial areas; preparing a comprehensive servicing plan for both storm sewer and water services; detailing a road network pattern providing a minimum of two road connections from the existing residential neighbourhood through to Tranquille Road; and including a pedestrian buffer strip along the railway spur line.

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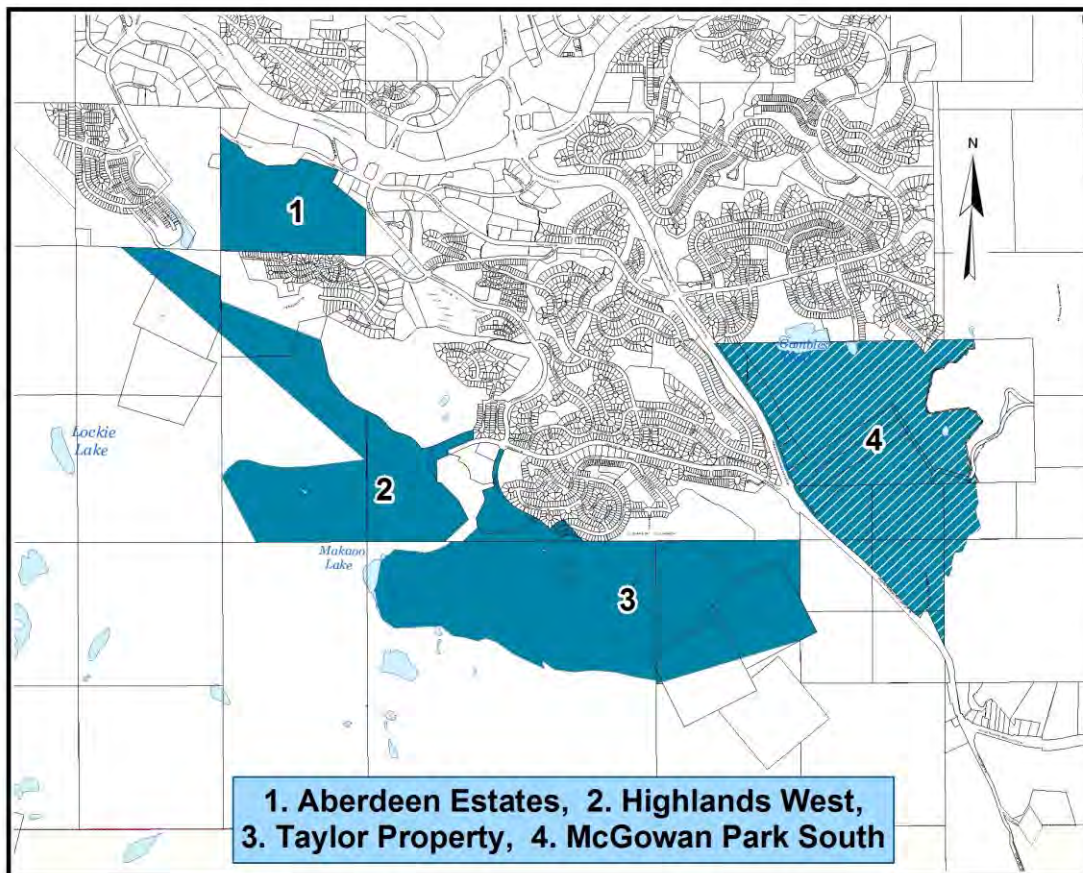


Southwest Sector

- Aberdeen Estates - A development plan was prepared in 1984 and proposes a population of about 1,000 persons. The site is considered to be infill since Pineview Valley and development to the south in Aberdeen have proceeded.

- Highlands West - Part of the *Aberdeen Hills General Development Plan (1982)*. This is a logical extension of the existing Aberdeen Hills neighbourhood and is projected to accommodate about 1,200 residential units (3,000 people). Major issues involve water, roads, geotechnical considerations, and integration within the larger potential upper Aberdeen community.
- Taylor Property - Originally part of the *Aberdeen Hills General Development Plan (1982)*, *TravelSmart (1999)* allocates a population of over 7,000 people to these lands by the 120,000 horizon. Major issues involve roads, including the timing and costs of a new southwest link, and sanitary sewer. The Agricultural Land Commission excluded part of these lands from the ALR in 1983.

(5-1-2426) The Aberdeen Estates, Highlands West, and Taylor Property Special Development Area policy statements as follows are superseded by the Aberdeen Plan (2008).



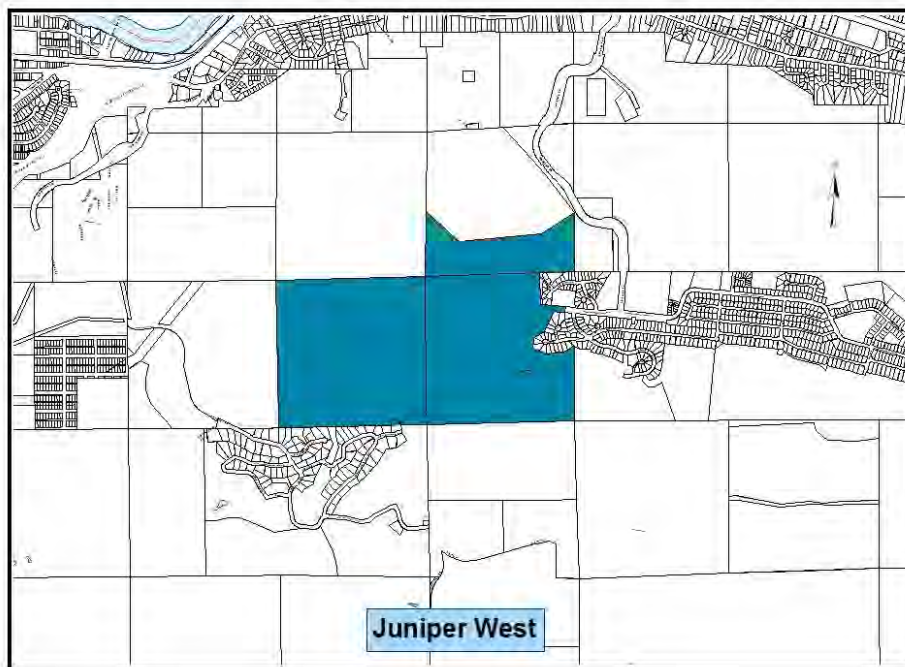
- McGowan Park South - This area, which includes properties owned by R-450 Holdings Ltd., the Frolek Cattle Company, and others, is located to the south of Gamble pond between Peterson Creek and Highway 5A and has been identified as having the potential to accommodate between 800 and 1,000 residential units in a more cost-effective manner than other potential development cells in the Southwest. The property is currently located within the Agricultural

Land Reserve (ALR), and an application to the Agricultural Land Commission will be required. Further study in the context of an overall Southwest Sector Plan will be required to address servicing, geotechnical, phasing, and other issues.

- The potential urban development of McGowan Park South is uncertain because the land is currently within the ALR and the Commission has not indicated that it supports the use of this area for residential development. Discussion with the Agricultural Land Commission will be necessary, as will further study in the context of a Southwest Sector Plan to address geotechnical issues, servicing, and phasing.

Southeast Sector

- Juniper West - Part of a development area which has a potential for adding several thousand more people to the Juniper benchlands. *TravelSmart (1999)* recommends limiting the development to about 150 more units (400 people) until such time as major upgrading occurs in the road connections to these benchlands. Issues involve not only the capacity of Highland Drive, but also the impact of adding major traffic volumes to the Valleyview Drive/Trans Canada Highway east-west corridor. Construction of the Valleyview bypass is expected to reduce these constraints.



These are the only additional lands contemplated for development to accommodate growth prior to the 120,000 population horizon.

3.3 Development Cost Charges for development within the City:

- All urban areas of the City shall be subject to DCCs.

- Special development areas shall be subject to DCCs. On-site mitigation measures necessary to allow development to proceed and any off-site costs that may be required in order to support the development shall be at the developer's cost.

(5-1-2572) 4.0 SPECIAL INDUSTRIAL DEVELOPMENT AREAS

The City's Industrial Land Review 2010-2015 determined that while the City contains vacant industrial land, the existing inventory is fragmented and some of it is not considered to be prime land for industrial development due to location, lack of access to a highway, topographical constraints, tenure issues, and other factors such as inadequate servicing and power supply. The establishment of Special Industrial Development Areas facilitates future industrial growth in the City by reserving lands in various locations that are suitable for development. These areas are not yet zoned for development and require individual land use, servicing, environmental, and traffic and design studies prior to development approvals. A range of light, medium, and heavy industrial uses may be considered, subject to evaluation.

GOALS

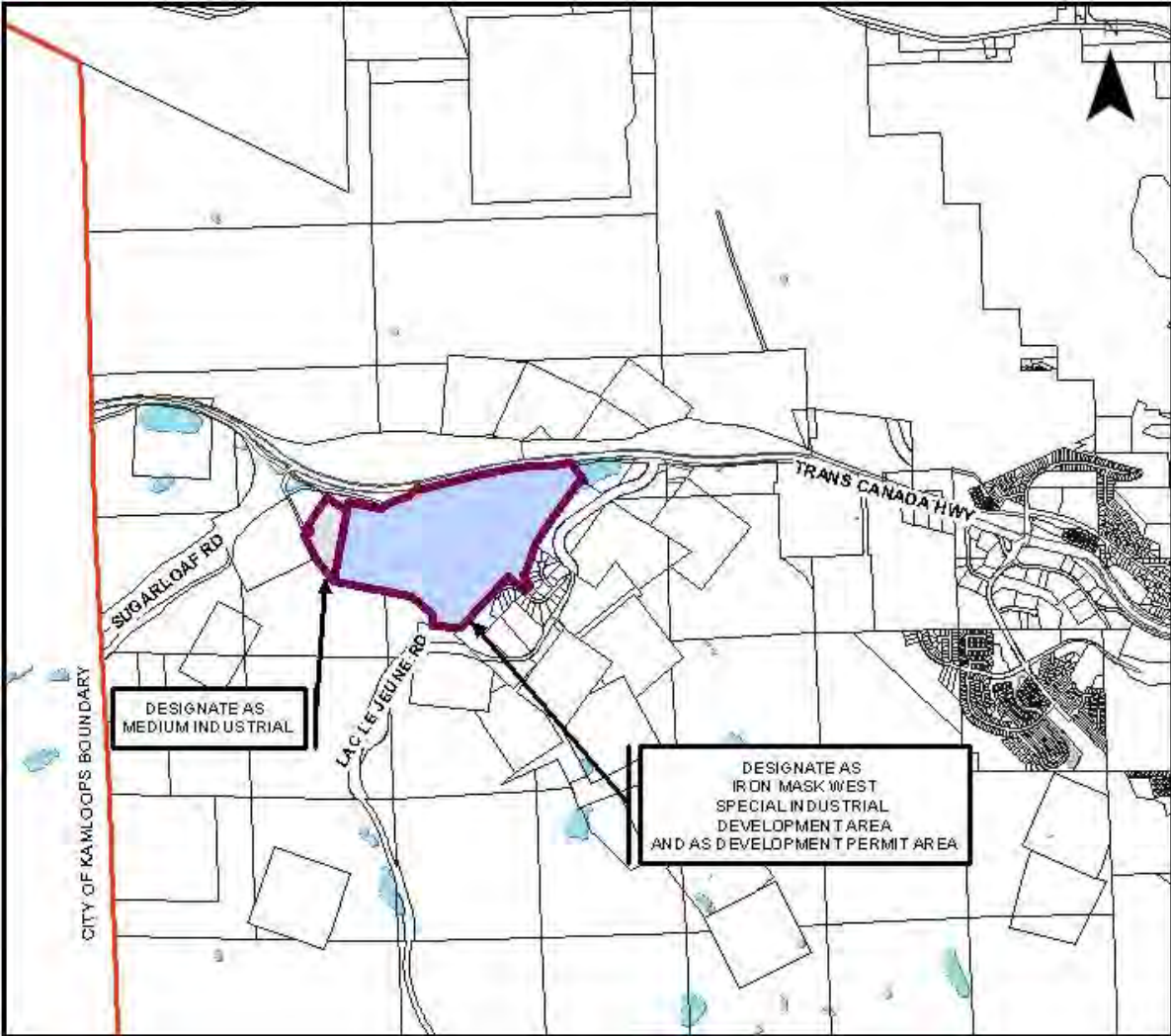
- To reserve sufficient lands to accommodate future industrial growth to the year 2036 and beyond.
- To ensure that appropriate design considerations are made to facilitate attractive development at the entrance to the City and reduce conflict with other land uses in the proximity.

Special Development Area Policies

4.1 The following Special Industrial Development Areas are shown on Map 1. These are areas that may be considered for development within the term of the plan and that require special study and evaluation prior to designating specific land uses. They will be evaluated individually on the basis of consistency with the policies contained in this plan. Where these areas are located in an entrance corridor to the City, careful consideration will need to be made to ensure that any development makes a positive visual impact on people entering and leaving the City, and that other factors such as noise, dust, and traffic are considered. The use of Development Permit Area designation may be used to control the form and character of development.

4.2 The following is identified as a Special Industrial Development Area:

Iron Mask West - This is an area located between the Iron Mask West and Bowers Industrial Areas, with access to the Trans Canada Highway via Sugarloaf Road and the Trans Canada Highway Frontage Road. Development of this area will need to address environmental constraints including the presence of two small salt ponds, topographic issues, and subsurface mineral rights registered against title. The need for improved access to the Trans Canada Highway will require cooperation with the Ministry of Transportation and Infrastructure.



SECTION III - NEIGHBOURHOODS

The Shuswap people have lived in villages comprised of pit houses at the confluence of two rivers for thousands of years. The first Europeans came less than 200 years ago, in 1811, when two fur traders from the Pacific Fur Company crossed the height of land from the Okanagan and spent a winter "among a powerful nation called the She-Waps". The fur traders returned the following year and "encamped at a place called by the Indians Cumcloups, near the entrance to the North Branch of Thompson's River." (The spelling of the place name is probably a misprint; other early references spell it Kamaloops, with three syllables.)

These first American fur traders were joined in November 1812 by traders from the Montreal-based North West Company. Eventually, the North West Company was bought by the Hudson's Bay Company, which operated a fort north of the Thompson River from 1821 until moving to the North Shore in 1842 then over to the South Shore in 1862.

The discovery of gold in the late 1850s brought the first wave of fortune seekers into the Interior of BC. One of those early entrepreneurs was James McIntosh who arrived in 1866. McIntosh invested in land and various businesses, including the first townsite along what is now Victoria Street West and the West End.

The construction of the Canadian Pacific Railway along the southern bank of the Thompson River in the early 1880s solidified Kamloops' position as a major supply and distribution point for ranches, lumber, and mining in the surrounding region. One of the newcomers was John Andrew Mara who, along with two partners, formed the New Townsite Syndicate in 1884. The Syndicate bought up John Peterson's ranch, everything from 1st Avenue east to where 6th Avenue now runs, down to the river, and up the hill past what is now Columbia Street. They had the land properly surveyed with streets, blocks, and lots on a perfect grid which stood in contrast to McIntosh's jumble of streets, crooked lanes, and odd shaped lots in the West End. Thus 1st Avenue became the border between two visions of development, a pattern that has continued to this day.

Mara and his partners set about enticing businesses, schools, and residents to locate in the new townsite. A real coup was achieved with the construction of St. Andrew's Presbyterian Church in 1887 well inside the new townsite. Nevertheless, many members of the establishment remained faithful to McIntosh and built lavish homes in the West End.

These conflicting forms of development placed the Bank of Commerce in a quandary. It could see the way of the future lay eastward with Mara and his partners, but its reputation dictated that it choose McIntosh on the establishment side. This explains why the bank was located in the middle of 1st Avenue, on an awkward triangle of land where Victoria, Lansdowne, and 1st Avenue all come together.

The feuding between Mara and McIntosh raged for years. It eventually forced people to lobby hard for incorporation, the only way to wrest control of Kamloops from private hands into those of elected community officials. Kamloops was incorporated in 1893. One of the first acts of new Mayor Sibree Clark and Council was to buy McIntosh's water and electrical companies.

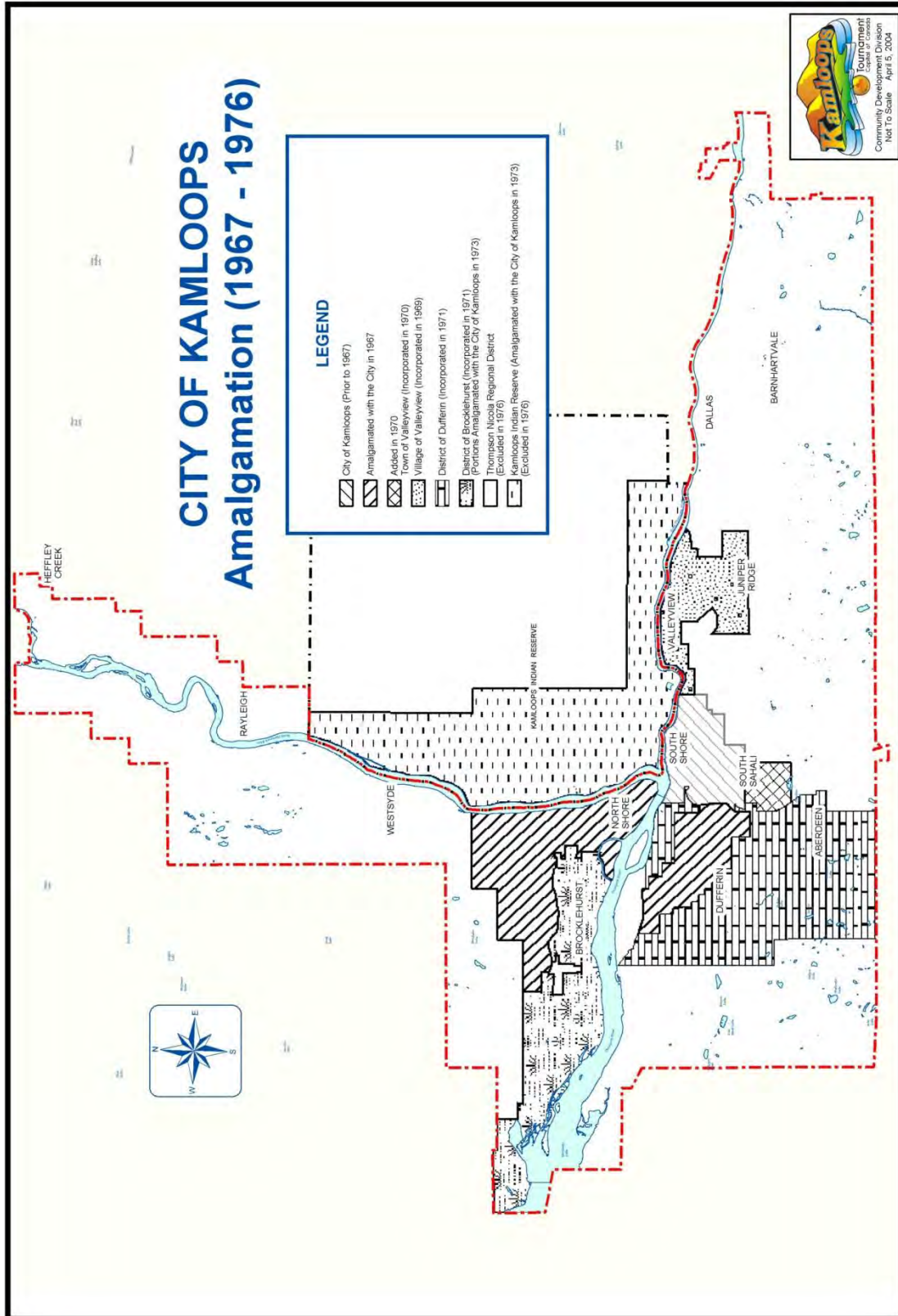
Development on the north side of the river proceeded at a much slower pace. The first ranch was pre-empted in 1866. In 1903, Canadian Real Properties, an English syndicate, planned an irrigation scheme for its properties on the North Shore. Progress was slow until 1909, when they incorporated as BC Fruitlands and more capital became available. The superb irrigation scheme, using water from Jamieson Creek, was a major achievement and served a massive orchard on Blocks A, B, and C, roughly equivalent to what is now Westsyde, Brocklehurst, and North Kamloops.

The Valleyview neighbourhood got its start in 1946 as a Veteran's Land Administration subdivision for veterans returning from World War II. It incorporated as the Village of Valleyview in 1969 and as the Town of Valleyview in 1970.

When the Trans Canada Highway was completed in 1962, it was routed along the South Thompson River, and Columbia Street became a segment of the new highway. This explains the presence of motels along Columbia, which have continued in business to this day. The bypass was built in 1973.

Kamloops' first enclosed shopping mall, Thompson Park Mall, opened in 1970. Early department stores, such as the Hudson's Bay Company (now the Kamloops Daily News building) and Woodward's (now BC Lottery Corporation), were also located in the City Centre. Commercial development on the North Shore occurred more slowly, with the Northhills Mall being built on the site of the Skyway Drive-In in the late 1970s. Both of these early shopping centres were challenged in 1980 with the construction of Kamloops' first regional shopping centre, Aberdeen Mall in the Southwest Sector.

On the political front, Kamloops saw significant changes in the 1960s and 1970s. The first change occurred in 1967 when the Town of North Kamloops was amalgamated with the City of Kamloops. 1967 also saw the construction of the Yellowhead Bridge over the South Thompson River and the construction of a control tower at Kamloops Airport.



The 1967 amalgamation was followed six years later by a more widespread amalgamation which brought the Town of Valleyview, the District of Dufferin, the District of Brocklehurst, and unincorporated parts of the Thompson-Nicola Regional District such as Westsyde, Rayleigh, Heffley, Dallas, and Barnhartvale into the City of Kamloops. (The amalgamation initially included parts of the Kamloops Indian Band land, but these were removed from the City in 1976.) The legacy of the 1973 amalgamation, including different standards of development in different neighbourhoods as required by the previously five separate jurisdictions, is still a factor in Kamloops thirty years later.

Land Use Map

Land use designations are shown on Map 1 - Generalized Land Use Plan. Individual lots are not identified, and the boundaries between land use zones are approximate. Following are explanations of each land use category and policy guidelines governing future development. The land use map designates a sufficient land base for each category to the year 2036 (120,000 population horizon).

1.0 RESIDENTIAL DEVELOPMENT

GOALS

- To encourage infill, intensification and full utilization of existing service capacity prior to peripheral expansion.
- To ensure a broad range and choice of housing types and locations capable of meeting the needs of residents of various age groups, family types, lifestyles, and income levels.
- To ensure that residential development proceeds in an orderly, cost-effective manner.
- To retain established neighbourhood character, amenities, and quality while encouraging more diversity.
- To encourage innovation, flexibility, and quality in the provision of housing.
- To encourage suitable housing opportunities and convenient community services for residents having special housing requirements.
- To encourage suitable affordable housing options for Thompson Rivers University students.

Residential Development Policies

- 1.1 There are three residential categories: **Urban**, **Suburban**, and **Rural**. These are identified on Map 1.
- 1.2 Residential development will continue to be modelled on the following neighbourhood concept in **Urban** areas:
 - elementary school within walking distance,
 - complementary local commercial/service uses,

- mix of uses,
- range of housing types/densities,
- separation of local/through traffic,
- pedestrian and bicycle linkages,
- combined neighbourhood park/elementary school, and
- parks, including passive and active space, that provide for a wide range of recreational opportunities and purposes.

1.3 The City will consider residential development proposals within **Urban** areas in order to encourage infill, intensification, redevelopment, and cost-effectiveness.

1.4 The **Urban** category identifies the principal residential areas housing most of the community (88% of the City's residents). There are 28,180 dwellings in the **Urban** area compared to 3,280 (10%) dwellings in the **Suburban** areas and 620 (2%) **Rural** dwellings (2001 census).

(5-1-2630)

Urban areas generally have complete City services and incorporate a full range of housing types (single, duplex, suites, townhouses, apartments and mobile homes) with net densities ranging from about 10 to 150 units per hectare. The City strongly encourages the infill of urban areas prior to peripheral expansion.

1.4.1 Appropriate urban land uses include:

(5-1-2630)

- single and two family dwelling units,
- garden suites/secondary suites/carriage suites
- multiple family residential,
- mobile home development,
- community care facilities (limited by regulation),
- home-based businesses (limited by regulation), and
- parks and public open space.

(5-1-2563)

1.4.2 Associated urban uses which require individual evaluation include the following:

- schools, churches, and other institutions,
- neighborhood commercial,
- recreation facilities,
- larger scale community care facilities; and
- intensive residential development.

1.4.3 Criteria used to evaluate urban development proposals will include:

- community consultation,
- compatibility with adjacent land uses,
- access and transportation considerations, including pedestrian and bicycle linkages and transit,
- proximity to arterial routes,
- availability and standard of services,
- interaction with related facilities and amenities, including shopping, community facilities, parks and leisure services, and schools,
- scale and intensity of development consistent with existing neighbourhood standards, and
- landscaping, parking, and site design standards.

(5-1-2630)

1.4.3A General criteria to consider in evaluating the siting of suites should include all of the following:

- Availability and capacity of existing municipal servicing (i.e. water and sewer capacity, fire servicing, etc.);
- Availability and accessibility of public transit;
- Encouragement of suites on corner lots and lots with lanes;
- Suites shall be discouraged on properties fronting a cul-de-sac or on panhandle lots;
- There are no on-street parking restrictions and physical barriers such as fire hydrants, crosswalks, mailboxes, or other similar features;
- Noise and visual impact associated with the suite are minimized through the provision of additional landscaping on-site or other applicable measures;
- Ability of the suite to be sensitively integrated with existing/surrounding neighbourhood (i.e. addressing issues such as height, footprint, massing, scale, and setbacks); and
- Suite proposals for lots located in the silt bluffs, flood plain, or other hazardous areas will be discouraged and subject to individual evaluation.

(5-1-2563)

1.4.4 Intensive Residential Development will be encouraged as follows:

- in the case of garden and carriage suites,
 - as an alternative to the subdivision of large lots;
 - on standard sized double fronting lots or on lots serviced by a lane; and
 - in flood prone areas.

- in the case of Small Lot Residential Development
 - as a form of infill development where a neighbourhood is characterized by small lots and small single family dwellings.

1.5 Multiple family residential development is considered an appropriate form of development within the **Urban** designated areas and in some commercial locations. The City will encourage the distribution of multiple family accommodation throughout the community in a variety of locations to avoid large concentrations of higher density housing and to encourage more diversity within neighbourhoods. Dispersed multiple family development should be limited to a size and scale appropriate to the neighbourhood. As a general guideline, new low- and medium-density multiple family development may be limited to a maximum size of approximately 50 units per development cell to ensure sensitive integration. Development Permit Area designation may be used for multiple family residential projects to ensure integration.

1.5.1 Multiple family developments shall provide sufficient amenity space for the recreational needs of their residents. Where multiple family developments are located in close proximity to neighbourhood parks, the City will explore partnership/funding opportunities with the developer to provide recreational equipment for all neighbourhood residents in the neighbourhood park space.

1.5.2 Low-density multiple family development includes townhouses and other cluster forms of development at net densities of about 25 to 50 units per hectare.

1.5.3 Medium-density multiple family residential includes stacked townhouses and walk-up apartments with net densities ranging from 40 to 100 units per hectare.

1.5.4 High-density multiple family residential is limited to the City Centre, North Shore Towne Centre, and Tranquille Commercial District and is designed to accommodate high-rise apartments at net densities of 80 to 150 units per hectare.

High density - multiple family residential development on the property identified as Lot A, D.L. 255, K.D.Y.D., Plan KAP78609, except Plans KAP79406, KAP79471, and KAP79881, will be subject to a 206-unit per hectare density limit.

1.5.5 The City will encourage higher densities and mixed use clusters in **Urban** neighbourhoods, particularly in areas of greatest accessibility, along transit routes, or adjacent to major services and amenities such as schools, recreation facilities, or places of worship.

1.5.6 The City will encourage a variety of affordable student housing for Thompson Rivers University students, both on campus and in the "Main Street" section of the McGill corridor.

1.5.7 Where a townhouse multiple family development exceeds 50 units, the City will encourage the use of various building forms, exterior finishes, and architectural details to ensure design variation.

(5-1-2264)

1.6 The **Suburban** designation comprises outlying low-density residential settlement areas, including Rayleigh, Heffley Creek, Rose Hill, Viking Estates, Dallas, and central Barnhartvale. These areas are serviced by community or City water systems. Sewage disposal is by means of connection to the central sanitary sewer system (Dallas, central Barnhartvale). In some areas, sewage disposal is by on-site septic tank or by community septic field (Rayleigh). Densities range from about two to five units per hectare.

1.6.1 Appropriate land uses in suburban areas include:

- single family residential development,
- low-density cluster development,
- community care facilities (limited by regulations),
- home-based businesses (limited by regulations), and
- parks and public open space.

1.6.2 Associated uses which may be appropriate subject to individual evaluation include:

- two family residential development and suites, subject to municipal servicing,
- suites, subject to municipal servicing and infrastructure; proximity to and accessibility of amenities such as food and other retailers, schools, health services, and parks; and consideration of criteria identified under Item 1.4.3A above.
- schools, churches, other institutions,
- neighbourhood commercial, and
- recreation facilities.

1.6.3 These uses would be subject to individual evaluation prior to development. Criteria used to evaluate such proposals will include:

- community consultation,
- access and transportation considerations, including pedestrian and bicycle linkages and transit,
- availability and standard of municipal services and infrastructure,
- interaction with related families and amenities,
- scale and intensity of development consistent with existing neighbourhood, and
- landscaping, parking, and site design standards.

1.6.4 The City will limit additional subdivision in **Suburban** areas subject to current zoning, existing servicing, and area specific constraints (topography, access, soils, etc.).

(5-1-2630)

(5-1-2630)

1.6.5 Dallas is included in the **Suburban** category and will continue to develop on that basis, as outlined in the *Southeast Sector Plan (1992)*.

1.7 The **Rural** designation outlines existing areas of isolated single family residential development, including Knutsford, Iron Mask, south and east Barnhartvale, Karindale, and Noble Creek, with lot sizes ranging from 0.5 to 8 ha and greater. City services and utilities generally are not provided, although several of these areas are serviced by the central water system. Sewage disposal is primarily on-site septic tank.

1.7.1 Standards of servicing for these areas will continue to be rural. Domestic water service generally will be provided by individual wells. Where extension of City water service has been provided or is requested, it shall be designed to accommodate existing use only and not to provide or encourage further subdivision. Sewage disposal will be by individual septic tanks, subject to Ministry of Health approval.

1.7.2 The City will support the retention of large farms and/or ranches having a minimum parcel size of greater than 8 ha in order to ensure the continued economic viability of the ranch or farm.

1.7.3 The City will discourage further subdivision and development of **Rural** residential areas.

1.7.4 The City will not support additional rural residential development proposals which will result in the creation of parcels smaller than 8 ha in size beyond those areas currently zoned.

1.7.5 **Appropriate uses in rural areas include:**

- single family dwellings,
- mobile homes,
- **farming,**
- the keeping of livestock subject to the Animal Control Bylaw, and
- home-based businesses (limited by regulations).

1.7.6 The City will not support suites in designated rural areas.

1.8 The City will consider assigning priority for development proposals based on the following criteria:

- High - infill, intensification, or redevelopment of existing serviced areas.
- Medium - incremental expansion, including special development areas, where cost-effectiveness can be demonstrated.
- Low - peripheral expansion, requiring upgrading of infrastructure.

(5-1-2630)

- 1.9 In order to maximize cost-effectiveness, the City will continue to use development cost charges or other means of reducing general municipal costs.
- 1.10 The City supports the redesignation of lands between Ord Road and the CNR tracks for residential purposes. The City will require submission of an overall development plan addressing land use and servicing concerns, in particular drainage, prior to approving individual development proposals.

2.0 AFFORDABLE HOUSING

The provision of clean, safe, suitable, and affordable housing for all residents is a fundamental component of any healthy and sustainable community. Housing is a basic need which is essential to the physical and social well-being of all residents. Once shelter needs are met, a person can focus on employment, training, education, raising one's children, and being able to budget for food, clothing, and other expenses. Research has demonstrated that having an adequate supply of affordable housing reduces the demand on social services, including health care and correctional services.

The City of Kamloops has a long history of supporting the creation of affordable housing in the community. Over the past decade, the City has partnered with non-profit agencies to create 526 affordable housing units, including 303 seniors units (Legion, Cottonwood, Greenfield, Bedford Manor), 135 family units (Hilltop, Spencer Court, Stokstad, Elizabeth Court, Habitat), 67 singles/at risk units (New Life Mission, Lighthouse, Victory Inn), and 21 special needs units (Corner House, Hospice).

KAMPLAN 97: The Official Community Plan supported the use of grants in lieu to offset the cost of development cost charges (DCCs) for social/special needs housing. Since 1995, the City has provided \$672,588 in grants to ten projects to offset DCCs.

KAMPLAN 97 also included the policy that the City will partner into affordable housing projects by "identifying surplus City-owned land which could be used for social/special needs housing or directing a portion of proceeds from the sale of City-owned land to social/special needs housing through a Housing Reserve Fund". This policy has allowed City Council to provide \$1.36 million worth of City-owned land to six projects and to use revenue from the sale of land for the Legion Manor and Cottonwood seniors' housing projects to provide a direct cash contribution of \$100,000 to the Elizabeth Court family housing project through the Housing Reserve Fund.

In addition, the City has deferred receipt of \$935,000 for 25 years for the Legion and Cottonwood Manor projects. These projects are subject to an agreement between the City and the non-profit agencies to use 15% of the profit on the resale of units to purchase additional rental units tied to the SAFER (Shelter Assistance for Elderly Renters) guidelines.

More recently, senior levels of government have changed their focus and funding allocations in support of affordable housing. At the federal level, funding is focused on addressing issues of homelessness through the Supporting Community Partnership Initiative (SCPI). The SCPI program was initiated in 2002 and runs to 2006. Most of the \$3.4 million in SCPI funding received in Kamloops to date has been directed to programs, with minimal amounts going to transitional housing or shelters. At the provincial level, funding is currently (2003/2004) targeted to frail seniors through the Independent Living BC program. Given the limited focus of these two programs, the City's housing policy needs to address all segments of the affordable housing market, particularly those segments not covered by other programs.

In the spring of 2003, the City of Kamloops in conjunction with the University College of the Cariboo (Thompson Rivers University) conducted a housing needs analysis. The study concluded that female lone parent families, low income singles, and seniors were all in need of affordable housing. Due to the Statistics Canada privacy policy which creates data limitations, the need for special needs housing was not able to be identified.

Also in the spring of 2003, as part of the budget review process, City Council approved an annual allocation of \$50,000 to the Affordable Housing Reserve Fund. This money is intended to be used by Council to support appropriate affordable housing projects.

The City's affordable housing policy is based on a number of principles. These are:

- that senior levels of government will continue to be the prime source of funding to address issues of homelessness and housing affordability;
- that the City of Kamloops' role in housing is to assist in the timely processing of development applications and to partner with the non-profit sector where appropriate to address the needs of specific target groups;
- that the City of Kamloops is not a housing provider or manager; and
- that the non-profit sector will continue to initiate the construction of and be responsible for the ongoing management and maintenance of affordable housing projects.

The City of Kamloops recognizes the need for and benefits from ensuring an adequate supply of affordable housing. The City will continue to be a leader in providing innovative solutions that increase the supply of affordable housing to meet the needs of Kamloops residents. The City will continue to work with other levels of government and the non-profit sector to address issues of housing affordability.

DEFINITIONS

Affordable housing means:

- rental or owned housing provided to low income households who have an affordability problem (pay in excess of 30% of their income on housing) and earn less than the median income for Kamloops;

- is subsidized by senior levels of government or by a non-profit agency, which enables rents to be provided below-market rates on a "geared to income" basis;
- which are typically publicly owned or owned and operated by a non-profit agency; and
- do not include shelters or transitional housing.

Affordable housing is permitted in all residentially zoned areas of the City including the CBD (Central Business District), and C1-T (Tranquille Road Commercial) zones.

Shelters or hostels are emergency or temporary housing for males, females, youth, or families, including temporary housing for transient individuals who have no suitable permanent housing and include uses such as the Y Women's Shelter and the Men's Christian Hostel. Shelters or hostels are permitted in the P-4 (Public and Quasi-Public Use) zone under the Zoning Bylaw No. 5-1-2001.

Transitional housing means short-term housing for persons who, in addition to needing affordable housing, need non-housing support services including meals, housekeeping, medical care, counselling, and skills training to enable them to maintain occupancy in permanent housing. These non-housing support services may be funded by a variety of provincial programs, including health, social, and corrections-based programs.

ROLES

Senior levels of government are the best suited to be the prime source of policy development and funding to address issues of homelessness and housing affordability.

The City of Kamloops will participate, where appropriate, in federal and provincial initiatives that address housing needs in Kamloops.

The City of Kamloops is not a housing provider or manager.

The City of Kamloops will seek opportunities to partner with the non-profit and private sector to provide affordable housing in either new construction or in rehabilitating existing housing stock.

Partnership with the City of Kamloops will be on the basis of exempting development cost charges for not for profit rental housing in accordance with Section 933 (12) of the *Local Government Act*; making City-owned land available either at market value or below market value; supporting variances for innovative housing types; direct cash contributions; a combination of any of the above-noted mechanisms; or such other means as supported by City Council.

The Social Planning Council will advise City Council on housing issues, including recommendations on City of Kamloops funding support for affordable housing projects.

The Social Planning Council will identify research and public education needs related to housing. The research and public education may be conducted by the City of Kamloops, the Thompson Rivers University, or the non-profit sector.

The non-profit sector, including partnerships with the private sector, will build and manage affordable housing for the residents of Kamloops.

Affordable Housing Policies

2.1 The City will continue to monitor the housing market and pursue strategies designed to ensure that there is a reasonable balance between market demand and supply.

2.2 The City will continue to pursue the following affordable housing strategy:

- encourage a housing mix and a broad range of housing choice in all neighbourhoods in the City, including multiple family housing and affordable housing in all areas of the City designated as urban residential;
- encourage infill development on existing serviced lots prior to peripheral development which requires the construction of extensions to municipal utilities;
- encourage higher densities and intensification;
- encourage the physical design of multiple family housing projects to reflect the characteristics of and be sensitive to the neighbourhood. As density increases, so does the need for good urban design;
- expedite development applications for affordable housing projects by assisting non-profit sector applicants, providing guidance on the approvals process, assisting with neighbourhood consultation, and making recommendations to City Council in a timely manner;
- support site specific variances and zoning changes to permit unique forms of housing that are sensitive to neighbourhood characteristics;
- support, where appropriate, large-scale rezoning and subdivision that will increase the supply of housing to meet demand and provide choice for residents;
- utilize housing agreements to allow affordable housing in market (for profit) housing projects;
- update the housing needs survey, which provides statistical data on segments of the population in need of affordable housing, every five years as new census data becomes available;
- continue to work with the non-profit sector to monitor the supply and demand (wait-lists) for affordable housing; and
- encourage places of worship, service clubs, and non-profit agencies to work co-operatively to provide affordable housing for the residents of Kamloops.

- 2.3 The City will continue to provide financial assistance to affordable housing projects by:
- exempting the requirement to pay development cost charges for not for profit rental housing as provided under Section 933 (12) of the *Local Government Act*, and
 - considering the waiving of Building Permit fees and other development fees for affordable housing projects upon application.
- 2.4 The City may partner with the non-profit sector on affordable housing projects by:
- identifying surplus City-owned land which could be used for affordable housing, including the closure of surplus road rights-of-way or lanes where appropriate;
 - deferring payment for City-owned lands or making them available at below-market rates;
 - making a cash contribution to the project from the Affordable Housing Reserve Fund;
 - such other mechanism that may be suitable to increase the supply of affordable housing; or
 - identify surplus City-owned land appropriate for affordable housing and make it available to housing providers through a long-term lease at a cost of \$1.00.
- (5-1-2566)
- 2.5 The City will allocate \$50,000 per year to the Affordable Housing Reserve Fund. City Council may use these funds to assist the non-profit sector in providing affordable housing. Projects shall be considered based on the following guidelines:
- priority shall be given to non-market affordable housing projects for lone parent families, low income singles, including homeless and at-risk individuals, seniors, and residents having special needs, including physical and/or mental disabilities;
 - primary funding is to be provided by a senior level of government or by the private sector;
 - provide rental housing;
 - managed by a not-for-profit organization which is incorporated;
 - be located on sites suitably zoned for the proposed housing project;
 - be of scale and design sensitive to the surrounding neighbourhoods;
 - providing rents equal to the shelter allowances permitted to tenants in receipt of social assistance;

- the housing project can be new construction, an upgrade of an existing dwelling/apartment building, or a purchase of units in market housing for persons identified above;
- is within the City of Kamloops boundaries;
- projects that displace existing tenants will not be considered; and
- a minimum of four new units created.

2.6 The City will encourage developers of large-scale housing projects (over 20 units) to provide an additional 5% of units as affordable or rent geared-to-income dwelling units through a density bonus over and above the density allowed under existing zoning. Affordable housing units provided through density bonusing will be:

- exempt from development cost charges in accordance with Section 933 (12) of the *Local Government Act*, and
- subject to a housing agreement with the City of Kamloops.

The City will encourage developers to set up rental agreements with non-profit housing providers to manage the 5% affordable housing units on a rent geared-to-income basis.

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The City shall encourage affordable housing developments utilizing City-owned land to build 5% of units as adaptable. In developments intended to house seniors, the City shall encourage 20% of units to be adaptable.

2.7 The City will consider affordable housing projects that:

- adhere to the principles of the City of Kamloops Social Plan;
- strengthen and enhance the well-being of the community;
- address affordable housing concerns;
- follow the affordable housing policies of KAMPLAN 2004;
- address the need for clean, safe, suitable, and affordable housing;
- where possible, provide support and training to address barriers that individuals are facing;
- demonstrate partnerships with other service providers and businesses in the community;
- provide statistical data on numbers that access the affordable housing once it is established; and
- exhaust other funding options such as BC Housing, Provincial; SCPI, Federal, and CMHC partnership programs.

The fund shall not:

- provide direct financial assistance to individuals or families in the affordable housing project;

- make the City of Kamloops, if in receipt of funding, responsible for maintaining the project or of being a housing provider or manager; nor
 - provide assistance to nursing homes, retirement homes, student residences, and crisis line facilities.
- 2.8 The City will encourage non-profit housing providers to locate affordable housing projects in all neighbourhoods in the City. Affordable housing projects should be located:
- on transit routes in close proximity to schools;
 - in close proximity to shopping and medical services;
 - where support services such as counselling, training are accessible to residents; and
 - not within 1 km of a similar affordable housing project.
- 2.9 Areas of the City that are not suitable for multiple family, designed affordable housing are Heffley Creek, Rayleigh, Campbell Creek, Iron Mask, Karindale, Noble Creek, and Barnhartvale due to their remote location.
- 2.10 Ideally, affordable housing projects located on infill lots should be small scale to more sensitively integrate into the surrounding neighbourhood. However, small scale adds to the per unit cost, making such projects less affordable. Larger projects are more economically viable, but require a higher level of design to mitigate the potential impact on the surrounding neighbourhood. Projects in excess of 50 units will be discouraged by the City.
- 2.11 City Council shall consider partnership opportunities on the basis of the approved evaluation criteria, which may be amended from time to time. City Council reserves the right to not partner with any project. Funds requested will be reviewed with other requests. The City of Kamloops reserves the right to not allocate any funds. Funds may be distributed to one project or to several projects. The receipt of an Affordable Housing Reserve Fund does not guarantee funding in the years following. A presentation may be requested as part of the evaluation.
- 2.12 The City recognizes that secondary suites are an alternative form of affordable rental housing. The City will:
- continue to encourage the use of Development Variance Permits to allow secondary kitchens for aged, disabled, or immediate family members;
 - encourage the rezoning of proposed and existing neighbourhoods to allow secondary suites where they can be sensitively integrated; and
 - continue a limited enforcement procedure for illegal suite complaints.
- 2.13 The City recognizes that rooming house accommodation is an important low income housing option. The City will continue inspections of licensed rooming house facilities to ensure rooming houses meet fire safety standards.

3.0 COMMERCIAL DEVELOPMENT

GOALS

- To encourage efficient utilization of existing serviced and designated lands prior to outward expansion.
- To support the role and function of the City Centre, North Shore Towne Centre and Tranquille Commercial District as the primary commercial and cultural centres.
- To encourage a high standard of development to enhance the urban environment.
- To establish an orderly framework for guiding commercial activity.
- To encourage innovation and quality in design and development.
- To provide sufficient locational opportunities for commercial enterprises.
- To establish a strong, diversified commercial base to provide employment and a high level of service to residents and visitors.

Commercial Development Policies

3.1 There are three major commercial categories within the City:

(5-1-2398)

- **Town Centres**
 - City Centre
 - North Shore Town Centre
 - Tranquille Commercial District

Shopping Centres

- Shopping Centres
- Neighbourhood Centres
- Southwest Sector Commercial Area
- Southeast Commercial Area

Service/Highway Commercial

These are identified on Map 1. The City will consider commercial development proposals in these designated areas within the term of this plan.

(5-1-2398)

3.2 There are three major **Town Centres** within the City: **City Centre, North Shore Towne Centre, and Tranquille Commercial District**. Each town centre functions as a high activity, high density, major focal point for the City and includes opportunities to live, work, and play within the **Town Centre**. The **Town Centre** designation includes:

- 3.2.1 The City will support the role of the **Town Centres** as the focal points of economic activity in the community, particularly with respect to its:
- specialized retail and office functions,
 - governmental, institutional, and cultural functions
 - convention and hospitality industry functions,
 - transportation focus, and
 - high density residential function.
- 3.2.2 The City will encourage hotels, cultural, entertainment, performing arts, and convention facilities to locate within the **Town Centres** in order to develop the areas as the cultural and entertainment district for Kamloops. The City may consider use of appropriate approaches to encourage such development including, but not limited to:
- municipal real estate assets,
 - municipal authority to close lanes,
 - joint venture/partnership,
 - senior government grant programs,
 - off-site requirements waiver, and
 - revitalization tax exemptions program as per the *Community Charter*, Division 7, Permissive Exemptions, Section 226.
- 3.2.3 The **City Centre** identifies the City's central commercial core, commonly referred to as the Central Business District (CBD), which will remain as the focal point for the community and the surrounding region. The concentration of existing intensive commercial, major public facilities, cultural and high density residential activities creates a vibrant and recognizable centre of the community. High density development will be encouraged, with floor area ratios ranging from 3.0 to 6.0.

Government offices (except those with extensive storage requirements and those providing neighbourhood decentralized services) and major office buildings will be encouraged in the **City Centre**.

High density residential development will be encouraged within and adjacent to the **City Centre**. Mixed use residential/commercial will be encouraged, with residential above the ground floor.

Pedestrian oriented commercial activity will be encouraged within and adjacent to the **City Centre**, while land extensive and automobile oriented commercial uses will be discouraged. Parking will be provided primarily by way of parkades and underground parking incorporated within developments rather than at grade. Development will be encouraged by the elimination of parking requirements, continuation of the designated parking area, and the development of parkades.

The **City Centre** is subject to Development Permit Area designation in order to ensure appropriate consideration is paid to design and site development concerns (form and character, landscaping, streetscape, signage, etc.).

Many of the projects identified in the *City Centre Plan (2005)* have now been completed. The plan needs to be reviewed in light of these changes, and new capital projects and priorities need to be identified.

(5-1-2398)

3.2.4 The *North Shore Towne Centre* and *Tranquille Commercial District* are identified from the intersection of Tranquille Road and Fortune Drive, running south along Tranquille Road to finish near the intersection of Royal Avenue and Tranquille Road. These areas function as major commercial centres for the North Shore and the city as a whole. Tranquille Road is a relatively intensive mixed use shopping street with a distinctive character, appealing to many city-wide shoppers. The Towne Centre, on the other hand, has the biggest potential for redevelopment with some large vacant parcels of land ready for new construction initiatives. Extensive revitalization and continuing beautification efforts along Tranquille Road have successfully built on and enhanced its image. High density development, including office complexes and intensive commercial activities, will be encouraged, with maximum floor area ratios in the range of 2.0-4.0 and maximum residential densities permitted to 150 units per hectare.

Residential development will be encouraged above the ground floor. Intensive mixed use development will be encouraged, with limited peripheral expansion into adjacent residential areas.

Hotel/motel/restaurant and other tourism and recreation related facilities will be encouraged to take advantage of the opportunities created by McArthur Island Park and the City's Tournament Capital program.

Parking will be provided through on-site, at grade parking, on-street parking, and, where appropriate, parking structures should be incorporated into developments. Where parking is to be provided on-site, it should be provided underground or located to the rear of the lot. This area is well served by transit and is the location of the City's second major transit exchange.

The *North Shore Towne Centre* and *Tranquille Commercial District*, as all other commercial areas of the North Shore, are subject to a Development Permit Area designation in order to ensure appropriate consideration is paid to design and site development concerns, such as landscaping, streetscapes, signage, facade design, and building height and mass. Variances to further these regulations may be considered through the Development Permit process.

The *North Shore Neighbourhood Plan* (2008) promotes mixed commercial/residential land use, pedestrian and bicycle access, sustainable development, and green building practices and provides specific policy guidance for the continued development and enhancement of the *North Shore Towne Centre* and *Tranquille Commercial District* shopping areas.

- 3.3 The **Shopping Centre** designation identifies existing areas of concentration of retail use, primarily in the form of shopping malls. This includes major shopping centres, which serve City-wide and regional shopping needs, and minor or neighbourhood centres, which cater primarily to the shopping needs of the immediate neighbourhood. Neighbourhood centres include the Westsyde and Brocklehurst Shopping Centres and a proposed shopping centre adjacent to the Trans Canada Highway in Dallas.

The **Shopping Centre** designation also includes two distinct areas of commercial activity which are distinguished not by type of land use activity, but by the scale and scope of development and the primary market areas served. These two areas are:

- the Southwest Sector commercial area extending from the Sahali Mall and Superstore in the north to Costco in the south; and
- the Southeast commercial area in Valleyview extending from the Valleyview interchange along the Trans Canada Highway to Tanager Road.

All shopping centre categories permit a wide range of retail, entertainment, and personal service uses, including restaurants.

3.3.1 Shopping Centres

Appropriate uses include retail, personal service, automobile oriented service, convenience uses, restaurants and entertainment, small clinics and limited office, and motel/motor hotels. Residential use is not considered appropriate within the confines of a regional shopping centre, but may be appropriate in adjacent commercial development and should be encouraged nearby in the form of multiple family accommodation.

Densities will continue to be limited to a scale associated with shopping centre and similar development, with maximum floor area ratios of 0.5 to 1.0.

Parking will be at grade and on-site for the most part. Landscaping, screening, access, signage, and other site design and development concerns may require additional consideration by way of Development Permit Area designation.

Commercial activity adjacent to existing centres will be encouraged to develop in clusters and nodes and integrate with existing development through use of shared access points, parking, and sympathetic site development.

3.3.2 Neighbourhood Centres

Neighbourhood centres range from about 5,000 m² to 20,000 m² in size. These centres provide a similar range of goods and services in terms of retail, personal service, office, convenience, and food outlets as the larger centres do, but generally service a smaller market. Accordingly, there is less pressure to develop complementary and associated satellite commercial facilities.

For the term of this plan, these centres should be encouraged to redevelop in a manner consistent with the urban village/town centre concept. Residential development in conjunction with a neighbourhood centre should be encouraged. Development and intensification of land use will be considered in light of potential impact on surrounding residential neighbourhoods, particularly with respect to site design, landscaping, lighting, signage, parking and access. Density of development should be encouraged to increase from floor area ratios of about 0.3 - 0.4 to 1.0 - 1.5.

3.3.3 Southwest Sector Commercial Area

In the late 1990s, the Southwest Sector of the city evolved rapidly into a regional shopping centre serving a regional market in excess of 120,000 population. Shoppers from throughout the Thompson-Nicola Regional District and the South Cariboo travel to Kamloops for their shopping needs.

Originally anchored by the Sahali Mall and Aberdeen Mall, the Southwest Sector commercial area saw modest expansion in the mid-1980s with the construction of the Columbia Square Shopping Centre. It was not until the mid-1990s that the Southwest Sector's role as a regional shopping centre was solidified with the construction of the City's first big box retailers, Costco and Real Canadian Superstore. These big boxes have been joined more recently by Wal-Mart and Home Depot. The Southwest Sector has achieved "critical mass", which will likely ensure its continued dominance of the regional retail sector for the term of this plan.

The City will encourage large-scale, big box retailers catering to a regional market to locate on arterial roads, where the road network is able to handle the anticipated traffic demands.

3.3.4 Southeast Commercial Area

The Valleyview area is unique in the Kamloops commercial context. It contains elements of a neighbourhood centre for the residents (e.g. Cooper's and the Valleyview Shopping Centres), a service commercial area (along Falcon Road), and a highway commercial area along the Trans Canada Highway. In addition, the Valleyview commercial area hosts Convergys, which employs over 1,000 people, and the Kamloops Auto Mall between River and Tanager Roads, as well as the Kelly Douglas light industrial area.

Given the wide variety of commercial and light industrial uses in the southeast commercial area, any new development or redevelopment must be compatible with adjacent land uses and use high quality design standards to improve or enhance the appearance of the area, reflecting its location on the eastern gateway to the City.

The City will support redevelopment of the Flamingo Road area of Valleyview for commercial or multiple family residential use on an individual application basis. In order to minimize the impact of redevelopment on neighbouring properties, special attention must be paid to building massing, landscaping and screening, views, lighting, signage, access, and parking.

- 3.4 The **Service/Highway Commercial** designation identifies those commercial areas which cater primarily to automobile oriented enterprises or which rely on arterial and highway exposure for their business. Generally the uses permitted in this category include a broad range of service and convenience uses, uses requiring relatively large land areas in relation to building size, including tourist and accommodation and strip commercial development, automobile sales and service, and related outlets.

This category includes strip or ribbon commercial development along arterial routes on the Trans Canada Highway East and West as well as service or highway commercial clusters such as the Rogers Way area, the Tranquille Road/Briar Avenue area, Victoria Street West, and Victoria/ Battle Street between 7th Avenue and the Valleyview interchange.

(5-1-2398)

Excluded are general retail and office uses which are more appropriately located in the **City Centre, North Shore Towne Centre and Tranquille Commercial District**, or **Shopping Centre** categories, although some limited retail and office activity is permitted. Land use intensity should remain generally low, with floor area ratios from about 0.3 - 1.0.

- 3.5 The City will encourage more intensive utilization of existing serviced land and existing commercial floor space prior to outward expansion.

3.5.1 Small scale local commercial developments serving the needs of the immediate neighbourhood have not been specifically identified, but generally will be encouraged and are permitted in areas designated other than commercial, including residential, subject to the following criteria:

- community consultation;

- access and transportation considerations, including pedestrian linkages and transit usage;
- availability and standard of services;
- scale and size of development appropriate to the neighbourhood;
- minimum distance of 0.5 km to nearest elementary school;
- distance/access to other neighbourhood facilities, services, amenities, and institutions;
- landscaping, screening, parking, signage and site development standards;
- combining residential development with local commercial; and
- zoning approval and Building Code requirements.

Development Permit Area designation may be used in order to ensure sensitive integration into existing and planned neighbourhoods.

3.5.2 Commercial conversion developments will be considered in areas designated other than commercial subject to the following criteria:

- community consultation;
- uses shall be limited to those that are intended to serve the needs of the immediate neighbourhood;
- building style shall retain the basic form and character of a single family dwelling;
- landscaping, screening, parking, signage, and site development standards shall be consistent with the neighbourhood;
- residential development will be considered an appropriate ancillary use as part of a commercial conversion project; and
- zoning approval and Building Code requirements.

Development Permit Area designation may be used in order to ensure sensitive integration into existing and planned neighbourhoods.

3.5.3 The City will support the continuation and expansion of home-based businesses within residential areas provided they have minimal impact on other residents within the neighbourhood.

3.6 The City will ensure that the need for managing development is balanced by the need for maintaining a flexible marketplace orientation and business friendly climate in order to encourage development.

3.7 The City will encourage a high standard of quality in commercial development and will ensure that it complements and enhances the urban and natural environments. Development Permit Area designation may be used to help guide the form and character of commercial areas in the City.

- 3.8 The City will discourage further strip commercial development along arterials or highway routes. The City will encourage additional arterial commercial development to locate within existing designated development areas or in planned cluster developments. Careful attention must be paid to access, parking, landscaping, screening, and signage in these kinds of developments in order to avoid conflict with arterial and highway traffic patterns and to provide an attractive streetscape for residents and visitors alike.
- 3.9 The City will encourage the revitalization and beautification of existing arterial commercial developments and, for that purpose, may invoke Development Permit Areas or develop design guidelines.
- 3.10 The City will support and work with the two Business Improvement Areas (BIAs) and one Business Association as a means of maintaining, upgrading, and revitalizing commercial areas. The two BIAs are the North Shore BIA, the Kamloops Central BIA, and one Business Association is the Valleyview and East Business Association. In addition, the City will support the efforts of the Kamloops Chamber of Commerce and Venture Kamloops to promote business in Kamloops. The Southwest BIA closed in December 2004.
- 3.11 The City will encourage the establishment or evolution of sustainable commercial centres having the following qualities or characteristics:
- higher densities should be encouraged in town and neighbourhood centres having the necessary urban services;
 - residential development should be encouraged above the ground floor in commercial development;
 - building forms should be encouraged to promote a more transit and pedestrian friendly environment; and
 - a mix of uses should be encouraged, with strong emphasis placed on good design and a high standard of development.
- 3.12 The City will support the establishment of neighbourhood centres which serve local neighbourhood needs including personal services, convenience/food outlets, and small-scale offices. Mixed use development will be encouraged in conjunction with the neighbourhood centre concept.
- 3.13 The City will discourage commercial activity in the Iron Mask area for the term of this plan.
- (5-1-2346) 3.14 The implementing zoning regulations shall not allow kennels, in any form, to locate within the City of Kamloops.

Kennels may be considered, upon receipt of an application for rezoning, subject to a parcel specific evaluation. The evaluation may take the following criteria into consideration:

- The size of the facility including the maximum number of dogs to be housed;
- The facility's distance from proposed property boundaries;

- Adjacent land uses;
- Screening requirements as found within the implementing by laws; and
- Other matters the City considers relevant including possible sound attenuation measures and neighbourhood consultation.

The City stresses that it is the responsibility of the applicant to demonstrate, to the City's satisfaction, that any potential adverse impacts upon adjacent properties are addressed.

(5-1-2614)

- 3.15 The City will discourage the clustering of pawnshops by not allowing them as of-right in retail trade areas. The City will consider site specific rezoning applications for pawnshops, subject to the following criteria:
- The scale of the establishment relative to the development site and adjacent properties;
 - Compatibility with adjacent land uses;
 - Visibility of the establishment from the street; and
 - Consultation from relevant business improvement areas.

4.0 INDUSTRIAL DEVELOPMENT

GOALS

- To encourage industrial development as a means of expanding and diversifying the City's economy and tax base.
- To encourage a high standard of development to enhance and protect the urban and natural environment.
- To encourage efficient utilization of existing services and serviced land.

Industrial Development Policies

- 4.1 Land designated industrial on Map 1 will be considered for industrial development within the term of this plan. Included in this category is the full range of industrial activity: **Light**, **Medium**, and **Heavy**.
- 4.2 Land designated **Light** industrial shall be oriented towards warehousing, storage and distribution, light manufacturing, high technology industries including research and development, and wholesaling, with some retailing of large bulky goods requiring extensive storage and display space. Office uses will be prohibited except those related to high technology uses and research and development. The City will discourage further encroachment of retail commercial and general office uses into industrial areas. Service uses serving industrial employees such as recreation facilities and restaurants are appropriate in industrial areas.

4.2.1 Southgate Industrial Park

The Southgate Industrial Park, situated adjacent to the Thompson Rivers University is a high standard, light industrial park in transition. Southgate has evolved from a district exclusively permitting a range of light industrial uses to an area now permitting a broader range of activities including service commercial, government, and major utility office space, as well as technology based offices.

In 2001, the City, in partnership with the University College of the Cariboo (Thompson Rivers University) and the Southwest BIA, examined the evolution of Southgate and how to enhance its relationship to the university. Given its proximity to UCC and the southwest regional shopping centre, the City supports the continued evolution of Southgate in accordance with the policies of the *McGill Corridor/Southgate Concept Plan (2002)*.

The City recognizes that the offices currently located in the government precinct in the City Centre may relocate to the government office complex at Dalhousie Drive and McGill Road within the time frame of this plan.

A node of service commercial development will continue to evolve on Notre Dame Drive between Columbia Street West and Hillside Drive. New arterial and service commercial development along Notre Dame Drive will need to give careful attention to site planning and design to minimize impact on surrounding industrial uses. As well, new activities should not limit the ability of Notre Dame Drive to function as an arterial roadway.

Future development within the Southgate Industrial Park will be considered in light of the following criteria:

- new development needs special attention to site planning and design to minimize impact on surrounding industrial uses;
- new activities will not limit the ability of the roads to function as movers of large truck and transport traffic. Parking will be provided through on-site at grade parking. On-street parking will be discouraged; and
- continuation of the current landscaping, screening, signage and site development standards will be required.

The Southgate Industrial Park is subject to Development Permit Area designations: one for the "Main Street" area along McGill Road, and one for the arterial commercial area along Notre Dame Drive. The main street designation along McGill Road seeks to strengthen the City's role as a university city and to enhance the expansion of the university by encouraging compatible mixed use development off campus. The detailed design guidelines for these two areas are contained in Schedule 1 of this plan.

The *McGill Corridor/Southgate Concept Plan (2002)* provides specific policy guidance for the term of this plan.

- (5-1-2319) 4.2.2 Retail trade that is complementary and subordinate to an industrial use will be permitted in the area classified as "Technology Park" in the McGill Corridor/Southgate Concept Plan (2002).
- 4.3 Land designated for **Medium** and **Heavy** industrial purposes will continue to be oriented towards more extensive manufacturing activities including the storage and processing of raw materials, such as logs and wood products, sand/gravel, concrete and minerals, metallic industries, and petroleum products. These activities require special attention by reason of their potential impact on the urban and natural environment, and accordingly, the City may undertake special initiatives such as Development Permit Area designation, an environmental assessment, or other appropriate mechanisms.
- (5-1-2572) 4.4 The City will encourage infill and the development of existing serviced industrial lands prior to considering outward expansion. Cost-effectiveness will be a significant criterion in evaluating development proposals; however, given the demand for industrial lands in the southwest sector, the City will support the creation of Iron Mask West as a Special Industrial Development Area to accommodate future development.
- 4.5 The City will ensure that access to industrial development is designed in such a manner as to minimize conflict between industrial and residential traffic.
- 4.6 The City will ensure that the impact of industrial development on adjacent land uses will be minimized through the use of buffers, screening, landscaping, site design and the arrangement of buildings, parking, and circulation. This will be critical in areas where industrial development is close to residential land use. The City may invoke Development Permit Areas to control and manage these site development concerns.
- 4.7 Where large areas of land are designated for industrial purposes (e.g. Campbell Creek), the City will require an overall development plan addressing land use and servicing concerns to be prepared prior to approving individual development proposals.
- 4.8 The City will support the establishment of increased employment generating industrial and commercial activity on the North Shore, particularly as this relates to the future development of the airport lands for light industrial uses.
- (5-1-2572) 4.9 In the long term, the City of Kamloops should encourage industries that have an impact on air quality to locate out of the valley so as not to adversely impact the Kamloops airshed. The redevelopment of the old oil refinery site in the Iron Mask East Industrial Area and the expansion of industrial development in the Iron Mask West Industrial Area should be pursued to provide means of achieving this goal.
- 4.10 The City will work with Venture Kamloops on an ongoing basis to identify the needs of companies that may be interested in investing in Kamloops and outline strategies for meeting these needs.

5.0 AGRICULTURAL/RESOURCE LANDS

GOALS

- To preserve and protect environmentally sensitive and unique natural areas.
- To ensure that agricultural lands are preserved for agricultural purposes and to enhance the viability of agricultural operations within the City.
- To discourage urban development into the Agricultural Land Reserve.
- To retain public access to Crown lands for recreation and other public use purposes.

Agricultural/Crown Land Policies

- 5.1 This category includes lands within the Agricultural Land Reserve (ALR), lands outside the ALR but used for agricultural and grazing purposes or for resource development, Crown lands used for grazing or other specified purposes including public enjoyment and recreation, and lands not contemplated for urban development within the term of this plan.
- 5.2 The City will support the maintenance and enhancement of farming on land designated for Agricultural/Resource use on Map 1, except as noted in Policy 5.3 below.
- 5.3 The City will not support applications for exclusion from the ALR, with the exception of the following:
- minor adjustments/fine tuning of the ALR boundary in the East Barnhartvale area to reflect the Agricultural Land Commission endorsement of exclusion for the easterly portion of parcel A-D, Plan 22553.
 - three remnant parcels along the South Thompson River between Valleyview and Dallas.
 - the McGowan Park South Special Development Area shown on Map 1. This area is a Special Development Area within the Agricultural Land Reserve and further discussions must occur with the Agricultural Land Commission (ALC) for possible exclusion.
 - airport runway expansion to remove 21.37 ha from the Agricultural Land Reserve to extend the runway 915 m and to create buffer zones required by NAVCANADA. Some of the buffer zone will continue to have spray irrigation and be farmed for hay. Further discussions must occur with the Agricultural Land Commission (ALC) for possible exclusion.
 - when a site specific request is made that the City deems to be in the public's best interest, consultation will occur with the Agricultural Land Commission (ALC) to explore exclusion options from the Agricultural Land Reserve.

- "Softball City" is adjacent to the Kamloops Airport runway at the southwest corner. Two of the ten ball fields are within the Agricultural Land Reserve. There is interest to remove approximately 5.5 ha so as to proceed with the Tournament Capital of Canada Facility Expansion. Further discussions must occur with the Agricultural Land Commission (ALC) for possible exclusion.

The City considers utility installations, landfills, and composting sites as appropriate uses within the ALR. Agricultural Land Commission input must be sought through the ALR application process.

- 5.4 The City will encourage the use and development of private lands within this category for agriculture, including intensive agriculture, or for farming purposes. Single family residential uses on large lots are appropriate, including mobile homes, and ancillary residential uses where necessary for agricultural or for farming purposes. Other uses may include utility installations; municipal facilities including landfills, composting sites, and spray irrigation activities; and open land recreational activities such as hiking and horseback riding trails. The City will consult with the Agricultural Land Commission (ALC) about how it intends to mitigate any negative impacts of open land, recreation, and other non-farm uses on agriculture, for example, fencing, signage, etc.
- 5.5 The City supports the expansion of the Lac du Bois Grasslands Provincial Park as part of the *Land and Resource Management Plan* initiative.
- 5.6 The City recognizes the agricultural, recreational, and public use potential of Crown land in this category and supports the provincial government's general policy of integrated multiple use land management.
- 5.7 The City will discourage subdivision of lands within this category. City utilities and services generally will not be provided to these lands. Where water extensions have been approved, they will be limited to domestic supply only on the basis of one connection per existing lot.
- 5.8 The City will attempt to minimize conflicts between agricultural and other land uses (residential/recreational) through the use of:
- access restrictions, where appropriate;
 - minimum distance setbacks for intensive agricultural operations;
 - fencing requirements and landscape buffers for residential developments adjacent to agricultural operations;
 - restriction of further residential development in outlying areas; and
 - continued liaison with Provincial Ministries and Crown agencies in the planning, disposition, and management of Crown lands.

Development Permit Area designation may be used to protect farming on lands designated for agricultural use.

- 5.9 The City supports the ALR and the role of the Provincial Agricultural Land Commission in its efforts to protect and enhance farmland. Where land is in the ALR, minimum parcel sizes shall apply only when that land is:
- excluded from the ALR; or
 - approved for subdivision within the ALR pursuant to the *Agricultural Land Commission Act*, regulations thereto, or orders of the Commission; or
 - exempted by the *Agricultural Land Commission Act*, regulations thereto, or orders of the Commission.
- 5.10 The City will encourage all farming operations to comply with the following regulations and guidelines as administered by the province:
- environmental guidelines for farming practices as produced by the provincial ministries;
 - regulations pertaining to agricultural waste control; and
 - code of Agricultural Practice for Waste Management.
- 5.11 As the City of Kamloops is surrounded by grassland, the City will encourage preservation of large tracts of grassland areas for grazing and ranching. The City values the ecological diversity found in grassland areas:
- The City will encourage landowners to maintain the quality, integrity, and continuity of grassland habitats.
 - The City will consider partnering with the Grasslands Conservation Council of British Columbia and other similarly interested groups on future community planning and land use designation as it relates to conservation and stewardship of grasslands within City boundaries.
- (5-1-2652) 5.12 As the City recognizes the need to produce the equivalent of 10% of City (corporate) energy needs through alternative energy systems, the City will consider the siting of wind turbines on agricultural lands provided the wind energy project:
- a) Will be compatible with the general nature of the surrounding area; and
 - b) Will not be detrimental to the health or general welfare of the people living or working in the surrounding area, or negatively affect other properties or potential development in the surrounding area.

Each wind turbine will be considered on a site-specific basis and considered on its merits and mitigation of potential adverse impacts. Wind turbines will be subject to a rezoning process and a comprehensive review of the following criteria:

- a) Turbine height is sized appropriately for energy generation and safety;
- b) Number of turbines should be based on intended use (on or off the grid), property size, and proximity of abutting residences;

- c) Turbine spacing (in the case of multiple turbines) shall be appropriate based on industry standards;
- d) Setbacks as well as separation distance between dwellings on site, on neighbouring properties, and public roads are appropriate to prevent potential safety hazards;
- e) Sound generated is minimal and in accordance with decibel levels suggested by the Canadian Wind Energy Association;
- f) Visual impact is considered (e.g. locating wind turbines more in line with dominant topographical features such as ridge lines or other features such as transmission lines) and shadow flicker is mitigated;
- g) Turbine(s) are approved by the Canadian Standards Association (CSA);
- h) Authorization is granted from other regulatory agencies as applicable including, but not limited to, the Agricultural Land Commission (for Agricultural Land Reserve lands), Ministry of Environment, Transport Canada, BC Hydro, and NAV CANADA;
- i) Evaluation of sensitive habitats and ecosystems in proximity to the proposed wind turbine(s) shall be completed to account for environmental sensitivities that may exclude wind turbine feasibility;
- j) Turbines inactive for two or more years shall be decommissioned and the site restored to its natural state; and
- k) Turbines and associated components shall be a neutral colour with a non-reflective matte finish and contain no advertising.

6.0 SAND/GRAVEL EXTRACTION

GOALS

- To designate areas of existing and future sand and gravel extraction.
- To minimize land use conflicts between sand and gravel extraction operations and neighbouring properties.

Sand/Gravel Extraction Policies

- 6.1 Map 1 identifies the location of existing and potential areas of sand and gravel extraction. Sand and gravel extraction will be limited to the areas designated on Map 1 for the term of this plan. Non-commercial operations which involve only the excavation or deposit of material for limited construction purposes may be permitted where necessary, subject to the Earthwork Control Bylaw.

- 6.2 Sand and gravel extraction is limited to the excavation, screening and storage of materials. No additional processing of any kind is permitted, including crushing or the manufacturing of asphalt, concrete, or other materials. Such activities are permitted only in **Heavy** industrial areas or where permitted by Temporary Industrial Use Permits under conditions imposed by City Council.
- 6.3 The City will require sand and gravel extraction to be conducted in a manner which limits the impact on neighbouring properties including control of hours of operation, dust control, screening, access, traffic circulation, and site reclamation. The City may utilize the powers available to it under the *Local Government Act* including, but not necessarily limited to, the Earthwork Control Bylaw, the Zoning Bylaw, Development Permit Area designation and Temporary Use Permits to ensure that operations are conducted in an appropriate manner.

(5-1-2635) 8.0 GOLF COURSE

GOALS

- To promote environmental stewardship in the development, redevelopment, and maintenance of golf courses within the City;
- To support the role of the Agricultural Land Commission in advancing its mandate of protecting agricultural land assets in reviewing changes to existing golf courses within the Agricultural Land Reserve and proposals to develop new courses;
- To encourage developments to proceed in a manner that is sensitive to environmental features;
- To encourage private recreational amenities that complement the City's Tournament Capital program;
- To enable a mix of appropriate land uses that cater specifically to golfers and golf-related tourism
- To consider the implementation of design guidelines to ensure a consistent form and character for golf resorts/residential developments and the protection of natural environmental features; and
- To consider the development of off-season activities where it is demonstrated that the impact on adjacent properties is minimal.

Golf Course Development Policies

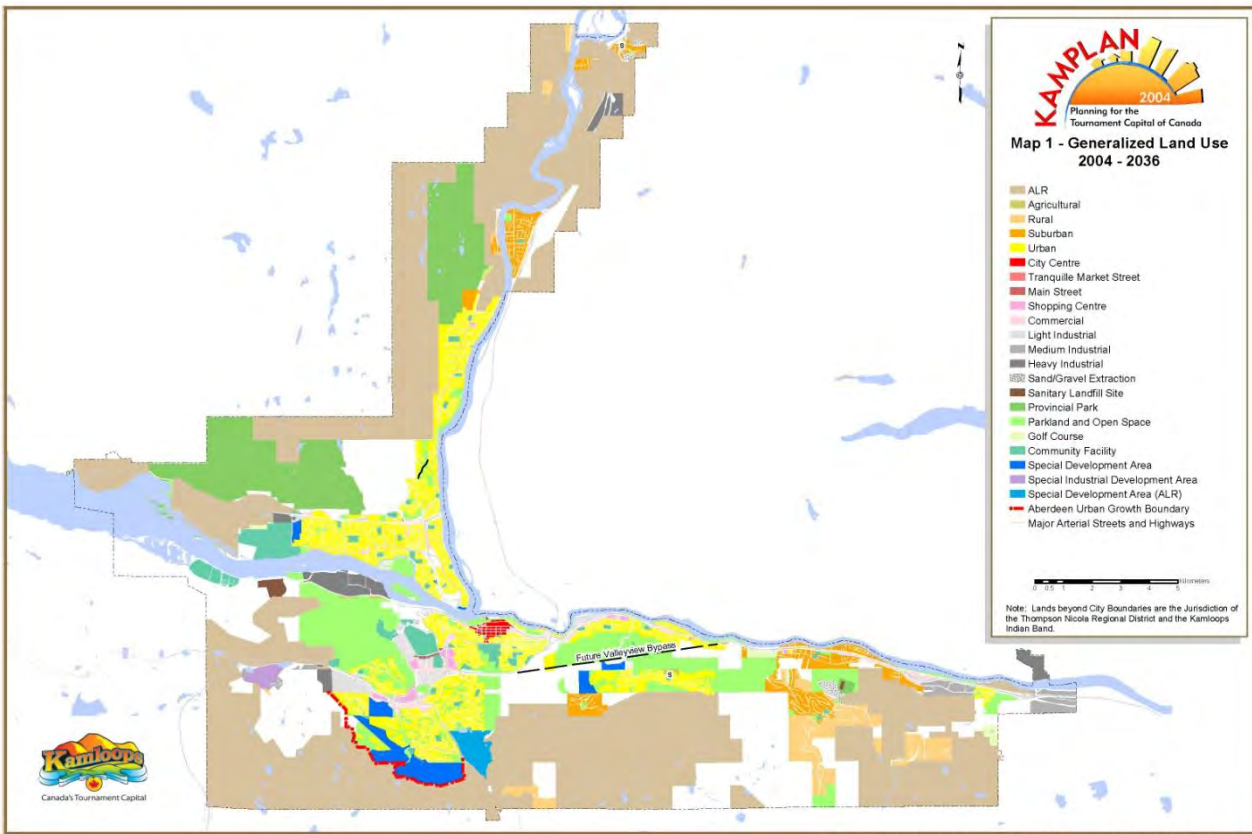
- 8.1 Locating new golf courses within the Agricultural Land Reserve shall only be supported subsequent to detailed consultation with the Agricultural Land Commission.
- 8.2 The City will not permit the use of domestic water sources for irrigation purposes.

- 8.3 Accessory commercial uses (restaurants; concessions; the sale, rental, and repair of sporting equipment; and banquettes/meeting spaces) shall be encouraged on a limited scale.
- 8.4 Additional accessory uses that may be considered appropriate subject to a site specific evaluation and public consultation include:
- 8.4.1 Single, two family, and multiple family residential development where:
- The lands are designated Urban;
 - Support from the Agricultural Land Commission is obtained¹ for the development of lands within the Agricultural Land Reserve;
 - Compatible with adjacent land uses and the density does not exceed 25 units per hectare; the City may consider low and medium density, multiple family residential development to a maximum density of 75 units per hectare as a means to protect identified environmentally sensitive lands, provide larger buffers between adjoining properties, and reduce the developable footprint to ensure large tracks of land remain viable for agricultural purposes;
 - Located in proximity to public transit;
 - Amenities such as shopping, community facilities, parks, leisure services, and schools are available and can service the residential development;
 - The scale and intensity of the development respects the existing neighbourhood;
 - Landscaping, parking, and site design standards are maintained;
 - The proposed development can be fully serviced without requiring extension of municipal infrastructure; and
 - The development generally complies with all other components of the Official Community Plan.
- 8.4.2 A hotel, motel, or bed and breakfast facility where:
- Tourist traffic can be directed away from local roads preferably to arterial routes;
 - The proposal can be sensitively integrated and is demonstrated to have limited impact on adjacent properties;
 - The building height recognizes existing built form, does not compromise privacy, and is positioned in such a manner as to preserve viewsapes to the extent possible.
- 8.4.3 A spa and/or health facility where developed strictly in conjunction with a hotel/motel;

8.4.4 A campground where:

- The design recognizes adjacent land uses and incorporates sufficient buffers;
- Adequate servicing is available to accommodate the development;
- Tourist traffic can be directed away from local roads preferably to arterial roads;
- The proposed development complies with the City's Campground Guidelines (2002), as amended;

8.4.5 Other compatible outdoor recreational facilities such as hiking/biking trails, cross-country ski facilities, an outdoor arena, and other such facilities that do not negatively impact adjacent lands; and



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SECTION IV: QUALITY OF LIFE

The residents of Kamloops are blessed with a multitude of sports and cultural facilities which greatly enhance our quality of life. We have 82 parks, including the 749 ha Kenna Cartwright natural park, the McArthur Island Sports Complex, and the Hillside Stadium/Canada Games Pool at the Thompson Rivers University, as well as Sport Mart Place, home of the Kamloops Blazers. On the arts scene, we have the nationally recognized Kamloops Art Gallery, two theatres (Sagebrush, Pavilion) which play host to the Kamloops Symphony Orchestra and the Western Canada Theatre, plus numerous community-based arts organizations. In addition, Kamloops is a mecca for mountain bikers and is the gateway to Sun Peaks, one of the top ski resorts in North America.

In 2001, the City branded itself the "Tournament Capital of Canada" (TCC) to build on the success of our Tournament Capital of BC program in athlete development, sports tourism promotion, and economic development. The City hosted 105 tournaments in 2003, which accounted for 22,940 visitors, 66,743 participant days, and an estimated \$8.3 million in spending going into the local economy. 2003 was declared "The Year of the Tournament".

In November 2003, City residents approved a referendum on the Tournament Capital of Canada Facility Expansion Program which will upgrade facilities at both the McArthur Island Sports Centre and the Hillside Stadium/Canada Games Pool complex (estimated cost \$37.6 million over the next five years).

The upgrades on McArthur Island include:

- soccer centre - reconstructing six fields plus a tournament field with bleachers and a tournament centre;
- baseball centre - consisting of 12 diamonds and a full renovation of Norbrock Stadium;
- sports centre - including a second ice surface (Olympic size), renovations to the curling rink, and trade show/convention space; and
- park enhancements - including increased parking and improved signage.

The new facilities at Hillside Stadium will include:

- athletics/gymnastics field house - including a 200 m indoor track built to international standards; a multi-function gymnasium with three basketball/ volleyball courts, moveable partitions, and seating for 2,500; over 1,200 m² of gymnastics space; and lease space for retail, fitness club, food court, and/or sports medicine; and

Facilities

82 parks
 39 tennis courts
 25 ball diamonds
 16 soccer pitches
 10 golf courses in the area
 10 fitness centres
 5 arenas
 4 racquet sports facilities
 3 indoor pools
 2 theatres
 2 curling rinks
 2 outdoor pools
 2 beaches
 2 lawn bowling greens
 2 skateboard parks

- Hillside Stadium upgrades - including a resurfaced and relocated track, artificial infield surface, upgraded bleachers, and additional parking; and two soccer fields with reconstructed grass.

The *Parks and Recreation Master Plan* sets the overall direction for many services provided by the City of Kamloops to enhance the quality of life for all residents. The plan includes detailed policies for eight functional areas:

- Tournament Capital of Canada,
- parks and natural areas,
- recreation facilities,
- programs and activities,
- arts and culture,
- social planning,
- cemeteries, and
- parks and recreation delivery system.

This plan will be supplemented by a *Cultural Strategic Plan (2003)*, which provides more detailed direction for the City's support of heritage preservation, cultural presentation, cultural tourism and marketing, and cultural infrastructure.

The City has developed the following goals and policy statements in response to quality of life issues.

1.0 PARKS AND RECREATION

GOALS

- To ensure that all citizens of Kamloops have adequate opportunities for personal growth in their leisure, thereby enriching the life of individuals and the community.
- To protect, preserve and enhance the environment for the enrichment of the community and the enjoyment of all people.
- To ensure that public parkland of all types is distributed throughout the community in an equitable manner to meet the community's needs.
- To develop and maintain an aesthetically appealing environment.
- To support the development of a strong community image.

Parks and Recreation Policies

- 1.1 The *Parks and Recreation Master Plan (2003)* will serve as a general guide for future parkland acquisition and facility development. The plan will be updated and revised in accordance with changing demographics, patterns of development, and user needs.

- 1.2 Construct \$37.6 million in new athletic facilities on McArthur Island and at Hillside Stadium/Canada Games Pool as part of the Tournament Capital of Canada Facility Expansion Project.
- 1.3 Aggressively promote the Tournament Capital of Canada strategy as a mechanism to enhance the quality of life for all Kamloops residents.
- 1.4 The City will provide recreation and park facilities and programs at a cost that is affordable to users and taxpayers.
- 1.5 The City will ensure art, cultural, athletic, and other recreation services are accessible to all citizens.
- 1.6 The City will identify prevailing demographic and leisure trends and ensure that these are met through the provision of leadership, facilities, and programs.
- 1.7 The City will encourage and facilitate the provision of recreation opportunities by sports groups, service clubs, cultural groups, and other community organizations.
- 1.8 The City, subject to the availability of resources, will continue to provide financial and, where necessary, administrative and management support for a variety of major sports and cultural facilities, including the following:
 - Canada Games Aquatic Centre,
 - Westsyde/Community "Y" indoor pools,
 - Brocklehurst and McDonald Park outdoor pools,
 - Museum/Archives,
 - Art Gallery,
 - Sagebrush/Pavilion Theatres,
 - Wildlife Park, and
 - Arenas (Memorial, Sports Centre, Brocklehurst, Valleyview, Sport Mart Place).
- 1.9 Six categories of parkland are designated and recognized in this plan:
 - **City-wide parks,**
 - **district parks,**
 - **neighbourhood parks,**
 - **tot lots,**
 - **natural areas,** and
 - **biking/walking trails.**

Map 2, Parks and Schools, designates the approximate location of existing and proposed parks in these categories. Map 2 also identifies the approximate location of trails, parks, and natural areas that should be protected or preserved prior to the onset of future development.

- | | |
|---|--|
| <p>1.10 City-wide parks are so designated by reason of their size, the scope of recreation facilities, uniqueness, and/or environmental characteristics which make them attractive to the entire community. Improvements to and expansion of City-wide parks should be a priority, with special attention given to the acquisition of riverfront property to extend the "riverfront park system" along the North and South Thompson Rivers.</p> <p>1.10.1 The <i>Kamloops Rivers Plan (1988)</i> and <i>Millennium Rivers Trail (2000)</i> outline a long-term comprehensive development concept for the City's riverfront land on a joint basis with the Kamloops Indian Band. The City will seek to re-establish its partnership with the Kamloops Indian Band in order to achieve the future development of the Rivers Trail.</p> | <p><u>City-wide Parks</u></p> <p>McArthur Island</p> <p>Riverside Park</p> <p>Pioneer Park</p> <p>Exhibition Park</p> <p>Memorial Hill Park</p> <p>Gaglardi Square</p>
<p><u>District Parks</u></p> <p>Brocklehurst</p> <p>Albert McGowan</p> <p>Rae-Mor</p> <p>Pacific Way</p> <p>Centennial</p> |
| <p>1.11 District parks include open space developed for recreation purposes which provide for passive recreation and active facilities of a more intense nature than a neighbourhood park, including senior and competitive level sports and activities on a year-round, day and evening basis.</p> <p>1.11.1 The acquisition of lands necessary for the future development of district parks may be independent of a secondary school site, although planning for joint use school site/park development should be considered at the time of acquisition.</p> <p>1.11.2 The minimum size for district parks should be:</p> <ul style="list-style-type: none"> • 6 ha to 16 ha ideally as level land, or • 12 ha with 8 ha of usable land when developed in conjunction with a secondary school. | |
| <p>1.12 Neighbourhood parks include open space which is developed and used for neighbourhood recreation purposes, including:</p> <ul style="list-style-type: none"> • playgrounds, • passive landscaped parks, and • athletic and playing fields. <p>1.12.1 The joint development of neighbourhood parks and elementary schools is encouraged.</p> <p>1.12.2 The minimum size for a neighbourhood park should be:</p> | |

- 2.0 ha of level land, or
- 4.0 ha with 2.8 ha of usable land when developed in conjunction with an elementary school.

1.12.3 The City should review the value of existing tot lots (small playground areas) in older, well established neighbourhoods in order to determine whether or not the parkland should be retained, redeveloped for other recreation or community uses, or sold.

1.12.4 In areas where a substantial amount of infilling is anticipated, neighbourhood park sites should be provided, with priority for sites adjacent to existing park or school grounds.

1.13 **Natural areas** include lands which are:

- of a unique environmental nature and require special planning prior to their being opened to any public usage;
- of an environmentally sensitive nature which may limit their development potential for urban uses, including passive recreation;
- of archaeological or historical value and may contain heritage sites; and
- of biological diversity and special ecosystems inclusive of grassland areas.

The City will continue to implement the recommendations of the *Kenna Cartwright Park Master Plan (1999)* and the *Peterson Creek Master Plan (2000)*.

Expansion of natural areas should be a priority, with special attention given to the acquisition or dedication of large-scale, unique natural areas such as the Valleyview benchlands, Coal Hill in Aberdeen, and a proposed addition to the Lac du Bois Grasslands Provincial Park.

The general land use map (1) designates parkland and open space which is City and Crown Land. It also shows private open space which is not for public use. These two areas are noted as it is the City's intent to have these areas as undeveloped because of the unique landscape features and non-developable soils.

1.14 **Biking/walking trails** are developed or natural pathways linking neighbourhoods, connecting existing parks, or serving to preserve or protect important corridors in the City.

The City will continue to implement the recommendations of the *Bicycle Master Plan (2002)*, the *Pedestrian Master Plan (2002)*, and the *Millennium Rivers Trail (2000)* to create an integrated City-wide trail network.

Where feasible, new multi-use trails (pedestrian and bicycle) will be encouraged throughout the City in order to provide recreation alternatives and to connect areas of the City. In particular, consideration should be given to linking neighbourhood parks with district parks, natural areas, and City-wide parks through a continuous trail system using open space, corridors, ravines, gulleys, and walkways.

- 1.14.1 A safe pedestrian/bicycle route through the Valleyview interchange (estimated cost \$1.8 million) should be aggressively pursued in co-operation with the Ministry of Transportation and ICBC through the Safer City program.
- 1.14.2 When committees are created to develop bike and walking trails, the Ministry of Transportation recommends a representative from the Southern Interior Transportation Region and one from the Thompson Nicola Transportation District be involved on the committees.
- 1.15 The City will work with the mountain biking community and adjacent community associations to develop a mountain bike park between the Juniper and Valleyview neighbourhoods.
- 1.16 Private lands designated as parkland and open space are not prohibited from alternative forms of development by reason of that designation. The purpose of the designation is to identify lands which, for the term of this plan, are expected to be used and maintained as parks, recreation and open space areas, or as major land extensive private recreation areas.
- 1.17 Private golf courses serve a valuable function not only as private recreation facilities, but as urban open space. Resort residential development may be considered as an appropriate ancillary use in conjunction with golf courses, subject to the following criteria:
 - availability of servicing,
 - scale/intensity of development,
 - access, and
 - consistency with the City's overall growth management strategy.
- 1.17.1 The Eagle Point Golf Course may develop into a destination resort within the time frame of this plan.
- 1.18 Conceptual development plans for future parks should be prepared in conjunction with local residents, potential user groups, local naturalists and, where appropriate, senior levels of government to ensure that the diverse needs of the community are adequately addressed.
- 1.19 At the time subdivision takes place, the City may take five percent of the subdivided land for future park development or take the cash equivalent of the five percent parkland dedication in accordance with the *Local Government Act*. The decision between land or cash in lieu and criteria for selection will be based on the following:

- Where the subdivision takes place in proximity to a designated neighbourhood park, district park, city-wide park, natural area, or biking/walking trail (as shown on Map 2) and contains land required to complete the park, land may be taken as dedication.
 - Where a subdivision is removed from the location of a designated park, cash in lieu of parkland may be required of the developer unless the developer owns or has the opportunity to purchase land in proximity to a designated park, in which case the land required to complete the park may be taken as dedication.
 - Ravines and areas of steep slope are important natural components that add to the biodiversity of a local ecosystem. These lands are not suitable for development. At the time of subdivision, the City may, at its discretion, designate these lands as open space under the Subdivision Control Bylaw or take such lands as a portion of the five percent parkland dedication.
 - The silt bluffs between Valleyview, Juniper, and Barnhartvale are hazard lands subject to Development Permit guidelines.
 - Parcels of less than 0.4 ha may not be accepted for park dedication unless the land will form an integral part of a designated park.
 - The priority for parkland dedication or acquisition will be city-wide park, district park, neighbourhood park, natural areas, and biking/walking trail.
 - Funds acquired in lieu of parkland dedication may be used to acquire land for designated parks. Funds acquired can also be used to improve or develop these categories of parks.
- 1.19.1 Where the City's Development Cost Charge Bylaw provides for a charge to acquire or improve/develop parkland, the bylaw shall identify the category of parkland, refer to a specific park location for which the charge is being collected, and the capital cost for which the charge is to be collected. Where parkland dedication or cash in lieu is also required for the same park identified in the Development Cost Charge Bylaw, the developer shall provide the greater of the two charges (the development cost charge or the parkland dedication).
- 1.19.2 The funds collected through development cost charges may be used for fencing, landscaping, signage, public art, benches, drainage and irrigation, trails, restrooms and changing rooms, and playground/playing field equipment.
- 1.20 Where smaller landscaped spaces are considered to be desirable elements of a development, neighbourhood or commercial area, local landowners will be encouraged to construct and maintain these areas as "community projects" for the benefit of the local area. Priority areas for enhanced landscaping include:
- the Tranquille Road corridor from the airport to Crestline Road,

- the south side of Fortune Drive between 8th Street and Sydney Avenue,
 - the intersection of Fortune Drive and Leigh Road/Fort Avenue, and
 - the Valleyview corridor along the Trans Canada Highway between Tanager Road and Comazzetto Road.
- 1.21 Outline plans should be prepared for all new development areas in order to determine parkland and school requirements and to identify specific sites. These plans should also identify the appropriate mechanism to acquire identified parks.
- 1.22 The City will identify additional land (approximately 15 ac.) within the term of this plan that will provide for the future cemetery and burial needs of the community at such time as the existing Hillside Cemetery is at capacity. The preferred location for a future cemetery is a portion of the City-owned land at the west end of McGill Road. Its close proximity to Hillside Cemetery will ensure operational costs are minimized.

2.0 COMMUNITY FACILITIES

GOALS

- To ensure that community and social services are available to meet the needs of existing and future residents.
- To designate sufficient land to accommodate the needs of community and social service facilities.
- To maintain existing service levels, improve where feasible, and encourage maximum utilization of existing service systems prior to expansion.
- To encourage sensitive integration of small-scale care facilities within neighbourhoods.
- To support the senior governments' primary role in the delivery of health care, affordable housing, and social support services.

Community Facilities Policies

- 2.1 Major community facilities and institutions are designated generally on Map 1 and include the following:
- schools,
 - Thompson Rivers University,
 - Royal Inland Hospital,
 - major community care facilities,
 - Kamloops Regional Correctional Centre,

- federal and provincial government facilities,
 - airport, and
 - municipal facilities.
- 2.2 The City will encourage and facilitate the provision of community and social services and facilities through co-operation, liaison and, where appropriate, joint provision and participation. The City will not significantly expand its role in the direct provision of community and social services for the term of this plan.
- 2.3 School District No. 73 has experienced declining enrollment throughout the district and recently closed two schools in Kamloops. This trend is expected to continue as household size decreases and the population ages.
- 2.3.1 A new elementary school may be necessary in Juniper prior to the 100,000 population horizon. No site has been identified to date and should be considered as development of Juniper West proceeds. The school should be developed in accordance with the joint elementary school/neighbourhood park concept.
- 2.3.2 No new secondary schools are anticipated within the term of this plan.
- 2.3.3 Given that significant growth is not anticipated in either the Crestline Avenue area of Brocklehurst or in Dallas, the School District should be approached regarding the potential use of the vacant designated school sites for other alternative forms of development (housing, parks/public use, community use).
- 2.3.4 Schools that may close due to declining enrollment should remain available for redevelopment to serve other community uses. The City should continue to pursue opportunities for co-operation with the School District in the provision of facilities for residential, community, or social services.
- 2.3.5 The City supports the Community Schools concept and will encourage the School District and the appropriate provincial government ministry to increase the number of schools and neighbourhoods benefiting from the program.
- 2.4 Royal Inland Hospital will continue to expand as a regional referral centre as population growth warrants or new programs are developed or expanded. Sufficient land has been designated, as shown on Map 1, to accommodate future expansion needs.
- 2.5 The demand for extended, intermediate, and specialized care facilities will continue to increase as the community's population ages and as the provincial government continues its policy of deinstitutionalization. Sufficient land has been designated to accommodate the future expansion plans of these facilities for at least the term of this plan.

- 2.5.1 The City will continue to encourage the integration of smaller care facilities such as group homes and child daycare within existing and planned residential neighbourhoods. These facilities will be limited in size to reduce the potential impact on the neighbourhood. Where new community care facilities are proposed and they require a change in zoning or are not permitted, the City will evaluate the proposal based on the following criteria:
- community consultation;
 - level/availability of servicing;
 - access to transit/transportation
 - access to amenities (community shopping, parks, services, facilities, clinics, etc.)
 - scale/intensity appropriate to neighbourhood; and
 - proximity to facilities or uses of a similar nature.
- 2.6 Major government administration offices will be encouraged to locate or remain in the City Centre to take advantage of the linkages between civic offices, the RCMP, Courthouse and Land Title Office, and major financial and legal offices. Government facilities which require extensive land for storage of materials, heavy equipment, or which have research and development activities associated with the operation will be encouraged to locate in Southgate Industrial Park.
- The Government Precinct area of the City Centre has the potential to change within the term of this plan. Should the existing government administrative offices, in particular Forestry and Transportation and Highways, relocate to the Dalhousie Drive and McGill Road area of the Southgate Industrial Park, redevelopment of the precinct area to medium- and high-density multiple family housing would be appropriate, along with some related retail/service/office commercial.
- 2.7 Aggressively support the retention of senior government functions to maintain and enhance Kamloops' role as a regional government centre.
- 2.8 The City supports the Kamloops Museum and Archives, the Kamloops Art Gallery, the Western Canada Theatre, and the TNRD Library as they endeavour to provide a wide range of informational/educational/cultural services to the community.

3.0 HERITAGE

Since 1979, the City of Kamloops has actively encouraged the preservation of valuable heritage buildings and sites. That year, the City established the Kamloops Heritage Advisory Committee, now called the Kamloops Heritage Commission, to advise Council on matters pertaining to heritage. While some important heritage structures have been demolished since then, awareness of the importance of heritage buildings has grown considerably. There is now a substantial inventory of residential, commercial, and public structures that have been recognized as significant heritage resources.

Buildings and property that are officially designated by bylaw must maintain certain standards of appearance and maintenance in order to protect their heritage value. Designation, heritage covenants, and heritage revitalization agreements provide legal, permanent protection of heritage resources. Ten buildings in Kamloops are legally protected heritage resources, including:

<u>Address</u>	<u>Original Owner/Name</u>	<u>Date</u>
118 Victoria Street	Bank of Commerce	1904
7 Seymour Street West	Court House	1909
245 St. Paul Street	Stuart Wood School	1907
5225 Yellowhead Highway	Seven-O-Ranch	1867
850 Lombard Street	Chinese Cemetery	1887
Columbia Street West	McIntosh Memorial	1932
961 Lorne Street	Sheep Breeders Building	1929
159 Seymour Street	St. Andrew's Presbyterian Church	1887
500 Lorne Street	CNR Station	1926
405 Victoria Street	Plaza Hotel	1926

Heritage recognition, as opposed to designation, is a voluntary conservation program undertaken by the Kamloops Heritage Commission. It has no legal restrictions on construction materials or additions. In order to receive heritage recognition, however, the building must substantially retain its original appearance. There are currently over 55 properties which have received a heritage recognition plaque. The list of properties can be obtained from the Development and Engineering Services Department. Additional buildings and resources are expected to receive plaques as new applications are received from property owners.

Another mechanism to protect heritage resources is a Heritage Registry which will be developed by the City and the Kamloops Heritage Commission over the next five years. Properties in Kamloops will be evaluated to determine their historic significance. The Registry does not provide official protection of a site; however, it does provide City Council with additional protection measures for heritage properties as identified in the *Heritage Conservation Act*.

The City of Kamloops, through legislation established under the *Heritage Conservation Act* and the *Local Government Act*, is offering a Tax Incentive Program (TIP) to property owners to assist with the costs of preservation, restoration, and/or rehabilitation of an eligible Heritage Resource within the City of Kamloops. The Tax Incentive Program has evaluation criteria.

A Historic Buildings Inventory is also being developed to identify resources that have historical value.

Among recent accomplishments within the heritage field are the heritage protection for the CNR Station and the Plaza Hotel under Heritage Revitalization Agreements and relocation of the Wilson Street House to 115 Tranquille Road. In future, the Development and Engineering Services Department will work towards the creation and implementation of a heritage management plan.

The Kamloops Heritage Commission is working on several projects including the Heritage Recognition Plaque Program, hosting the annual heritage achievement awards reception, development of a heritage open house to coincide with Heritage Week, promoting Kamloops heritage through the use of publications and printed material, and working towards a public art program based on historic events, themes, or people, such as the Overlanders statue in front of City Hall.

GOAL

- To preserve, enhance and promote the community's cultural heritage for the benefit of residents and visitors alike.

Heritage Policies

- 3.1 The City will identify and conserve the community's cultural heritage through designation, recognition plaques, completing a Heritage Registry, and installing public art reflecting historically significant Kamloops events, themes, and residents.
- 3.2 The City will place \$20,000 per year in a Heritage Conservation Reserve Fund. City Council may use these funds to preserve, maintain, enhance, educate, and purchase the City's historic places. The Kamloops Heritage Commission will develop guidelines on how funds will be allocated. These guidelines will be adopted by City Council.
- 3.3 The City will initiate a Heritage Statue program by commissioning a statue each year for ten years. These statues will commemorate famous Kamloops residents or significant cultural and historical events from the City's history.
- 3.4 The City will continue to recognize historically significant heritage resources that should be preserved for the benefit of future generations through the Heritage Recognition Plaque program.

- 3.5 The City will work with the Kamloops Heritage Commission and other groups to develop a *Heritage Strategic Plan*. The aim of the plan will be to:
- review and update the heritage resources inventory,
 - review the management of heritage resources and provide policy recommendations to the City which will guide the City's approach to heritage preservation, and
 - recommend to the City the best use of the tools provided for by Part 27 of the *Local Government Act*, including the possible creation of heritage conservation areas in the West End, downtown east, and South Shore neighbourhoods.
- 3.6 The City will, wherever possible, preserve heritage buildings in their original location. Where this is not possible and as a last resort, the City will encourage relocation and restoration in an appropriate alternative site.
- 3.7 The City will assist the Kamloops Heritage Commission on collaborating with cultural and heritage organizations in Kamloops to continue fostering open communications.
- 3.8 The City will retain the Tax Incentive Program and consider ways to promote and publicize this program.
- 3.9 The City and Kamloops Heritage Commission will pursue heritage education opportunities for all ages.
- 4.0 The City will support in principle the construction of a heritage railway museum in order to reclaim the cultural railway history of Kamloops and the surrounding region.

4.0 POLICE AND FIRE PROTECTION

GOAL

- To provide and maintain police and fire protection service levels in accordance with the growing and changing needs of the community.

Police and Fire Protection Policies

- 4.1 The City will apply CPTED (Crime Prevention Through Environmental Design) principles where possible through RCMP involvement in development site design.
- 4.2 The City supports the community policing concept and will encourage the continued use and expansion of this program.

- 4.3 Conduct a needs assessment on a regular basis to identify current policing and crime issues and potential strategies to address them.
- 4.4 The City currently operates six fire stations:
- South Shore,
 - North Shore,
 - Valleyview,
 - Westsyde (combination career/on call),
 - Raleigh/Heffley Creek (on call), and
 - Dallas/Barnhartvale (on call).
- 4.5 The City will explore the benefits of adjusting building requirements and development standards with the goal of improving fire protection and lowering response times. This includes:
- monitoring growth in the Southwest Sector and considering construction of a new station in Aberdeen at such time as demand warrants;
 - exploring the option of mandatory sprinkling for all new residential development in outlying, unprotected areas and for all new multiple family and commercial development; and
 - providing emergency access routes in consultation with Fire & Rescue Services for residential developments where access is limited to one location.
- 4.6 The City will provide fire and rescue protection in relation to evolving needs. The *Fire Department Strategic Plan (1994)* will serve as a guide in determining future resource and facility planning.

5.0 SOCIAL PLANNING

GOALS

- To encourage and assist in the development of social action strategies that strengthen and enhance the well-being of our community.
- To recognize the City's limited revenue base when determining an appropriate level of involvement in addressing social issues.
- To recognize that senior levels of government have the primary responsibility for social services planning and programming.

Social Planning Policies

- 5.1 The City will encourage and facilitate the provision of community and social services and facilities through co-operation, liaison and, where appropriate, joint provision and participation. The City will not significantly expand its role in the direct provision of community and social services for the term of this plan.
- 5.2 The City, in co-operation with the Kamloops Social Planning Council, will review and revise the *Social Plan (2009)* to ensure that it continues to address social needs, social well-being, and social development of the community.

The following specific actions are identified in the *Social Plan (2009)*:

Quality of Life

- 5.2.1 The City will ensure that its cultural and recreation programs and facilities are available to meet the needs of as many people as possible.
- 5.2.2 The City will continue to work with community residents and neighbourhood associations to increase the sense of community and co-operation in its neighbourhoods.
- 5.2.3 The City will encourage the identification and implementation of methods to reduce violence in public spaces and its neighbourhoods.
- 5.2.4 The City will continue to manage its community resources (land, water, air, and infrastructure) to reduce the negative impacts of growth and urban sprawl.
- 5.2.5 The City will encourage grass-roots direction to community health and social service providers to ensure that the community's needs are met.
- 5.2.6 Consider the use of City parks, school yards, rights-of-way, surplus properties, and redevelopment parcels at least in part for food production and distribution.
- 5.2.7 Encourage initiatives aimed at local food self-reliance such as community kitchens and community gardens.
- 5.2.8 Promote the City's achievements in areas of food policy and security.



Education

- 5.2.9 The City will, in conjunction with the School District, identify initiatives that encourage youth to stay in school.
- 5.2.10 Continue to promote agriculture in all schools, with emphasis on food production in the local region and school gardens to train children in growing skills and provide food for school meals.

5.2.11 Continue to provide nutrition and food preparation courses, with emphasis on the importance of a sound diet and environmentally friendly growing, including "Food Safe" as elements of school curricula.

Children and Youth

5.2.12 The City will provide locations and activities for children and youth, including:

- provide City facilities where children and youth can meet on an informal basis;
- identify opportunities where children and youth can participate in community functions and organizations;
- continue to review its programming and facilities to establish or increase the number of corporate sponsored youth and teen events;
- T.K.O. (Teens of Kamloops Organization) provides an avenue for youth to express concerns, exchange ideas, and arrive at solutions for youth related issues; and
- coordinate efforts of youth-specific agencies to address the many needs of youth, such as education, employment, housing, health, and life skills.

5.2.14 Encourage the provision of healthy school meals, snacks, and beverages in all School District No. 73 schools.

Employment

5.2.15 The City will, in conjunction with the appropriate senior levels of government, encourage employment training for youth and the unemployed or underemployed.

5.2.16 The City will continue its review of the Zoning Bylaw with the intent of identifying opportunities for inclusionary zoning for daycares and group homes as permitted uses.

5.2.17 Promote the development of new businesses and the expansion of existing businesses that improve access to locally produced, affordable, and nutritious food. This could be done in partnership with business improvement associations, Community Futures, and Venture Kamloops.

Health

5.2.18 The City will liaise with the Interior Health Authority and social service providers to investigate opportunities to improve program coordination and to enable agencies to meet on a regular basis to continue co-operation and collaboration.

5.2.19 Recognize the Kamloops Food Policy Council as the lead agency in the development of food policies.

5.2.20 Kamloops Food Policy Council partner with the Interior Health Authority to encourage residents to undertake food security strategies.

Housing

5.2.21 The City will assist in determining the needs and demands for affordable housing in the community.

5.2.22 The City will continue to encourage affordable housing through its policies and bylaws.

5.2.23 The City will undertake means to increase public awareness for the need to maintain the supply of affordable housing.

5.2.24 The City will continue to encourage a mix of housing types within each neighbourhood to accommodate a broad range of housing needs, incomes, and lifestyles.

5.2.25 The City will continue to communicate to senior levels of government local priorities in relation to the provision of affordable housing and the importance of senior government funding programs to provide such housing.

SECTION V: ECONOMIC DEVELOPMENT

Venture Kamloops is the regional economic development agency which is jointly funded by the City of Kamloops and the Thompson-Nicola Regional District. Its mandate is to promote economic development and tourism in Kamloops and the Thompson-Nicola Region. The agency's vision statement is:

To work co-operatively with all communities and organizations within the Thompson-Nicola Regional District to create a tourism and economic development agency that improves the quality of life for all citizens by linking the region's competitive advantages to its innovative strengths.

In January 2003, Venture Kamloops completed an Industry Sector Opportunity Analysis Strategy which examined seven key industrial sectors (agriculture, mining, forestry, manufacturing, transportation, tourism, and high technology) to determine opportunities for growth and development of these sectors. The report provides an overview of the Kamloops economy in relation to the region and examines the strengths and weaknesses in each of the sectors analyzed. Key recommendations from the report are:

- building a stronger, more diversified economic base;
- supporting existing businesses to ensure their retention and expansion; and
- attracting new businesses by focusing on the strengths and opportunities Kamloops has to offer.

The report will also form the basis of an economic development strategy to be followed by Venture Kamloops in the years ahead.

Kamloops traditionally has been an industrial city with an economy relying on basic sector activities such as forestry, agriculture, mining, transportation, construction, and public administration. These sectors, which are typically high paying and generally stable, are not expected to grow significantly over the next 10 to 15 years. In fact, the resource sector is expected to decrease in relative importance as the Kamloops economy moves from a resource based economy to a service economy.

The projected closure of Highland Valley Copper in 2009 will lead to the loss of 463 mining jobs and between 400 and 570 related jobs in Kamloops. The after tax employment income attributable to these jobs is estimated to be between \$34.8 and \$37.5 million or approximately 2% of total community income.

Kamloops' Major Employers	
School District No. 73	1,700
Royal Inland Hospital	1,400
Convergys	1,000
Highland Valley Copper	950
Weyerhaeuser Canada	950
Thompson Rivers University	900
City of Kamloops	650
Overlander Extended Care Hospital	450
Walmart	300
BC Lottery Corporation	285
Pollard Bank Note	275
McDonald's Restaurants	250
Ponderosa Lodge	250
Save On Foods	230
CP Rail	220
Canada Safeway	220

On the plus side, over the same period (2003 to 2009), the Thompson Rivers University is expected to increase in enrolment from 6,542 full-time enrollment (FTE) students to 9,607 FTE students, an increase of 47%. This will require an increase of approximately 250 academic, administrative, and support staff. Growth at the university is projected to continue until 2030 when there will be a projected enrolment of 16,000 students. This will have significant benefit to the local and regional economies.

Annual Economic Impact of UCC Student Body			
	2003	2008	2013
Canadian Students	\$138 million	\$200 million	\$250 million
International Students	44 million	75 million	94 million
Total	\$182 million	\$275 million	\$344 million
Plus: the \$175 million current operating budget spent in the local economy.			

Kamloops will continue in its role as the regional centre for government and health care. Local area economic dependencies for 1996 indicate that the public sector (health, education, and government) was the single most important base sector, accounting for 26% of all income flowing into the community (*Kamloops Sector Analysis 2003*).

The majority of new jobs being created are in the service producing, non-basic sectors such as retailing, tourism accommodation, food services, and personal services, which typically tend to be lower paying and less stable. Between 1981 and 2001, the goods producing labour force in Kamloops declined from 25% of the labour force to 17%. This is despite the fact that the Kamloops labour force as a whole grew by almost 24% in the same period. The trend is expected to continue toward a greater proportion of jobs found in service rather than basic sectors of the economy, which will result in a slower rate of community income growth.

The two sectors of the economy identified as having the greatest potential for expansion are tourism and value-added wood products (manufacturing). In the tourism sector, Venture Kamloops is working hard at branding Kamloops and region as "BC's Adventure Destination", and the City of Kamloops is building on the success of its Tournament Capital of BC program by pursuing recognition as the "Tournament Capital of Canada". In 2002, 91 tournaments were held on City facilities, 21,625 people visited Kamloops to participate in sporting events, and \$8.4 million was spent in the local economy by participants. The enhanced Tournament Capital program will involve aggressive tournament marketing and recruitment, athlete and coaching development, and development of national calibre tournament facilities on McArthur Island and at Hillside Stadium/Canada Games Pool. New regional tourism opportunities which will benefit Kamloops include such ventures as the Rocky Mountaineer Raitours, Sun Peaks Resort, and the proposed Six Mile development.

In the manufacturing sector, Kamloops manufacturers have traditionally been tied to the resource sector. In recent years, the trend has been toward smaller, more flexible and niche-oriented companies. This is true in Kamloops where the majority of firms are small, with fewer than five employees. The absence of a single dominant company or group of related companies limits the development of an industrial cluster of related businesses. The one exception to this is value-added wood manufacturers. Venture Kamloops is seeking to recruit manufacturers of wood products to develop an economic cluster in Kamloops.

The consensus that has emerged through the planning process recognizes the importance of continued economic development. While Kamloops cannot afford to ignore economic opportunities no matter what sector they may fall under, Venture Kamloops and the City will continue to focus on high employment, higher paying activities and businesses. Major developments should be reviewed on an individual basis to determine what level of support the community would be prepared to provide. The City will continue to encourage development of all kinds, subject to appropriate environmental and community safeguards.

The City has developed the following goal and policy statements in response to the community's economic issues.

1.0 ECONOMIC DEVELOPMENT

GOALS

- To develop a strong, diversified, and sustainable economy that will provide expanded opportunities for employment, support community growth, and foster community pride.
- To encourage economic development while maintaining an equitable balance between business/industry and residential taxpayers.

Economic Development Policies

- 1.1 The City strongly supports and encourages employment generating development, particularly that which will not negatively impact the environment or the community's image.
- 1.2 The City will support Venture Kamloops' efforts to develop an economic development strategy based on the findings of the Industry Sector Opportunity Analysis Strategy and, in particular, to target specific economic sectors and businesses to locate in Kamloops. The economic development strategy should include efforts to:
 - develop a strong and identifiable Kamloops brand,
 - increase tourism and value-added wood manufacturing opportunities,
 - create a supportive business climate, and
 - support cultural diversity and activity.
- 1.3 Aggressively support the Tournament Capital of Canada strategy as an economic development initiative and significantly increase the number of tournaments hosted by the City each year.
- 1.4 The City will support Venture Kamloops' efforts to encourage tourism development, particularly those projects that will reflect the "BC's Adventure Destination" brand or which will assist in extending the length of visits.
- 1.5 The City will continue to pursue the development of a strong, well defined community image, building on such opportunities as:
 - Tournament Capital of Canada program,

- Thompson Rivers University development,
 - the community's vast and well trained volunteer base,
 - regional initiatives such as:
 - ▶ Sun Peaks expansion,
 - ▶ Rocky Mountaineer Railtours,
 - ▶ Six Mile Ranch development, and
 - ▶ Tranquille farm resort development.
- 1.6 Aggressively support the retention of senior government offices in Kamloops to maintain and enhance the City's role as a regional government centre.
- 1.7 The City will support and work with the two Business Improvement Areas (BIAs) and one Business Association as a means of maintaining, upgrading, and revitalizing commercial areas. The two BIAs are the North Shore BIA and the Kamloops Central BIA, and the one Association is the Valleyview and East Business Association. In addition, the City will support the efforts of the Kamloops Chamber of Commerce and Venture Kamloops to promote business in Kamloops.
- 1.8 The City will develop a mechanism to weigh the costs and benefits to the community of major developments.
- 1.9 The City will explore an assist factor for development cost charges on industrial zoned land.
- 1.10 The City will continue to pursue a co-operative relationship with the Kamloops Indian Band to address matters of mutual concern including, but not limited to, economic development. The City supports the Kamloops Indian Band in its endeavour to become economically self-sufficient.
- 1.11 The City, in co-operation with Venture Kamloops and the North Shore BIA, will actively pursue establishment of increased employment generating development on the North Shore, particularly relating to the future development of businesses and residential development along the Tranquille Market Street area, light industrial development at the Kamloops Airport, and the resort development at Tranquille Farm site.
- 1.12 The City will ensure adequate industrial land is available in the City to meet future demand.
- 1.13 The City will use the cost recovery mechanism of DCCs to offset costs arising from new growth and the need to expand City services and infrastructure. Further, the City will review DCCs to determine:
- their impact on development and the financing of services;
 - their competitiveness with other municipalities;
 - their use to further planning objectives and not simply to be used as a revenue generator; and

- the appropriateness of charging DCCs based on location of the proposed development, KAMPLAN policies encouraging infill, intensification, and redevelopment, the impact of development on existing neighbourhoods, the value of development, and cost-effectiveness of extension of services.

- 1.14 The City will provide municipal services in a cost-effective and efficient manner in order to minimize taxes and other costs.
- 1.15 The City will maintain a positive, pro-development attitude and will ensure that its regulations, policies, and procedures encourage community growth and development.
- 1.16 The City will encourage the development of a performing arts facility. The City will support the project with consideration being given to location, transportation access, partnership, services in the area, residential impact, and parking options.
- 1.17 The City will encourage the use of innovative approaches to facilitate and assist community development, including, but not limited to:
- use of municipal real estate assets,
 - senior government programs/incentives, and
 - joint ventures/partnerships.
- 1.18 Review the potential to develop a convention centre within the term of this plan.
- 1.19 The City will work co-operatively with businesses in Kamloops and those considering locating in Kamloops. We will maintain a streamlined operation and provide client services to guide businesses.
- 1.20 The City will consider developing partnerships with businesses that are actively and aggressively seeking new or expanded operations in Kamloops.
- 1.21 The City will consider strategies to specifically attract and nurture new significant economic investments in Kamloops.
- 1.22 The City is strategizing on ways to market the community globally so that when businesses are looking at communities, Kamloops is considered and chosen.
- 1.23 The City will examine global tourism opportunities and ways to market our qualities to new and growing tourism-seeking countries.
- 1.24 The City will encourage historic, cultural, and First Nations tourism ventures to provide more activities and educational opportunities for tourists who come to Kamloops.
- 1.25 The City will support in principle the construction of a heritage railway museum in Kamloops as a tourism and economic development initiative.
- (5-1-2399) 1.26 The City of Kamloops shall enact a tax exemption bylaw consistent with the following rationale and objectives:

The City of Kamloops recognizes that promoting revitalization in specified areas of the City is an essential component to the overall economic prosperity of Kamloops. The objectives of this designation are:

- To strengthen the viability of existing businesses;
- To stimulate and encourage new private investment;
- To encourage higher density residential development; and
- To improve the image and attractiveness of the tax exemption areas.

Additional objectives for the creation of a North Shore Revitalization Tax Exemption Program include:

- Encouraging sustainable development and green building techniques throughout the whole of the North Shore;
- Encouraging mixed-use development in core areas of the North Shore defined by Development Permit areas;
- Encouraging construction of non-market housing units; and
- Encouraging development throughout the whole of the North Shore.

2.0 AIRPORT

- 2.1 The Kamloops Airport has extensive land holdings which may be developed for ancillary and complementary commercial and industrial uses in accordance with the *Airport Area Land Use and Development Plan (2000)*.
- 2.2 The City will actively pursue the development/redevelopment of the Airport Lands with the intent of providing additional employment generating activities for the North Shore.
- 2.3 The City will actively pursue increased airline service to Kamloops.
- 2.4 The City will promote the importance of the airport to Kamloops' future economic development.
- 2.5 The City will ensure the services offered by the airport meet the needs of existing business and residents and are appropriate to the needs of new businesses considering locating in the City.
- 2.6 The City will review its Development Cost Charges Bylaw to examine if the lands around the airport should receive an assist factor for DCCs.
- 2.7 The City will be making application to the Agricultural Land Commission (ALC) to exclude 21.37 ha of land to permit the extension of the runway. This is in the City's and region's interest so as to open the opportunity for jet service.

3.0 THOMPSON RIVERS UNIVERSITY

3.1 Thompson Rivers University is expanding as it continues to serve as a four year degree granting institution. Land has been designated, as shown on Map 1, to accommodate its expansion. The City will encourage and facilitate growth of the university to 16,000 students in accordance with the *UCC Campus Master Plan (2003)* and will continue to pursue opportunities for joint development and operation of community facilities.

3.1.1 The City will encourage a variety of affordable student housing for Thompson Rivers University students, both on campus and in the "Main Street" section of the McGill corridor.

SECTION VI: MUNICIPAL INFRASTRUCTURE

Kamloops residents enjoy a high level of municipal services, which contribute significantly to our quality of life. To a large extent, this is a result of the planning and infrastructure development carried out in the 1970s and early 1980s, followed by extension of services to Barnhartvale and Westsyde in the early 1990s. In the spring of 2003, construction began on the Water Treatment Plant on River Street, which will use membrane filtration technology, making it one of the most advanced treatment facilities in North America.

Funding constraints and environmental stewardship principles are placing greater emphasis on cost-effective delivery of services and increased conservation measures. Increased levels of regulation by senior levels of government raise concerns about environmental liability.

1.0 GENERAL SERVICING STRATEGY

GOAL

- To provide a high level of services in a cost-effective and efficient manner, balancing demands with affordability.

General Servicing Policies

- 1.1 The City will favour infill over peripheral development and will pursue strategies designed to encourage maximum utilization of existing service systems prior to expansion.
- 1.2 The City will encourage the development of a more compact and efficient land use and servicing system emphasizing intensification and higher densities, energy conservation, environmental sustainability, and fiscal responsibility.
- 1.3 The City will continue to emphasize conservation and the reduction of waste through educational and promotional campaigns, such as WaterSmart, and through the provision of facilities such as the McGill Road yard waste and Cinnamon Ridge composting site and the recycling depots throughout the City.
- 1.4 The City will continue to develop demand management strategies to defer or reduce the costs of infrastructure growth.
- 1.5 The City will use development cost charges to offset costs arising from new growth and the need to expand City services and infrastructure. Further, the City will review development cost charges to determine:
 - their impact on development and the financing of services;
 - their competitiveness with other municipalities;
 - their ability to further planning objectives; and
 - the appropriateness of charging DCCs based on:
 - ▶ the impact of development,

- ▶ the size/value of development, and
 - ▶ community/public facilities vs. private/commercial.
- 1.6 The City will continue to pursue innovative methods of providing infrastructure including the use of new technologies and alternative financing approaches (e.g. public/private partnerships). User fees, service charges, and developer contributions (development cost charges, works and services provisions, latecomer agreements) will be considered viable means of assisting with the financing of future improvements.
- 1.7 The City will maintain existing service levels and improve where feasible, using formal infrastructure planning and the five-year capital budget as priority setting/evaluation tools.
- 1.8 The City's amalgamation in 1973 has resulted in different neighbourhoods in the City having different infrastructure standards and service levels for services such as street lights, sidewalks, drainage, and parks. The City recognizes that neighbourhoods will continue to have different standards and levels of service, although retrofitting will be considered on a limited basis for high priority areas through initiatives such as Safer City or in areas experiencing significant redevelopment through infill and intensification. New development will be required to meet current standards.
- 1.9 Upgrading of local infrastructure beyond current service levels will generally proceed through use of local improvement provisions where the primary benefiting parties are the landowners who live adjacent to the local improvement. The cost sharing formula between benefiting landowners and the City will be determined by City Council and established in the local improvement bylaw from time to time. Upgrades resulting from health, safety, or emergency concerns may require different cost sharing formulae. The City will continue to pursue cost sharing with the senior governments, recognizing that grant reductions will continue.
- 1.10 New developments will be required at the time of subdivision approval or Building Permit issuance to provide works and services in accordance with the following general standards:
- 1.10.1 Urban Areas (includes the principal residential areas of Kamloops)
- Full urban services including:
- sanitary sewers,
 - water mains,
 - storm sewers,
 - underground utilities (discretionary in small-scale infill situations where general area is overhead wiring),
 - paved streets with curb, gutter, sidewalks, and walkways, and
 - street lighting.

1.10.2 Suburban Areas (includes Rayleigh, Heffley Creek, Rose Hill, Viking Estates, Dallas, and central Barnhartvale)

Full urban services as above, except that where the local standard of servicing does not include urban infrastructure, the City may exercise discretion. For example, in areas where no communal sanitary sewer exists, septic tank disposal of effluent would be permitted subject to health approval.

1.10.3 Rural Areas (includes Knutsford, Iron Mask, south and east Barnhartvale, Karindale, and Noble Creek)

Rural services including:

- septic tank disposal,
- water mains; wells on large lots (2 ha or greater),
- open ditching,
- overhead wiring, and
- paved streets.

Specific standards and guidelines are contained in the Subdivision Control Bylaw and the City's Design Manual.

2.0 TRANSPORTATION

Kamloops is located at the intersection of three major highways: the Trans Canada, the Coquihalla, and the Yellowhead. In 1962, the Trans Canada Highway was opened over Roger's Pass, linking Kamloops to the rest of Canada. At that time, the highway passed through Kamloops on what is now Columbia Street. In 1973, the bypass was built, providing the opportunity to increase highway, commercial, and tourist accommodation development in Valleyview and the Southwest Sectors. A further bypass is currently under consideration that would bypass the Valleyview neighbourhood, leaving the existing highway in the vicinity of Holman Road and climbing onto the bench to the south of Valleyview, connecting back into the current alignment of the highway just east of Peterson Creek.

The Yellowhead route, including the Yellowhead Bridge over the South Thompson River, was built in 1967 connecting Kamloops to Edmonton along the North Thompson River valley.

The City is bisected by the Thompson River system, and the creation of a viable road network is constrained by the topographic constraints of a valley location. Despite these constraints, residents of Kamloops enjoy a high level of mobility and short travel times to destinations throughout the City.

The City of Kamloops integrates its land use and transportation planning functions through *TravelSmart (1999)*. This has been supplemented in recent years by work under the Safer City Program including the *Pedestrian Master Plan (2002)* and the *Bicycle Master Plan (2002)*. The City of Kamloops in collaboration with the Insurance Corporation of BC (ICBC), initiated the Kamloops Safer City program in 2001. The goal of this program is to significantly improve road safety in Kamloops through integrated transportation, engineering, traffic enforcement, and education programs. The primary partners in Kamloops Safer City Program are the City's Development and Engineering Services Department, ICBC, and the RCMP. Kamloops' experience is serving as a model for a number of other communities in BC that have commenced Safer City initiatives.

As a backdrop to this pioneering effort, it is valuable to understand the importance of road safety in our community. Over the four-year period (1999-2002), there was an average of \$18 million in insurance claims per year relating to vehicle accidents in Kamloops. Four fatalities occurred, over 700 injuries, and a total of 3,017 crashes.

The work of the City of Kamloops, ICBC, RCMP, and other stakeholders has been summarized in the document entitled *Kamloops Safer City Plan (2003)*. This plan sets out the objectives of the Safer City Program, reviews the work undertaken to date, and provides an action plan to support the ongoing implementation of Safer City initiatives.

- 2.1 The City's major road network (Ministry of Highways grid road plan) is identified on Map 5, including existing and future arterial routes.
- 2.2 The City's Integrated Land Use and Transportation Plan *TravelSmart (1999)* provides detailed guidance for future development of the City's street network and transportation system. The City's transportation goals are as follows:
 - maintain mobility levels as Kamloops grows by means of a transportation system which is effective, yet affordable;
 - integrate land use planning with transportation planning by managing future development patterns in a manner which minimizes the rate of increase in travel demand;
 - develop a transportation system and pattern of community development which are amenable to alternatives to provide choices as to where people can live and their mode of travel, including the private automobile, public transit, cycling, and walking;
 - strive for further reductions in the rate of increase in travel demand through implementation of other feasible travel demand management techniques tailored to Kamloops;
 - protect the integrity of the provincial highway corridors within Kamloops to facilitate through traffic;

- recognize the linkage between the goal of environmental sustainability and an integrated transportation and land use planning system aimed at reducing the rate of increase in travel demand; and
- ensure compatibility of transportation corridors and facilities with adjacent land uses and the overall character and image of the community.

2.3 The City's Road Network Classification system is based on the following hierarchy:

- provincial highways,
- arterials,
- collectors, and
- local streets.

The provincial highway network is intended to carry provincial traffic and traffic through urban centres. Traffic that originates in Kamloops and is destined to locations within Kamloops is intended to use the City's arterial road network, shown on the road network plan (Map 5).

2.3.1 Arterial routes move traffic from one part of the City to another. Collector routes link local streets with arterials and provide access to abutting properties. Local streets provide access to abutting properties. In planning for new developments or redevelopment, the City will encourage the following:

- single family residential units should not have direct access to arterials, and access to collectors should be discouraged;
- through traffic should be discouraged from using local streets; and
- truck routes and industrial traffic should be restricted to designated arterials and appropriate industrial collectors.

2.3.2 The City will develop an Access Management Plan within the term of this plan.

2.3.3 The provincial Agricultural Land Commission may not have agreed to all alignments shown within the Agricultural Land Reserve (ALR). The City will work with the Commission to fine-tune alignments and the ALR boundary and attempt to resolve areas of conflict. Any road extending into the ALR requires approval of the Agricultural Land Commission.

2.4 No additional river crossings are anticipated within the term of this plan, although upgrading and rehabilitation of existing bridges may be required from time to time.

- 2.4.1 While the Red Bridge is not within the City's control, the City recognizes the importance of a river crossing in this general vicinity and supports its continued rehabilitation or potential replacement. The preferred alignment for a replacement to the Red Bridge would connect the KIB to the City's road network at Lorne Street. It may be desirable to retain the Red Bridge as a pedestrian/bicycle link at such time as it is decommissioned as a vehicle bridge.
- 2.4.2 Alignments to connect Lorne Street to the South Shore/City Centre area will be reviewed in the future as the need arises.
- 2.4.3 Beyond the 120,000 population horizon, the City will continue to protect the Singh Bridge corridor from the Summit extension to Ord Road.
- 2.5 At the time of rezoning, subdivision, or development, the City shall ensure that alignments for all necessary streets are acquired (arterial, collectors, and local) in accordance with the terms and conditions of the *Local Government Act* and the *Land Title Act*.
 - 2.5.1 The road network in new plans of subdivision should reflect the overall transportation plan for the community and, where feasible, enable properties to be within 300 m walking distance of a transit stop.
- 2.6 The City will require a transportation impact study to be prepared in support of applications for significant development. Listed below are examples of issues to include in a transportation impact study:
 - 2.6.1 A multi-modal approach to provision of transportation infrastructure in new developments to achieve a reasonable balance between auto, transit, and non-motorized modes of travel.
 - 2.6.2 Impacts of development traffic on the capacity of adjacent, and nearby, transportation corridors accounting for the anticipated growth in levels of background traffic.
 - 2.6.3 Identifying the need for modifications to the transportation infrastructure to support and/or accommodate new development including modifications to the transit network and facilities for non-motorized modes of travel.
 - 2.6.4 Consideration of the potential for Travel Demand Management (TDM) measures in new developments.
 - 2.6.5 Take into account both the influence of anticipated future development in the surrounding area, as well as any planned infrastructure modifications.
- 2.7 A number of arterial road projects are required to provide access to lands zoned for residential development or identified as Special Development Areas within the term of this plan. The timing of these projects is dependent upon the rate of development activity. The projects include:
 - Grasslands Boulevard extension to McQueen Drive,

- Highland Drive access/capacity enhancements,
- Qu'Appelle Boulevard to Rose Hill Road,
- Valleyview Drive extension to Jimeva Farms,
- Copperhead Drive extensions to Aberdeen Drive and Cannel Drive,
- Pacific Way upgrading,
- Aberdeen Drive upgrading and/or extension, and
- Hugh Allan Drive extension to Pineview Valley.

2.8 The City will require developers within Special Development Areas to demonstrate cost-effectiveness prior to approving any extensions to the road network or upgrading which may be required to accommodate such development. Developers will be required to make road improvements necessary to accommodate their developments at their cost.

2.9 Prior to the City's population reaching the 100,000 horizon in the year 2021, the following arterial road projects are considered necessary:

- Columbia Street road widening between 3rd Avenue and 6th Avenue (estimated cost - \$2 million); and
- Hillside Drive extension, west of Home Depot to Summit Drive (estimated cost - \$8.5 million).

In addition, the Ministry of Transportation has identified the need to construct an uphill climbing lane on the Trans Canada Highway between the Valleyview interchange and the Peterson Creek Bridge by the year 2021; however, financial and government priorities will determine rehabilitation schedules.

2.10 Prior to the City's population reaching the 120,000 population horizon, the construction of the 6th Avenue extension from Columbia Street to Summit Drive at Springhill Drive (estimated cost - \$20 million) will be required.

In addition, the Ministry of Transportation has identified the need to construct a Valleyview bypass on the benchlands between the Valleyview and Juniper neighbourhoods within this time frame, as shown on the Major Road Network Plan (Map 5). The City will continue to work with the Ministry to protect the alignment of this bypass. Biological inventories for ecosystems should be conducted and recommendations provided to reduce environmental impacts.

2.11 Beyond the 120,000 population horizon, the City will continue to protect the Halston Avenue corridor to allow for potential four laning between 8th Street and Kamloops Airport.

2.12 The City will continue to fund its rehabilitation program for arterials, collectors, and local streets primarily from general revenue (or reserves), with priorities allocated each year in accordance with the ongoing municipal pavement management analyses.

- 2.13 The City continue to address the need to have sidewalks and open space areas accessible through such features as gradual grade transitions and depressed curbs at street corners.
- 2.14 The City will continue to explore travel demand management programs and to encourage the increased use of transit as an alternative to the automobile and to increase mobility and access to community services for those unable to use an automobile, within funding constraints. The *Official Transit Plan 00-2020 (2000)* approved in principle by City Council and incorporated as part of TravelSmart, outlines the specific goals and service objectives of the Kamloops Transit system.
- 2.14.1 The City will continue to work with Thompson Rivers University on the implementation of a U-Pass system, under which students will receive a transit pass as part of their student fees.
- 2.14.2 The City will work with major employers, such as Royal Inland Hospital or Weyerhaeuser, to explore the implementation of trip reduction programs such as the Pro-Pass transit program.
- 2.15 The City will pursue the development of a transit system based on three major transfer stations:
- the City Centre,
 - Tranquille Market Street, and
 - Thompson Rivers University.
- As development of the Southeast Sector comes on stream, consideration will be given to a transit exchange in Valleyview.
- 2.16 The City will pursue the development of a continuous comprehensive pedestrian system in accordance with the *Pedestrian Master Plan (2002)*, which was adopted in principle by City Council in 2002. The plan was developed in association with ICBC as part of the Safer City initiative and has a major emphasis on promoting pedestrian safety. Promoting walking as a viable mode of transportation, as an alternative to the automobile, will have significant benefits for the health and well-being of Kamloops residents.
- 2.16.1 The City will continue to ensure that walkways and pedestrian linkages are provided in all new developments, particularly for sites where topography is a factor or for major destination points, and these shall be provided by the developer at the time of subdivision.
- 2.16.2 The City will refer development proposals to the Ministry of Transportation Thompson Nicola Transportation District when intersection conflicts and potential safety issues for the travelling public exist.
- 2.17 The City will continue to pursue the development of a continuous, integrated bicycle network in accordance with the *Bicycle Master Plan (2002)* in order to promote and encourage bicycling as a commuting alternative to the automobile and as a means of active recreation.

2.18 The City will encourage road safety through a broad range of transportation infrastructure improvement, education, enforcement, and other initiatives designed to minimize the likelihood of unsafe situations arising between users of the transportation network. These initiatives are detailed in a report entitled *Kamloops Safer City Plan (2003)*. The recognition of interrelationships is fundamental to achieving road safety including the value of:

- links between road safety and the physical attributes of the transportation networks such as road form, access management, intersections, network planning and function, and traffic calming;
- travel demand management measures which reduce automobile travel and therefore the risk of collisions through the provision of safe pedestrian, bicycle, transit, and other facilities;
- connections between land use planning and road safety through the role of the community's land use pattern in reducing travel demands and maximizing safe travel routes; and
- partnerships involving all stakeholders.

2.19 The City will consider utilizing alternative street standards in new development areas, in conjunction with an overall development plan, in order to encourage a reduction in the impact of automobile traffic on neighbourhood liveability. In existing developed areas, traffic calming measures may be implemented.

(5-1-2501)

2.20 The City will utilize performance-based zoning regulations to support automatic minimum parking stall requirement reductions when a development supports access to alternative forms of transportation, provides building design that takes into account pedestrian-focused needs over the automobile, converts existing heritage structures into multiple family dwellings, and/or constructs affordable rental housing units.

The City will support the use of a cash-in-lieu payment (\$2,000 per surface parking stall, \$15,000 per above-ground parkade stall, and \$25,000 per underground parkade stall) for every approved parking stall reduced that was not already reduced through the performance-based measures and that is used for the Public Transportation and Pedestrian Upgrade Reserve Fund.

3.0 WATER SYSTEM

In April 2003, the City began construction of a membrane filtration water treatment plant adjacent to the main water intake on River Street. The plant will be operational by the end of 2004 and will initially serve areas currently serviced by the central water system. Extension of treated water to the Southeast Sector and the Noble Creek area is scheduled for 2005 through the construction of trunk mains. The trunk main to the Southeast is expected to cost \$3.6 million and will be sized to provide water to development lands identified as Special Development Area (Jimeva Farms). The extension to Noble Creek will be restricted to serving existing properties with domestic and fire flow only.

- 3.1 Water conservation shall be encouraged through continuing the WaterSmart Program and watering restrictions. Usage shall be monitored, and if necessary, the City may consider implementing additional measures to reduce consumption, including water meters and enforcement. The primary purpose of the WaterSmart Program is to increase public awareness of the benefits of reducing water consumption and the associated potential to defer capital costs of major supply improvements.
- 3.2 The City will continue to show leadership in water conservation initiatives affecting City operation such as computerized irrigation systems tied to soil moisture and evaporation rates. The City will encourage other government agencies in the private sector to adopt similar water conservation practices.
- 3.3 For the term of this plan, the City's water system will not be extended beyond:
 - the municipal boundary (with the possible exception of the KIB lands for emergency purposes only);
 - existing service limits; or
 - the designated Special Development Areas identified on Map 1,in order to support redevelopment of existing **Urban** lands, intensification, infill development and discouraging peripheral expansion.
- 3.4 Existing private water systems in Rayleigh and Heffley Creek shall be retained for the term of this plan. The City will not permit the establishment of new private water systems within the City.
- 3.5 The City will require developers within Special Development Areas to demonstrate cost-effectiveness prior to approving any water extensions or system upgrading which may be required to accommodate such development. The cost of such extension or upgrading shall be borne by the developer.
- 3.6 The City will continue to work co-operatively with senior government and other agencies to monitor and improve conditions in the South Thompson River watershed. Watershed management is important for two reasons: it is environmentally responsible; and the cleaner the water entering the water treatment plant, the less it will cost for treatment and ongoing maintenance.
- 3.7 The City will implement an Asset Management system to identify the age and condition of all water mains throughout the City. This will assist in identifying priorities for upgrading and maintenance of the water system.
- 3.8 The City is continuing to pursue the construction of an emergency water supply. The preferred option is currently under review.

4.0 SANITARY SEWER

Wastewater from the majority of neighbourhoods in the City (from all areas designated urban residential and the suburban areas of Dallas and central Barnhartvale) is collected and treated at the central treatment plant located on Mission Flats Road. After treatment, wastewater is either discharged to the river (60%), discharged in rapid infiltration beds (20%), or used as spray irrigation on the Cinnamon Ridge site (20%). The amount of effluent that can be discharged by spray irrigation increases above 20% during hot, dry summers.

The Rayleigh neighbourhood uses a combination of communal septic fields and individual septic tanks. Elsewhere in the City, individual septic tanks are the primary means of disposing of wastewater. It is anticipated that the sewer collection system will be extended to the Rose Hill and Karindale neighbourhoods within the term of this plan. In the longer term, Rayleigh may also be connected to the system.

Since 1999, the City has been involved in a comprehensive review of its wastewater treatment facilities. The Liquid Waste Management Plan (LWMP) examined alternate methods to reduce ammonia and phosphorus being discharged as effluent through biological nutrient removal (BNR) or partial BNR (with the potential for upgrading to full BNR if required), and a variety of discharge options including spray irrigation. The study also examined the impact of these treatment methods on the Thompson River system. Both senior levels of government have agreed to the proposed levels of discharge from a partial BNR plant subject to a monitoring program and upgrades, if required. The proposed partial BNR plant still requires formal Council and provincial government approval. The plant is estimated to cost between \$25 and \$30 million, which is less than half the cost of a full BNR plant. Construction is scheduled to begin in 2006, with the plant being fully operational by 2007.

The City is also constructing a sludge dewatering plant. Sludge from Mission Flats will be dewatered, then trucked to Cinnamon Ridge to be co-composted with yard waste. The resulting compost can be applied to land as a nutrient supplement and may be available to the public. A co-composting pilot was undertaken in 2003 and is expected to be fully operational in 2004. Cost of the dewatering facility is \$3.6 million.

- 4.1 The City plans to continue use of the central collection and disposal system as its primary means of sewage disposal. The existing system is estimated to support a city-wide population of 100,000 with significant upgrading and expansion of the effluent disposal system in accordance with the recommendations of the Liquid Waste Management Plan.
 - 4.1.1 Construct a partial BNR plant (estimated cost \$25 to \$30 million) in accordance with the Liquid Waste Management Plan.
- 4.2 The City shall continue to pursue innovative ways of treating and disposing of effluent including, but not limited to, biological nutrient removal, spray irrigation, rapid infiltration, and wetlands development.

- 4.3 Suburban and rural areas (except Dallas and central Barnhartvale) shall continue to use septic tanks as the primary means of disposing of sewage effluent. Holding tanks will be prohibited as a permanent long-term solution. Development will be prohibited where topography or soil conditions are not suitable for ground disposal.
- 4.4 Hazardous wastes or contaminants shall not be discharged to ground disposal.
- 4.5 The City's central system will not be extended beyond current limits and those Special Development Areas designated for future development on Map 1 within the term of this plan.
- 4.5.1 Notwithstanding Policy 4.5 above, the City will co-operate with the KIB to provide sanitary sewer servicing for development lands within the Reserve. An agreement has been approved by both Councils that provides for sanitary sewer collection and disposal for the KIB lands. The agreement provides for the acceptance, treatment, and disposal of effluent from serviced lands up to but not exceeding 6,000 dwelling units or equivalent.
- 4.6 The City will require developers within Special Development Areas to demonstrate cost-effectiveness prior to approving any extensions to the sanitary sewer system or upgrading which may be required to accommodate such development. Applicants will be required to make sanitary sewer system improvements necessary to accommodate their developments at their cost.
- 4.7 The City will implement an Asset Management system to identify the age and condition of the City's sanitary sewer system to identify priorities for upgrading and improving the system.

5.0 STORM DRAINAGE

In the City of Kamloops, storm water management takes an integrated approach to the planning, design, implementation, and operation of the storm water drainage infrastructure which seeks to balance drainage efficiency and functional operation while striving to maintain environmental values. The City's storm water management objectives include:

- the protection of life and property;
- conservation of natural, human, and financial resources;
- provision of infrastructure and services to benefit the community;
- sustainable development by encompassing an integrated approach to storm water management;
- orderly development and integration of land uses; and
- preservation and improvement of natural environmental features.

Significant storm water improvement projects within the last several years include the creation or enhancement of storm water detention ponds on Ord Road at Singh Street and Ord Road west, Batchelor Hills with two ponds in the Lac du Bois area, Mt. Dufferin pond, Gamble pond, and Makao Lake above Aberdeen. Channel stabilization has been undertaken on Guerin Creek and Peterson Creek, and overland flood routing was developed in some areas of Lower Sahali. In Valleyview, the Oriole Road storm water pump station was upgraded, and in Westsyde, drainage improvements were constructed at Anderson Terrace and along Westsyde Road.

- 5.1 The City shall continue to use the existing natural drainage pattern as the primary storm drainage system. Natural watercourses shall be protected from encroaching development and enhanced and improved where necessary. The City will continue to use storm water detention/retention facilities as its principal means of meeting the objective of maintaining postdevelopment flows at predevelopment levels. In addition, overland flood routing to major drainage features will be incorporated into new development.
- 5.2 The City will revise the Master Drainage Plan on a regular basis and shall establish an appropriate level of protection for each drainage basin, taking into consideration the cost of improvements, potential for loss of life and personal property damage, and the management of the municipality's risk and liability.
- 5.3 Funding for major storm drainage components shall continue to be provided from general revenue and from DCC Reserve Funds. Local benefit projects (minor improvements) will be financed by way of local improvement/specified area and will be carried out where possible in conjunction with street upgrading projects.
- 5.4 The City will require developers within Special Development Areas to demonstrate cost-effectiveness prior to approving any extensions to the storm drainage system or upgrading which may be required to accommodate such development. Developers will be required to make drainage improvements necessary to accommodate their developments at their cost.
- 5.5 The City, in conjunction with senior levels of government, shall work towards implementing surface runoff management methods that effectively address water quality issues prior to discharge into the receiving waters. The City may require new development to undertake on-site siltation control measures where runoff could enter the stormwater system or could damage nearby ecosystems.
- 5.6 The City shall require that the appropriate provincial and federal ministries review new development proposals that are affected by the existence of natural watercourses. The Ministry of Water, Land and Air Protection is in the process of developing best management practices. The Ministry will be shifting from reviewing development plans to measuring the success of the best management practice applications and compliance with existing regulations.

6.0 SOLID WASTE

Sanitary landfill sites are shown on Map 1, Generalized Land Use 2004-2036, including the Cinnamon Ridge composting facility.

6.1 The City will continue to work with the TNRD to implement and update the Regional Solid Waste Management Plan in partnership with the Regional Solid Waste Monitoring Committee.

6.2 The City shall continue to pursue the goal of a 50% reduction in the amount of solid waste going to landfills, in accordance with provincial guidelines and the Regional Waste Management Plan. The City will continue to use a variety of approaches including:

- public education, promotion, advertising;
- encouraging recycling through the operation of recycling depots;
- encouraging home composting;
- encouraging the use of the Cinnamon Ridge composting facility as well as the McGill Road and Barnhartvale yard waste drop-off sites;
- restricting curbside pickup; and
- user fees/rates.

6.3 The City will continue to use Mission Flats as the primary landfill site.

7.0 ENVIRONMENTAL MANAGEMENT SYSTEM

In 2001, the City undertook a comprehensive review of the *State of the Environment Report (1996)* to update the report and to recommend appropriate amendments, including the need for key environmental indicators that could be used to monitor future progress. The review also looked at the structure and organization of the Task Force on the Environment to clarify its roles and responsibilities. The result was a complete overhaul of the City's environmental management and monitoring system.

The new Environmental Management System (EMS) is based on the need to develop an "achievable and understandable long-term environmental strategy using a consultative, balanced, and integrated approach". The EMS recognizes the need to coordinate environmental services to reduce the environmental impacts of City operations; to better monitor and evaluate the long-term economic, environmental, and social costs of City actions; and to accelerate the adoption of more sustainable practices by Kamloops businesses, institutions, households, and residents. Unlike the previous *State of the Environment Report*, the new EMS focuses on City operations and what the City can achieve within its sphere of operations. It also emphasizes the need for ongoing monitoring and improvement to the EMS program.

The EMS will be monitored by two committees: an internal Environmental Strategy Team and an external Environmental Performance Advisory Committee. The Environmental Strategy Team will facilitate interdepartmental coordination and integrated problem solving based on sustainability objectives and will prepare an annual Environmental Strategy Report, an annual Environmental Initiatives Report, and an annual Environmental Indicators Report, all of which will be reviewed by the external committee. The external committee will also be responsible for an annual Environmental Performance Audit. The EMS structure and process were endorsed by City Council in 2002.

The EMS will begin as a pilot program in the solid waste management function and then be expanded to other City operations.

The City is already involved in a number of environmental initiatives as shown on the following table.

ENVIRONMENTAL INITIATIVES	
<p>CLIMATE, ENERGY, AND AIR QUALITY</p> <ul style="list-style-type: none"> • Community Energy Plan • TravelSmart Program • Alternative Fuels • Building Retrofit Program • Backyard Burning Ban • Commuter Challenge • Partners for Climate Protection 	<p>WATER CONSERVATION</p> <ul style="list-style-type: none"> • WaterSmart Public Education Program • Xeriscape Demonstration Garden • Water Use Efficiency Committee • Watering Restrictions • Computerized Irrigation in City Parks • Parks Naturalization Program <p>POLLUTION PREVENTION AND WATERSHED</p> <ul style="list-style-type: none"> • Environmental Management System • Non-point Source Pollution Public Education Program • Environmental Protection Handbook and Training Program • Habitat Conservation Stewardship Program • Interagency Team on Environmental Protection
<p>SOLID WASTE</p> <ul style="list-style-type: none"> • Regional Solid Waste Management Plan • Internal Annual Audit of Waste Management Activities • Internal Three R's Program • Drop Off Recycling Depots 	

ENVIRONMENTAL INITIATIVES	
<ul style="list-style-type: none"> • Mission Flats Recycling including Drop 'n Shop • Consumer Product Stewardship Program • Garbage Collection and Disposal • Cinnamon Ridge Compost Facility • Yard Waste Drop-Off sites • Christmas Tree Recycling • Grass Cycling 	<ul style="list-style-type: none"> • Participation in Fraser Basin Council, Watershed Committees, and Bear Aware • Community Greenways Pilot Program
	PARKS
	<ul style="list-style-type: none"> • Integrated Pest Management Plan • Kenna Cartwright Restoration and Education Program • Peterson Creek Management Plan • Communities in Bloom • Urban Forestry Program • Adopt-A-Road
LIQUID WASTE	DEVELOPMENT SERVICES
<ul style="list-style-type: none"> • Liquid Waste Management Plan • Wastewater Treatment, Monitoring, and Upgrades • Reuse of Effluent (Spray Irrigation) and Biosolids • Leachate Monitoring • Storm Water Management and Monitoring 	<ul style="list-style-type: none"> • South Thompson Settlement Strategy
DRINKING WATER	
<ul style="list-style-type: none"> • Water Treatment Plant Construction including LEED Certification • Drinking Water Quality Monitoring Program 	

- 7.1 The City is committed to becoming an environmentally sustainable community by:
- working in a manner that encourages environmental sustainability through leadership, stakeholder and City consultation, and integration with programs and initiatives;
 - meeting or exceeding environmental regulatory requirements;
 - preventing pollution through planning, pre-design, life cycle analysis, green procurement, and related activities;
 - ensuring co-operation, communication, and education among City staff, Council, and key stakeholders to inform environmental management objectives;
 - having financial accountability for responsible environmental management;
 - encouraging continual improvement through taking action to correct or improve environmental conditions; and
 - implementing and continually improving the Environmental Management System for operational effectiveness to fulfill the City's environmental objectives.

SECTION VII: ENVIRONMENT

The City of Kamloops has a total land area of approximately 29,000 ha. Less than 20% of that space (5,500 ha) is considered to be urban. Despite the large amounts of undeveloped area, concern about the environment has increased in proportion to population growth and as more is known about the importance of environmental protection. Previous environmental status reports and input into the KAMPLAN process highlighted the need to balance continued urban growth with environmental sustainability.

Through Municipal Attitude Surveys conducted between 1988 and 2003, at the KamTalk: Vision 2020 Community Workshop held in November 2001, and at the OCP open houses held during the spring of 2002, residents of Kamloops identified a number of environmental issues including:

- promote/preserve major waterways and watersheds,
- promote the compact community concept,
- streamside protection,
- take a proactive role in acquiring riverfront lands,
- focus on natural open space/passive areas,
- protect the grasslands,
- extension of trails systems (pedestrian and bicycle) and the promotion of alternate modes of transportation, and
- establish/continue waste management education, techniques, and programs.

One aspect of the environment that received particular attention was the river and streamside protection. The City of Kamloops is situated at the confluence of the North and South Thompson Rivers. People are drawn to the water's edge, and the City has provided many opportunities to enjoy the river. The Rivers Trail provides an alternative transportation corridor along the river. The City also has the responsibility to protect people, infrastructure, and property from flooding and is required by senior levels of government to conserve riparian habitat.

Since there are many opportunities, requirements, and regulations impacting the river, it is crucial that a well thought out and coordinated effort be implemented under a Greenways Plan. Issues that need to be considered through the Greenways planning exercise include:

- formal developed parklands,
- City-owned and private property,
- fisheries habitat enhancement and protection,
- access trails for recreation and alternative transportation,
- City infrastructure and private utility protection and maintenance,

- flood protection,
- riverbank stabilization,
- railway corridor protection,
- storm water discharge, and
- emergency preparedness and response.

Each of these issues has a specific focus, and at times, there can also be conflict between priorities. It is therefore imperative that a multi-faceted plan be developed so that these issues may be dealt with in a comprehensive manner.

It is also important that there be an overall riverbank plan throughout the City to ensure that an integrated program is established that is not continually constrained by one priority over another. Ideally, the riverbank would be modified or enhanced through different treatments, depending on the major influencing factors. However, over the course of time, the overall plan would achieve improved habitat values over existing conditions while providing the necessary amenities and protective structures to maintain community health and safety.

Another area of concern is air quality. The City of Kamloops is one of 110 communities participating in the national Partners for Climate Protection (PCP) program. PCP is a group of municipal and regional governments across Canada working together to reduce locally produced greenhouse gas emissions in an effort to meet Canada's commitments under the Kyoto accord. The primary objective of the program is to encourage municipalities and their citizens to undertake a sustained effort to reduce emissions. The ultimate goal is to reduce greenhouse gas emissions from municipal operations 20% below 1994 levels and to reduce community-wide greenhouse gas emissions at least 6% below 1994 levels.

The PCP program is based on five milestones:

- Milestone 1 - greenhouse gas emissions inventory and forecast;
- Milestone 2 - establish a reduction target;
- Milestone 3 - develop a local action plan;
- Milestone 4 - implement plan; and
- Milestone 5 - monitor progress and report on results.

Kamloops has completed Milestone 2 and is currently working on Milestone 3.

1.0 NATURAL ENVIRONMENT

GOALS

- To protect and enhance the quality of the natural environment.
- To protect and enhance fish and wildlife habitats in balance with flood protection and recreational access to riverbank and open space areas.

Natural Environment Policies

- 1.1 The City will continue its efforts to improve air quality through such means as:
- controlling and restricting open burning;
 - reviewing solid fuel burning appliances;
 - investigating alternatives to open burning (recycling, composting);
 - supporting additional monitoring, research and enforcement by the province; and
 - encouraging public transit usage, alternative transportation (walking/bicycling), and travel demand management (reducing reliance on automobiles).
- 1.2 The City will continue to participate in the Partners for Climate Protection program to reduce greenhouse gas produced locally.
- 1.3 The City will encourage the development of a compact, ecologically friendly and efficient land use and transportation system that emphasizes energy conservation, air pollution reduction, land stewardship, sustainability, and growth management.
- 1.4 The City will take a proactive role in preserving and acquiring riverfront lands, particularly focusing on:
- extending the Rivers Trail system outwards from the City Centre by taking advantage of vacant properties and redevelopment opportunities as they arise;
 - maintaining natural vegetation, but controlling erosion where necessary; and
 - ensuring public ownership and access along the waterfront.
- 1.5 The City will continue to work with senior levels of government to monitor and mitigate, where necessary, riverbank erosion, particularly in the Dairy Road and North Rayleigh bends. Development should be set back a minimum of 200 m from the top of bank in these two locations.
- 1.6 The City shall continue to pursue opportunities to reduce or mitigate the impact of development and activity on the Thompson drainage basin in an attempt to prevent any further degradation in the quality of the rivers. In particular:
- runoff and erosion from the Juniper, Stobbart, and Chase Creek tributaries will be monitored, and efforts will be undertaken in conjunction with the TNRD and provincial ministries to reduce the amount of silt entering the river; and
 - the City will undertake measures to prevent contaminants and untreated effluents from entering the river system.

- 1.7 The City supports the protection of riparian habitat along all environmentally sensitive watercourses including along the North and South Thompson and Thompson Rivers. Development should be set back a minimum of 15 m and, where possible, 30 m from the top of bank to ensure habitat preservation.
- 1.8 The City will work with senior levels of government and other stakeholders to develop community greenway concepts during development of lands or in a community planning process as per recommendations in the Stewardship Series "Community Greenways". Greenway concepts will seek to balance riparian habitat preservation, flood protection, riverbank stabilization, and recreational access, among other issues. Use of artists' renderings of the proposed riverbank treatments would allow all parties, including regulatory agencies, to understand the proposed works.
- 1.9 The City will focus its parkland and open space efforts, through such means as the annual budgetary process and/or its dedication/acquisition tools, on natural open space and passive areas. This includes such initiatives as ensuring natural linkages and connectors between neighbourhoods and pursuing large-scale natural areas.
- 1.10 The City will support the Environmental Performance Advisory Committee in its efforts to protect and improve the environment.
- 1.11 The City will identify important natural areas that require protection or preservation, including:
 - environmentally sensitive areas,
 - hazard lands,
 - parks and open space,
 - greenways, and
 - grassland areas.

2.0 URBAN ENVIRONMENT

GOAL

- To develop and maintain an aesthetically appealing and environmentally sensitive urban environment to enhance the quality of life for residents and visitors alike.

Urban Environment Policies

- 2.1 The City will encourage the development of the compact community, which includes promoting higher density living and/or intensification and mixing of land use, as a viable, environmentally sound growth management tool.
- 2.2 The City will continue to balance the need for economic development and fiscal responsibility with the necessity to enhance the urban environment.

- 2.3 The City will encourage good urban and building design through the establishment of Development Permit Areas (e.g. City Centre, North Shore Towne Centre and Tranquille Commercial District), efficient servicing standards, and design and landscape guidelines.
- 2.4 The City will support the designation and preservation of the community's natural and cultural heritage.
- 2.5 The City will show leadership in maintaining and enhancing municipal property and encouraging other government agencies and the private sector to do their part.
- 2.6 The City will continue its involvement in tree protection/tree planting initiatives, paying particular attention to encouraging new development to provide a better balance by designing around trees, and to recognize the importance of street trees in an urban environment.
- 2.7 The City may require boulevard/median construction and landscaping as part of all new development.

3.0 HAZARD LANDS

GOAL

- To ensure public safety and protection from property damage from areas exhibiting hazardous conditions.

Hazard Lands Policies

- 3.1 The location and boundaries of known Hazard Lands shown on Map 3 are approximate and may be revised with additional study or as development proceeds. These include:
 - flood plain,
 - silt bluffs,
 - areas of excessive slope (>25%), and
 - fire hazard lands (urban/wildland interface).

The City will identify areas of hazard and establish appropriate guidelines for development including mitigation measures or restrictions.
- 3.2 The City will consider undertaking the development of a *Flood Plain Management Plan* within the term of this plan.
 - 3.2.1 The City will work with the provincial Ministry of Water, Land and Air Protection on the review and updating of the flood plain mapping within the City's boundaries.
 - 3.2.2 The City will continue its dialogue with senior levels of government regarding the maintenance, rehabilitation, and funding of orphan dykes within the City's boundaries in order to provide a consistent level of flood protection within the City.

- 3.2.3 The City will determine its new roles in managing flood plain development issues in light of changes to the *Local Government Act*.
- 3.2.4 The City will continue to permit development within the designated flood plain (200 year plus 0.6 m allowance) in accordance with the provincial Ministry of Water, Land and Air Protection regulations.
- 3.3 The Valleyview silt bluffs (hazard lands) are subject to Development Permit guidelines.
- 3.4 Slopes steeper than 25% (4 horizontal to 1 vertical) shall be considered unsuitable for development and shall be designated open space unless it can be shown by geotechnical or engineering studies that the steep slopes can accommodate the proposed development and that there will be no detrimental impact on adjacent lands or the proposed development. Where it is demonstrated that development may occur on steep slopes, the developer shall provide the City with a save harmless covenant, and development must proceed in strict compliance with the recommendations of the geotechnical report.
- 3.5 Hillside subdivision design shall recognize severe climatic conditions, the difficulty of road maintenance, movement of pedestrians, and other environmental factors.
- 3.6 Where problem soils are encountered during development, a geotechnical investigation shall be carried out to determine the extent of the expanse of soils and, if necessary, make recommendations for construction or development techniques which will eliminate or accommodate the potential hazard.
- 3.7 The City will, in co-operation with the appropriate agencies, continue to participate in fire prevention procedures as identified through the Urban/Wildland Interface program. As a condition of approval of all new development or the issuance of Building Permits for existing lots within the urban/wildland interface, as shown on Map 3 - Hazard Lands, the City may:
- 3.7.1 Require as a condition of development approval, subdivision, or the issuance of a Building Permit, the registration of restrictive covenants to ensure that:
- potential purchasers are made aware of urban/wildland interface issues and the ongoing role that property owners must assume to protect their investment;
 - all roofing material and installation requirements must meet the Class 'B' fire rating requirements contained within the current Building Code;
 - the City is saved harmless in the event of damage to individual properties as a result of the spread of fire through the urban/wildland interface areas;

- fuel reduced buffers around individual homes from the house to property boundary or 10 m in distance, whichever is the lesser, are maintained. In this respect, "fuel reduced" shall mean an area which may contain natural tree cover, but is maintained by the owner with no accumulation of combustible debris;
 - All eaves, attics, decks, and openings under floors are screened to prevent the accumulation of flammable material; and
 - All-wood burning appliances are installed with approved spark arresters.
- 3.7.2 Require an assessment of the geographical features of the lands intended for development, in consultation with the Ministry of Forests, to address the potential for urban/wildland hazards and to determine the suitability of the proposed development to the land.

4.0 ENVIRONMENTALLY SENSITIVE AREAS

GOALS

- To preserve and protect environmentally sensitive and unique natural areas.
- To protect and enhance fish and wildlife habitats in balance with urban development and human use and enjoyment of open space.

Environmentally Sensitive Area Policies

4.1 Identified Environmentally Sensitive Areas (ESAs) in the City of Kamloops are shown on Map 4. These ESAs are based on information provided by the *State of the Environment Report (1994)*, by the *Endangered Spaces Project: Land for Nature in the Kamloops Area (1991)*, by government agencies, and by community input. These areas include:

- watercourses, including ponds, lakes, rivers or streams, and natural drainage courses;
- significant cliff features, benchlands, escarpments, bluffs, geological features, and grasslands;
- wildlife, aquatic, and riparian habitat; and
- habitat known to be occupied by rare or endangered species.

The location and boundaries of the ESAs shown on Map 4 are approximate and may be revised with additional study or as development proceeds.

- 4.2 The City will endeavour to preserve and protect ESAs, particularly those that are under pressure for development within the term of this Plan. The City may use one or more of the following tools or mechanisms, depending on which is appropriate to a given situation:
- Development Permit Areas and guidelines;
 - environment protection bylaws (Tree Bylaw, Earthwork Control Bylaw, and Natural Watercourse Bylaw);
 - dedication/acquisition;
 - leave strips or buffer areas;
 - density bonusing provisions;
 - conservation covenants; or
 - co-operation with other agencies.
- 4.3 The City will work co-operatively with senior levels of government and the community to:
- identify new ESAs;
 - review City bylaws and strategies to ensure appropriate protection and preservation of ESAs; and
 - identify additional means of preserving and protecting ESAs, which may include:
 - implementing specific techniques, measures or guidelines for development as outlined in *Land Development Guidelines for the Protection of Aquatic Habitat and Stream Stewardship: A Guide for Planners and Developers*;
 - undertaking a Community Greenway pilot project;
 - through the development approval process, requiring the preparation of environmental assessments to assist the City in determining location, extent, and environmental characteristics of an area;
 - specific measures to avoid, minimize and/or mitigate environmental impacts; and
 - specific site rehabilitation measures.
- 4.4 The City will incorporate ESA planning in future neighbourhood planning exercises in order to identify:
- the extent and location of significant environmental features;
 - new ESAs; and
 - appropriate tools/mechanisms to preserve and protect identified ESAs.

Neighbourhood planning is the most appropriate level for the City to undertake detailed ESA planning as this is when land use, servicing, and park requirements are considered and when sufficient information is available.

- 4.5 The City will develop a Bear Aware Strategy to minimize the potential of bear/human interactions.

SECTION VIII: IMPLEMENTATION

KAMPLAN 2004, the City's Official Community Plan (OCP), is enacted in accordance with the *Local Government Act*.

KAMPLAN 2004 is the City's statement of objectives and policies to guide decisions on planning and land use management (*Local Government Act Section 877*) for all lands within the City of Kamloops. A community plan must include statements on the following:

- the approximate location, amount, type, and density of residential development required to meet anticipated housing needs over a period of at least five years;
- the approximate location, amount, and type of present and proposed commercial, industrial, institutional, agricultural, recreational, and public utility land uses;
- the approximate location and area of sand and gravel deposits that are suitable for future sand and gravel extraction;
- restrictions on the use of land that is subject to hazardous conditions or that is environmentally sensitive to development;
- the approximate location and phasing of any major road, sewer, and water systems; and
- the approximate location and type of present and proposed public facilities, including schools, parks, and waste treatment and disposal sites.

In addition, a community plan may include statements (*Local Government Act, Section 878*) relating to:

- social needs, social well-being, and social development;
- a regional context statement of how matters in the Official Community Plan are consistent with the Regional Growth Strategy;
- policies of the local government respecting the maintenance and enhancement of farming; and
- policies of the local government relating to the preservation, protection, restoration, and enhancement of the natural environment, its ecosystems, and biological diversity.

The City has developed the following implementation policies for KAMPLAN 2004.

1.0 APPLICATION

- 1.1 **KAMPLAN 2004** applies to all lands within the City of Kamloops. The Official Community Plan (OCP) is a land use and servicing strategy which is reviewed and updated every five years. The OCP provides long-term guidance on land use, transportation, municipal infrastructure, and economic development for the period from 2004 to 2036. It will be monitored, reviewed, updated, and amended as necessary to retain its currency.

2.0 ADOPTION

- 2.1 The Official Community Plan (OCP) is adopted in accordance with Section 876 of the *Local Government Act* (as amended).

3.0 SECONDARY PLANS

- 3.1 The following plans form part of KAMPLAN 2004 and provide more specific direction for the areas covered therein:

- Southeast Sector Plan (1992),
- Westsyde Neighbourhood Plan (1994),
- City Centre Plan (2005),
- North Shore Neighbourhood Plan (2008),
- Mt. Dufferin Land Use Plan (1996).
- Batchelor Hills Land Use Plan (1997),
- Kamloops Airport Area Land Use and Development Plan (2000),
- McGill Corridor/Southgate Concept Plan (2002),
- McDonald Neighbourhood Plan (2004),
- (5-1-2426) • Aberdeen Plan (2008)
- (5-1-2566) • Social Plan (2009), and
- (5-1-2572) • Industrial Land Review 2010-2015
- (5-1-2639) • Tranquille on the Lake Neighbourhood Plan (2012)

These plans shall be reviewed and updated to ensure consistency with this plan and to reflect changing circumstances. Where there is an apparent conflict or inconsistency between a general statement or designation in the OCP and a specific statement or designation in a more specific adopted plan, the more specific shall prevail.

4.0 ZONING BYLAW

- 4.1 Zoning Bylaw No. 5-1-2001 (as amended) shall be reviewed and revised as necessary to ensure consistency with this plan.
- (5-1-2501) 4.2 The City will support the use of performance- or flex-based zoning regulations to encourage sustainable forms of development, pedestrian-focused environments, sensitive integration of new development into existing neighbourhoods, and the use of creative or innovative forms of housing, particularly in existing housing stock located in older Kamloops neighbourhoods.

5.0 TEMPORARY USE PERMITS

- (5-1-2587) 5.1 City Council, in any area of the City, may issue a Temporary Use Permit for a three-year period, renewable once only, provided that the temporary use:
- is not noxious or undesirable because of smoke, noise, vibration, dirt, glare, odour, radiation, electrical interference, or an offensive trade within the meaning of the *Health Act*, as amended from time to time;
 - does not have a negative impact on adjacent lands;
 - does not create a significant increase in the level or demand for services;
 - does not permanently alter the site upon which it is located;
 - is compatible with the property's land use designation; and
 - complies with all the conditions specified by Council in the Temporary Use Permit.

6.0 DEVELOPMENT PERMIT AREAS

- 6.1 City Council, by resolution, may issue Development Permits which can vary or supplement provisions of any bylaw adopted under Division (7) or (11) of Part 26 of the *Local Government Act* (as amended), including:
- zoning,
 - parking,
 - drainage,
 - signs,
 - screening, and
 - subdivision servicing requirements.

A Development Permit may address additional aspects of development which may vary by category of designation.

6.2 Development Permits may only be required in areas designated for:

- protection of the natural environment, its ecosystems, and biological diversity;
- protection of development from hazardous conditions;
- protection of farming;
- revitalization of areas in which commercial use is permitted;
- establishment of objectives for the form and character of commercial, industrial, or multiple family residential development; or
- establishment of objectives for the form and character of intensive residential development.

Where areas are designated, the special conditions or objectives which justify the designation must be described, and guidelines identifying how the conditions will be alleviated and how the objectives will be achieved must be specified.

6.3 Schedule 1 to this plan outlines the designated Development Permit Areas within the City and the justification, objectives, and guidelines associated with each area.

7.0 COMMUNITY INPUT

7.1 The City will encourage public input and involvement in the growth and development of the community and will monitor its various planning documents through a variety of means, including but not limited to:

- hosting neighbourhood forums, public meetings, open houses, and town hall meetings;
- conducting referendums;
- holding public hearings for development applications;
- conducting surveys, including the Municipal Attitude Survey, to gather public opinion;
- encouraging the establishment and involvement of neighbourhood associations; and
- undertaking planning exercises.

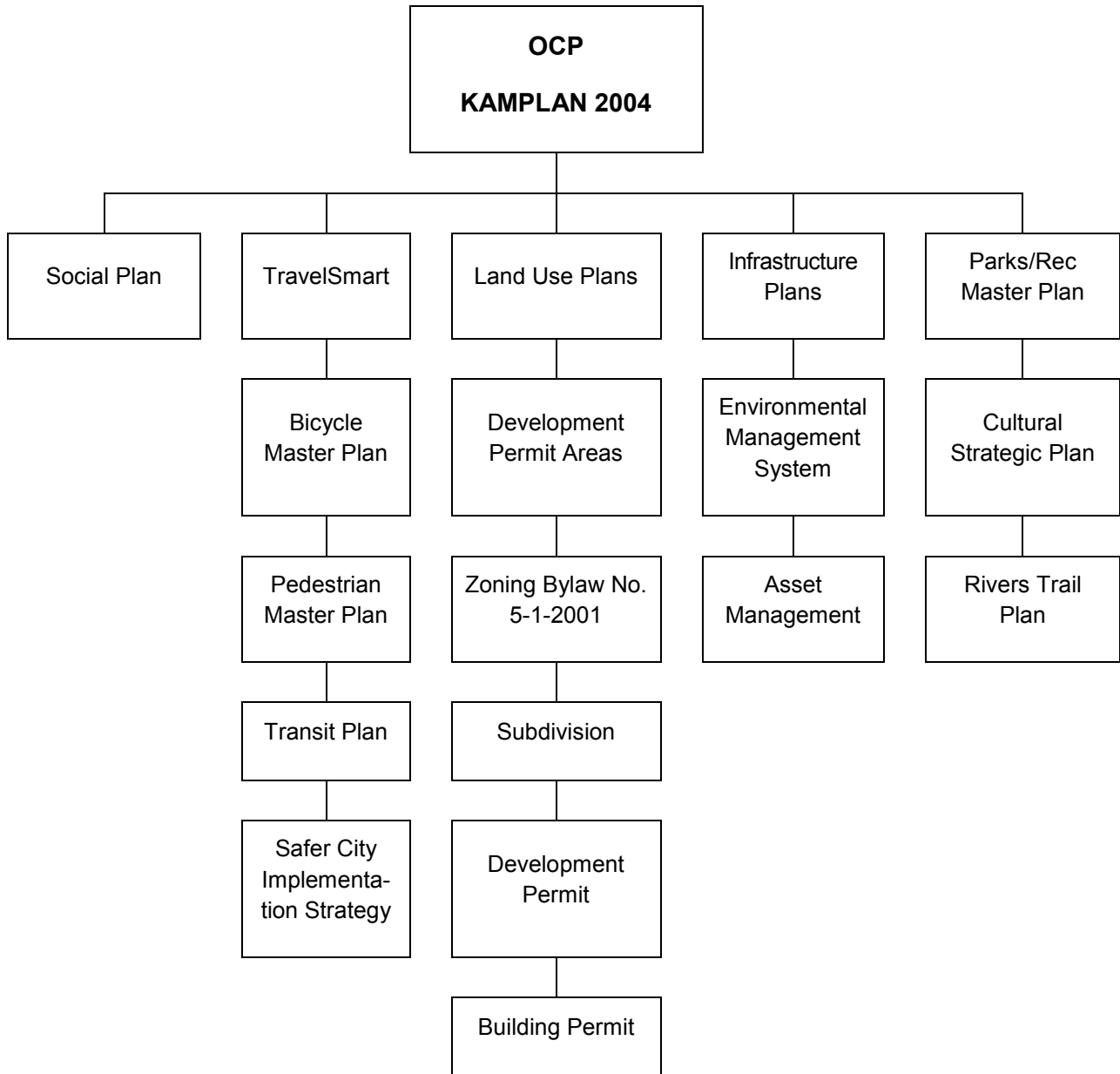
8.0 OCP MONITORING

8.1 Successful implementation requires a commitment to ongoing review and updating at regular cycles. KAMPLAN 2004: The Official Community Plan should be monitored continuously and revised as necessary to ensure that it remains valid. The City shall monitor regularly:

- population growth and demographic changes,
- economic, social and environmental factors,
- commercial/industrial development,
- housing,
- land supply/demand, and
- services and infrastructure.

9.0 OCP HIERARCHY

9.1 KAMPLAN refers to several additional planning documents, including detailed policies, regulations, guidelines, strategies, or master plans that have been prepared for specific topics. These plans have relevance to the OCP in that they either provide more specific direction about a particular common theme or issue, or they are required to implement the general policies found within the OCP. The following chart shows the OCP as it relates to these documents:



SCHEDULE 1 - DEVELOPMENT PERMIT AREAS

Schedule 1 to Bylaw No. 5-1-2146 - KAMPLAN 2004: A Community Plan for Kamloops

In accordance with the *Local Government Act*, Section 879, as amended, a Community Plan may designate Development Permit Areas within the City. Unless otherwise specified, a Development Permit must be approved by City Council prior to any development, subdivision, construction or alteration within a Development Permit Area.

Development Permits may only be required for the following categories:

- Protection of the natural environment, its ecosystems, and biological diversity,
- Protection of development from hazardous conditions,
- **Protection of farming,**
- Revitalization of areas in which commercial use is permitted,
- Establishment of objectives for the form and character of commercial, industrial or multi-family residential development, or
- Establishment of objectives for the form and character of intensive residential development.

1.0 The following are the designated Development Permit Areas within the City of Kamloops.

1.1 CITY CENTRE DEVELOPMENT PERMIT AREA

Application: Category (e) Form and character of intensive residential development.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial, industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: City Centre Development Permit Area is the area shown shaded grey on the attached map.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations,
- external renovations which do not affect the form and character of the building, or
- single or two family dwellings.

Implementation:

In accordance with Bylaw No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Justification:

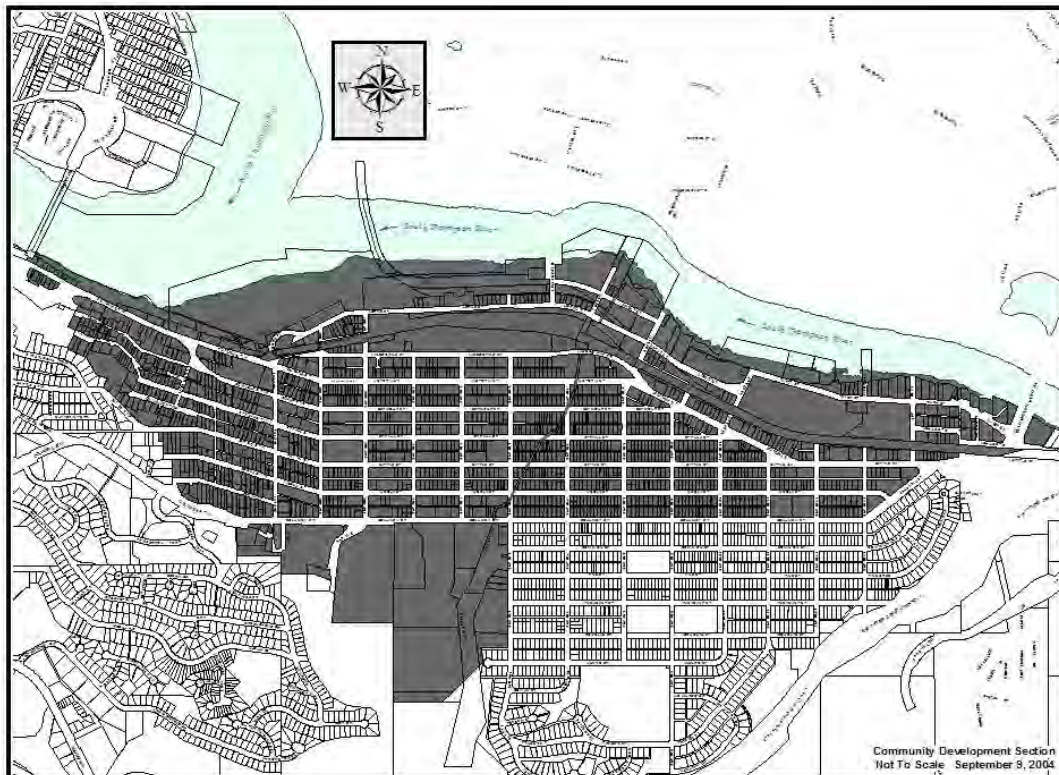
The City Centre is Kamloops' focal point. The concentration of office space, civic facilities, river front amenities, main street retail and well established residential neighbourhoods provides a strong, vibrant community centre. Kamloops is growing, and major new developments are anticipated within this area. The objective of the designation is to ensure that new development is compatible with the form and character of existing development.

Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines:

- a) New buildings should be designed so that their form and character are compatible with existing adjacent development.
- b) Continuity of building height is important in maintaining a pedestrian oriented street wall. Buildings should generally conform to the heights of adjacent buildings. When new buildings will be taller than their neighbours, they should be set back from the street front with a podium of the same height as adjacent buildings.

- c) Buildings taller than three storeys will require a building height impact assessment to identify impacts and suggest possible mitigation measures.
- d) Buildings should be oriented to the primary street on which the building fronts. Street character should be enhanced through ground level retail activity, ground level access, and use of a variety of building materials, textures and scales at ground level. Large expanses of blank wall should be avoided.
- e) Buildings should maintain the same setbacks as adjacent development. Buildings at intersections should provide corner cuts. Where buildings are set back from the street, parking should not be permitted in the front yard. These areas should be landscaped.
- f) Parking lots should be screened and landscaped. Parking should be underground wherever possible and accessible from rear lanes rather than primary street fronts.
- g) Buildings should be designed with architectural features or setbacks at regular intervals to avoid the appearance of long, homogenous facades and to reduce apparent building mass.
- h) The *City Centre Plan, 2005*, Section 11.5, Design Guidelines, should be referred to for more specific guidance.



CITY CENTRE DEVELOPMENT PERMIT AREA

(5-1-2398)

1.2 - North Shore Development Permit Area

AUTHORITY

In accordance with the *Local Government Act*, Section 920, as amended, Development Permit Areas may be designated within a city. The North Shore Neighbourhood Plan identifies one Development Permit Area that corresponds to the land use policies and the future vision and goals of the community. Unless otherwise specified, a Development Permit must be approved by City Council prior to any development, subdivision, construction or alteration within a Development Permit Area.

JUSTIFICATION

Development Permits are required for the following reasons:

- Protection of the natural environment, its ecosystems and biological diversity;
- Protection of development from hazardous conditions;
- Protection of farming;
- Revitalization of areas in which commercial use is permitted;
- Establishment of objectives for the form and character of intensive residential development; or
- Establishment of objectives for the form and character of commercial, industrial, or multi-family residential development.

The following Development Permit Guidelines are applicable to any commercial, industrial and/or institutional development located within the boundaries of the North Shore Neighbourhood Plan. These Development Permit Guidelines are intended to supplement the city-wide Multiple Family Development Permit Area Guidelines and the other area-specific Development Permit Areas of the North Shore as described and identified in this document.

EXEMPTIONS

Development Permit exemptions apply to internal renovations and/or external renovations that do not affect the form and character of the building.

IMPLEMENTATION

In accordance with By-law No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications with a construction value of \$250,000 or less; multiple family development of eight or fewer units; where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

VARIANCES

In accordance with the policies, vision, goals and principles of the North Shore Neighbourhood Plan, variances for parking, setbacks, building height and size and landscaping requirements will be considered as part of a Development Permit, where those variances provide for high quality building and site design, eco-friendly building and site design and development that makes a positive contribution to the existing neighbourhood.

ILLUSTRATIONS

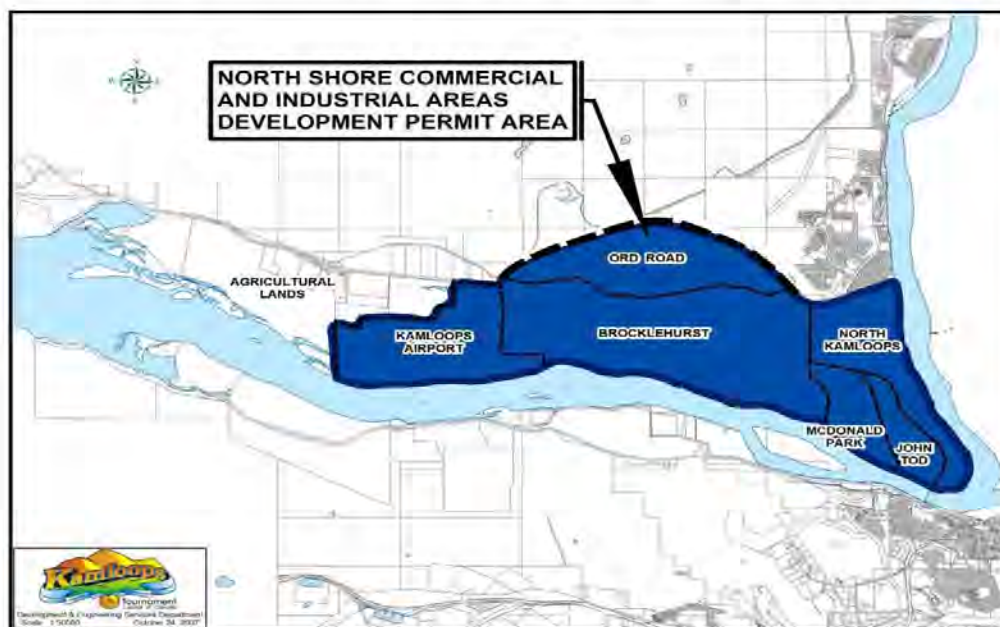
Images depicting various development types are included within these Guidelines to provide illustrative examples for reference.

1.2 - North Shore Development Permit Area

NORTH SHORE

DEVELOPMENT PERMIT GUIDELINES

As shown in Section 1.2 of the Official Community Plan, the following Development Permit Guidelines are applicable to the area shown on the Map below and are intended to supplement the city-wide Multiple Family Development Permit Area Guidelines.



APPLICATION

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial, industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation; pedestrian circulation and access; landscaping, building elevations, site layout, and street enhancements.

Building design and site layout shall be designed in such a way so as to be consistent with the vision, goals and principles identified in the North Shore Neighbourhood Plan by providing pedestrian-focused environments (except as noted), innovative designs, and sustainable building practices.

DEVELOPMENT PERMIT AREA

The North Shore has several areas of commercial and industrial development scattered throughout both North Kamloops and Brocklehurst. Some of these areas have unique development constraints and characteristics that necessitate additional development permit guidelines over and above the general ones listed here for the whole of the North Shore. These areas include: North Shore Towne Centre, Tranquille Commercial District, Tranquille South, Brock Shopping Centre, Airport Entry Corridor, 8th Street Corridor, Halston Entry Corridor and Ord Road Corridor. The additional specifications for development within these areas is listed within this document.

1.2 - North Shore Development Permit Area

NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - BUILDING MASSING

Required Elements	Recommended Elements
<ol style="list-style-type: none"> 1. Building massing shall avoid long, continuous and blank vertical surfaces adjacent to streets and sidewalks and in particular, adjacent to pedestrian access areas. 2. Building massing shall create pedestrian-oriented developments along street fronts and along major pedestrian thoroughfare routes. 3. Building massing shall create visual interest and appeal with the use of varied roof lines, roof pitches and roof heights. 	<ol style="list-style-type: none"> 1. Building massing shall avoid long, continuous and blank vertical surfaces throughout the entire development.

NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - BUILDING DESIGN

Required Elements	Recommended Elements
<ol style="list-style-type: none"> 1. Building design shall consider the relationship to and the transition between adjacent buildings and properties. 2. Building design shall completely screen all service areas and utility equipment from adjacent streets and properties and from pedestrian thoroughfares. 3. Building design incorporating residential uses shall use balconies. Balconies shall be large enough to allow active use and seating. 4. Buildings designed with flat roofs shall be compensated with the combination of the use of varied materials, building projections, articulations and architectural details and modulations so as to detract from the unvaried roof line. 5. Building design shall use lighting fixtures that create visual interest and that are located on the building and the site at a human scale rather than an automobile orientation. 6. Building design shall incorporate architectural or landscaping elements to screen roof-top mechanical equipment. 7. Building design shall orient entrances to the primary street which fronts the building. 8. Building design for high density developments shall avoid large wall building massing, shall use various heights and vertical setbacks to create visual appeal and to create a design that works toward appropriate neighbourhood integration. 	<ol style="list-style-type: none"> 1. Building design should emphasize texture, pattern, form, shadow, structure and variety. 2. Building design should include a significant portion of display windows at the ground floor or pedestrian level of the building. 3. Building design should incorporate canopies, awnings and other protective structures complimentary to the design, materials and color to add visual appeal and interest. 4. Building design should not include colors that are abrasive or jarring and should work to create pleasing tones and textures appropriate for a pedestrian-focused environment and appropriate to the surrounding natural amenities and natural features. 5. Building design using flat roofs should incorporate amenity areas and/or green roof systems. 6. Building design for mixed use developments should provide ground floor commercial uses that are accessible at grade. 7. Building design should seek to incorporate an urban identity with focal point open spaces throughout building placement and massing. 8. Building and site design should support an interesting and desirable image for the community. 9. Building design should not incorporate the same colour scheme for accents as is used on the main portion of the building.

1.2 - North Shore Development Permit Area

NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Parking locations within front and side yards and along street frontages can occur (except as noted) where a minimum of a 4 m landscape buffer strip is provided between the parking area and a property line. Parking areas shall not be located directly adjacent to major pedestrian thoroughfares. 2. Bicycle parking shall be provided on site in a convenient and easily accessible manner to a minimum of 0.1 bicycle stalls per vehicle stall. 3. Parking lots shall be designed to channel pedestrian movements to reduce conflicts with vehicles and to connect to public sidewalks. 4. Parking lot illumination shall be designed for when people are not in their vehicles and shall avoid light pollution or the spilling of light onto adjacent properties. 5. Parking areas within parking structures shall not be visible from the building's primary access points and shall be located either below ground or no lower than the second floor of a building and signed appropriately to allow for ease of way finding. 	<ol style="list-style-type: none"> 1. Bicycle parking should be provided in a transparent but enclosed area to allow for security and safety of the bicycle parking area. 2. Parking areas within parking structures are encouraged to be designed so as to only occupy 50 per cent or less of a single storey, within a multi-storey, mixed-use building.

NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - BUILDING MATERIALS

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Building materials shall not be limited to the use of one type only but rather incorporate a minimum of three different types of building materials to add variety and interest to the design. 2. Building materials shall not include vinyl siding, glassy vinyl fabric, mirrored glass, plastics and/or prefabricated metal sheets. Stucco coated exterior installations shall be limited in use. Unpainted concrete shall not be permitted. 3. Building materials, at street level, shall not include dark, highly-reflective glass, glass block or any other treatments that preclude viewing into buildings. 	<ol style="list-style-type: none"> 1. Building materials should be products such as timber, stone, brick, concrete, metal and glass. Glass block should be used only as accent material. 2. Building materials that include the use of stucco, concrete unit masonry, tile, wood trim and siding should be used as accents and secondary materials. Materials should create pattern and texture and convey quality and high standards of design. 3. Care should be taken to ensure that building colour does not clash with adjacent building facades.

NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
	<ol style="list-style-type: none"> 1. Green technology should be incorporated into the building site, design and function, including the use of on-site stormwater distribution and retention and the incorporation of stormwater features into landscaping and/or public spaces. The use of green roofs and the use of environmentally sustainable building standards such as Green Globes, Built Green or LEED.

1.2 - North Shore Development Permit Area

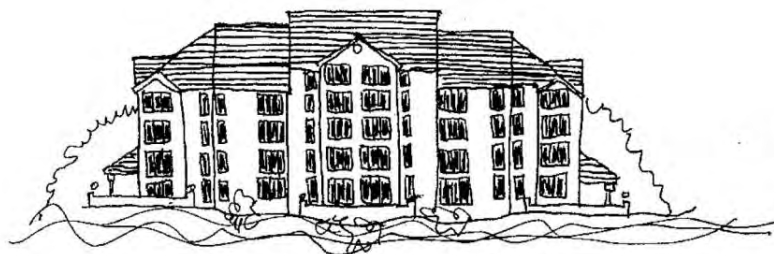
NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - SIGNAGE

Required Elements	Recommended Elements
<ol style="list-style-type: none"> 1. Signage shall be incorporated into the building in a manner that creates a coordinated and visually relative and appealing manner to that of the facade design. 2. Signage shall be principally pedestrian-oriented, made of durable, weather-resistant material, opaque, and coloured in such a way so as to coordinate with the facade of the building. 3. Signage shall not be the dominant feature of the facade or overall development. 4. Billboards and portable read-o-graph signs shall be prohibited. 	<ol style="list-style-type: none"> 1. Podium or free standing signs can occur where height and area are limited to a pedestrian scale and where natural materials and directional lighting is used, in conjunction with landscaping. 2. Back-lighting of signage can occur where individual, three-dimensional letters are used. 3. Window signs should not cover more than 20 per cent of the total area of the ground floor frontage facing any one street.

NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - SITE LAYOUT

Required Elements	Recommended Elements
<ol style="list-style-type: none"> 1. Site layout and building design shall incorporate the principles of CPTED: Crime Prevention Through Environmental Design. 2. Site layout and design shall take into consideration adjacent uses, adjacent existing building setbacks and existing pedestrian travel thoroughfares. 3. Site layout shall be designed so that access to primary entrances, from public sidewalks, are along fully accessible and direct pedestrian access routes. 	<ol style="list-style-type: none"> 1. Site layout and design should incorporate additional features, such as public art, to create interest and focal points.

Varied sizes along main commercial cores and multi-family buildings with varied roof lines create attractive urban spaces;
 Artistic Images: see Sources and References Section;
 Drawing: Carla Stewart



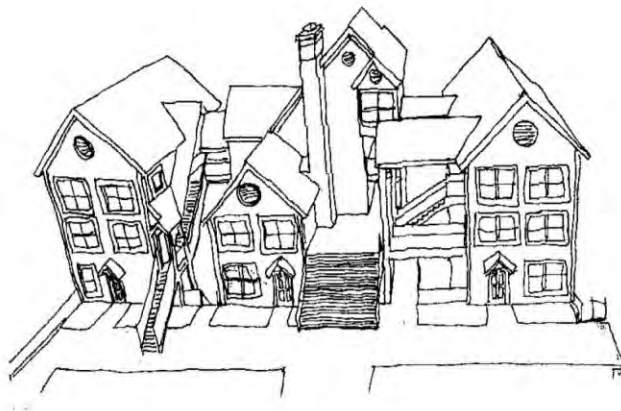
1.2 - North Shore Development Permit Area

NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - LANDSCAPING

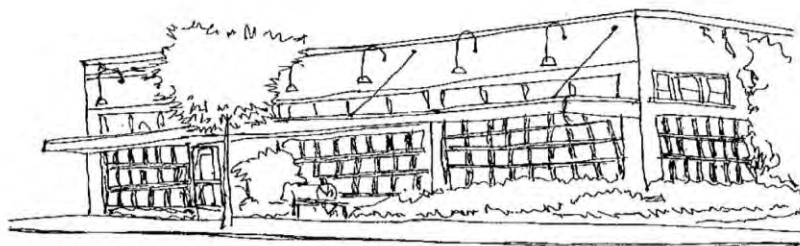
<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Landscaping shall be provided to screen all parking that may be visible from any street. 2. Landscaping shall be provided to screen all service areas, utility equipment and/or loading areas. 	<ol style="list-style-type: none"> 1. Landscaping should be used to provide amenity spaces on the roof of a building or as part of a green roof/green infrastructure storm water system. 2. Landscaping of an appropriate and sufficient manner, can be used to screen commercial garbage enclosures as a replacement or alternative to other hard material designs. 3. Landscaping of multiple family developments should use fruit-bearing species as part of the required amenity feature. 4. Landscaping should be incorporated into balconies and roof decks.

NORTH SHORE COMMERCIAL AND INDUSTRIAL AREAS - BUILDING HEIGHT

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Maximum building height is two (2) storeys. 	<ol style="list-style-type: none"> 1. Building height maximum can be increased to four (4) storeys with the addition of a combination of green building designs and eco-friendly storm water systems and roofs, market housing and non-market housing (if permitted in the zone) and/or public art.



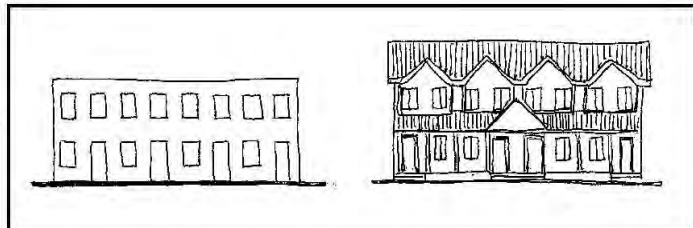
Residential and commercial developments with varied roof lines and architecturally appealing elements make for unique, vibrant and attractive urban spaces;
 Artistic Images: see Sources and References Section;
 Drawing: Carla Stewart



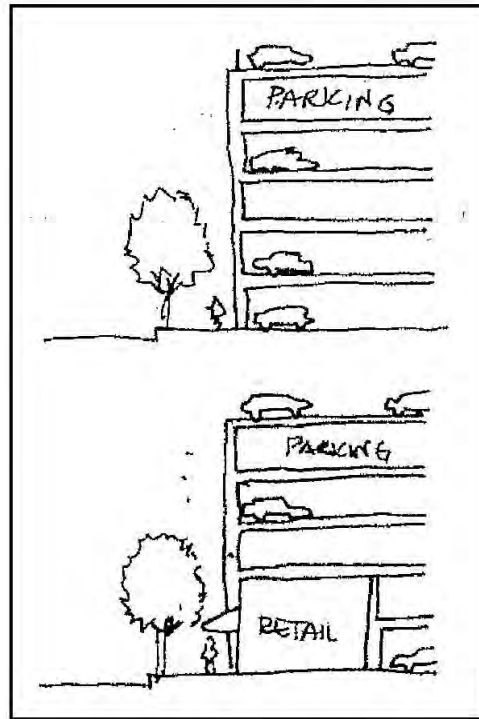
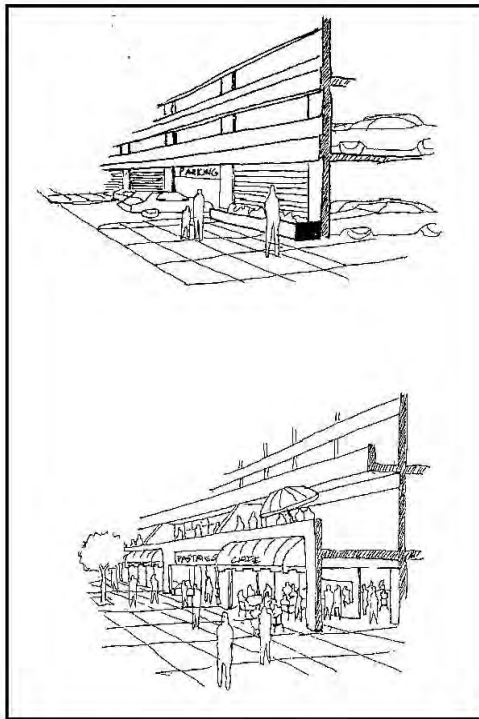
1.2 - North Shore Development Permit Area

DEVELOPMENT PERMIT GUIDELINE ILLUSTRATIONS

In order to supplement the Guidelines presented in this document, several graphic sketches are provided below to use as reference material when designing sites and buildings on the North Shore. These graphics are for illustrative purposes only. (Artistic sources for the images used in this section can be obtained in the Sources and References Section of this Plan.)



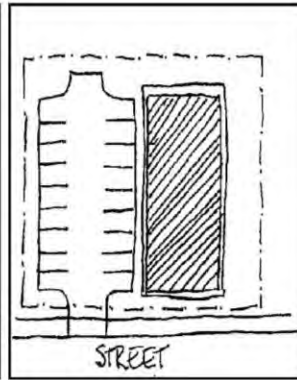
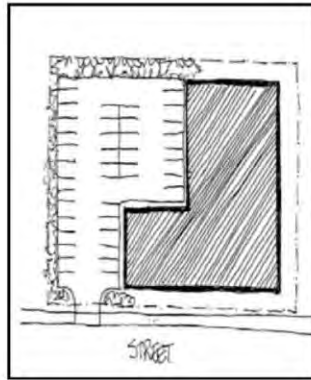
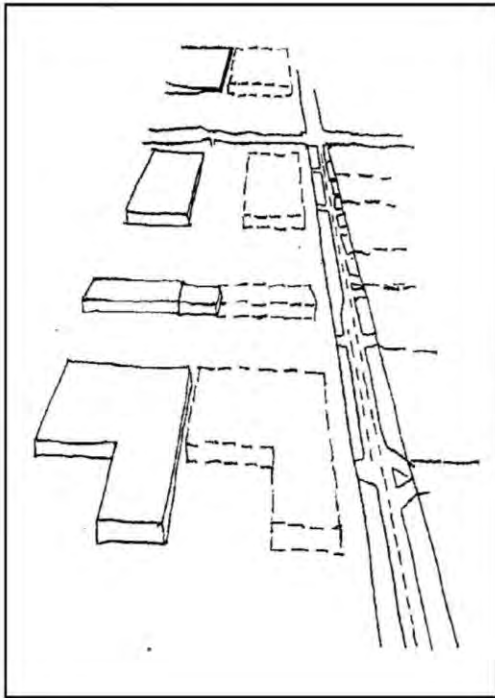
Buildings with varied roof lines and design elements providing attractive urban environments; Drawings: Carla Stewart



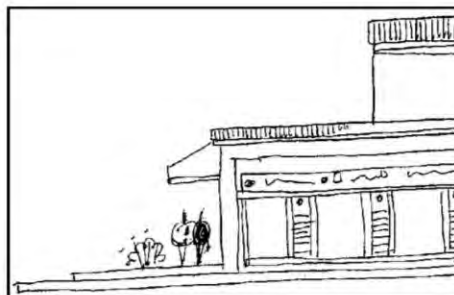
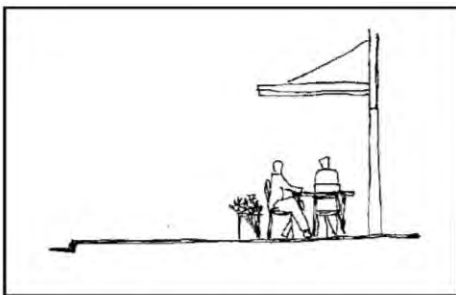
Parking structures need to be incorporated into the building design and function, providing for an active front that attracts pedestrian uses and activities; Drawings: Carla Stewart

1.2 - North Shore Development Permit Area

DEVELOPMENT PERMIT GUIDELINE ILLUSTRATIONS (cont.)



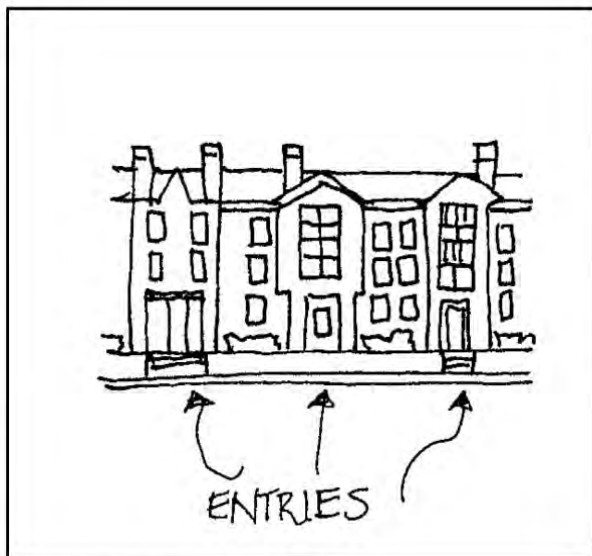
Buildings should be oriented toward the street to encourage pedestrian uses and activities; parking should never be located between a building and a sidewalk; Drawings: Carla Stewart



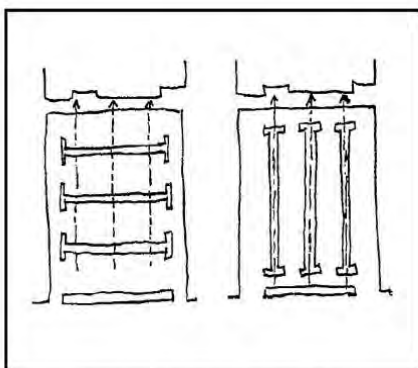
Buildings should encourage pedestrian uses by providing seating areas associated with restaurants and coffee shops, particularly along the Tranquille Commercial Corridor and within the North Shore Town Centre; Drawing: Carla Stewart

1.2 - North Shore Development Permit Area

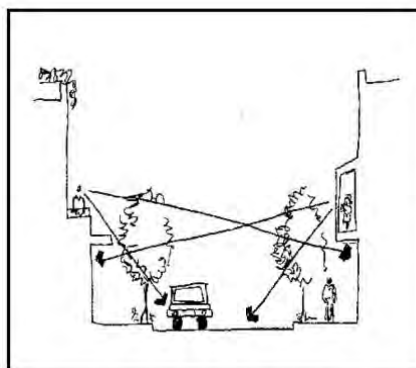
DEVELOPMENT PERMIT GUIDELINE ILLUSTRATIONS (cont.)



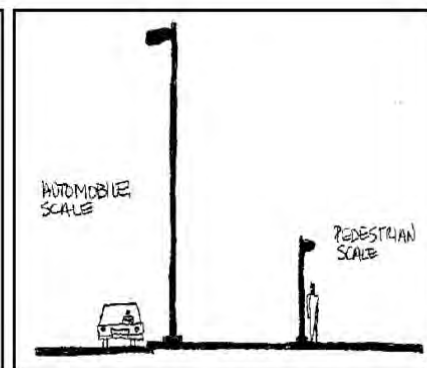
Mixed-use buildings should have multiple entrances and should have separate entrances for the residential and commercial units; Drawings: Carla Stewart



Parking lot layouts should be designed with the drive aisles running perpendicular to the buildings to create easier movements for pedestrians on site; Drawing: Carla Stewart



Buildings should be designed with natural surveillance opportunities including and especially useable balconies; Drawing: Carla Stewart



Light fixtures should be designed and installed to accommodate a pedestrian scale rather than a scale for automobiles; Drawing: Carla Stewart

1.2 - North Shore Development Permit Area

NORTH SHORE TOWNE CENTRE

AREA SPECIFIC GUIDELINES

JUSTIFICATION

The Towne Centre is a focal point for the North Shore with concentrations of commercial uses, heavy pedestrian and vehicle traffic and activities centred around the second busiest intersection in the city. Community input during the preparation of the North Shore Neighbourhood Plan identified the North Shore Towne Centre as a significant area that, when developed, should showcase the North Shore with significant buildings designed to make a connection to the high level of pedestrian activity existing throughout the area. Achieving this requires high design standards along with the use of innovative, creative and eco-friendly building and site design concepts that accommodate pedestrians over the vehicle.

GUIDELINES

The following Development Permit Guidelines are applicable to the area shown on the Map below and are intended to supplement the North Shore Development Permit Area Guidelines as well as the city-wide Multiple Family Development Permit Area Guidelines. Development Permits issued in this area shall be in accordance with the following Guidelines:



1.2 - North Shore Development Permit Area

NORTH SHORE TOWNE CENTRE - SITE LAYOUT AND PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Parking shall not be located in any front or side yard or along any street frontage. 2. Site layout and building design, for those properties fronting the Fortune Drive, Tranquille Road and 8th Street intersection, shall form or incorporate a landmark character feature and shall have building frontage extend along the street frontage. 3. Site layout and building design, for those properties fronting the Simcoe Avenue corridor, shall provide pedestrian-oriented development along both Simcoe Avenue and Fortune Drive in order to create a built form that is consistent with the roles and functions of a Towne Centre. 	<ol style="list-style-type: none"> 1. Parking locations within front and side yards and along street frontages can occur where a minimum of a 4 m landscape buffer strip is provided between the parking area and a property line.

NORTH SHORE TOWNE CENTRE - BUILDING SETBACKS

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Front yard setback minimum is 0 m. 2. Front yard setback maximum is 1.0 m. 3. Side yard and/or side street yard setback minimum is 0 m. 4. Side yard and/or side street yard setback maximum is 1.0m. 5. Rear yard setback minimum is 0 m. 6. Rear yard setback minimum, where a lot is adjacent to a residential zone with no intervening lane or street, is 2 m. 	<ol style="list-style-type: none"> 1. Front yard setback minimum and maximums can be increased only with the use of a pedestrian plaza or front entry landscape feature or public area feature at the corner of any property adjacent to the Fortune Drive, Tranquille Road and 8th Street intersection.

1.2 - North Shore Development Permit Area

NORTH SHORE TOWNE CENTRE - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Landscaping shall cover a minimum of five (5) per cent of the total site area, not including the boulevard. 2. Auto-oriented, vinyl, back-lit podium or free standing signs shall not be permitted. 	<ol style="list-style-type: none"> 1. Landscaping minimum can be reduced, up to three (3) per cent of the total site area, with the use of eco-roofs and green infrastructure storm water systems that use landscaped features.

NORTH SHORE TOWNE CENTRE - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Building design, for those properties fronting the Tranquille Road, Fortune Drive and 8th Street intersection, and along the Simcoe Avenue corridor, shall be designed and situated so as to create a landmark or focal point building for the North Shore Towne Centre. Landmark and focal point buildings shall incorporate significant architectural styles and features to distinguish them from adjacent properties and to create a positive impact on the landscape and in conjunction with the pedestrian movements and activities in the area. 2. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Towne Centre Area in the North Shore Neighbourhood Plan. 3. Building design shall contribute to a lively and useable pedestrian scale over and above auto-oriented designs. 	<ol style="list-style-type: none"> 1. Building design should provide a continuous street frontage to encourage a pedestrian-oriented community. 2. Building design should provide ground floor commercial uses that are accessible at grade. 3. Building design should include a number of separate street front shops with separate entrances.

NORTH SHORE TOWNE CENTRE - BUILDING MASSING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Building massing shall create focal points and landmark developments at the Fortune Drive, Tranquille Road and 8th Street intersection, and along the Simcoe Avenue corridor, to be consistent with the vision, goals and policies identified in the North Shore Neighbourhood Plan. 2. Building massing shall create pedestrian-oriented developments along street fronts and along major pedestrian thoroughfare routes, specifically along the Simcoe Avenue corridor. 	<ol style="list-style-type: none"> 1. Building massing shall accent focal points at primary sightlines throughout the Towne Centre.

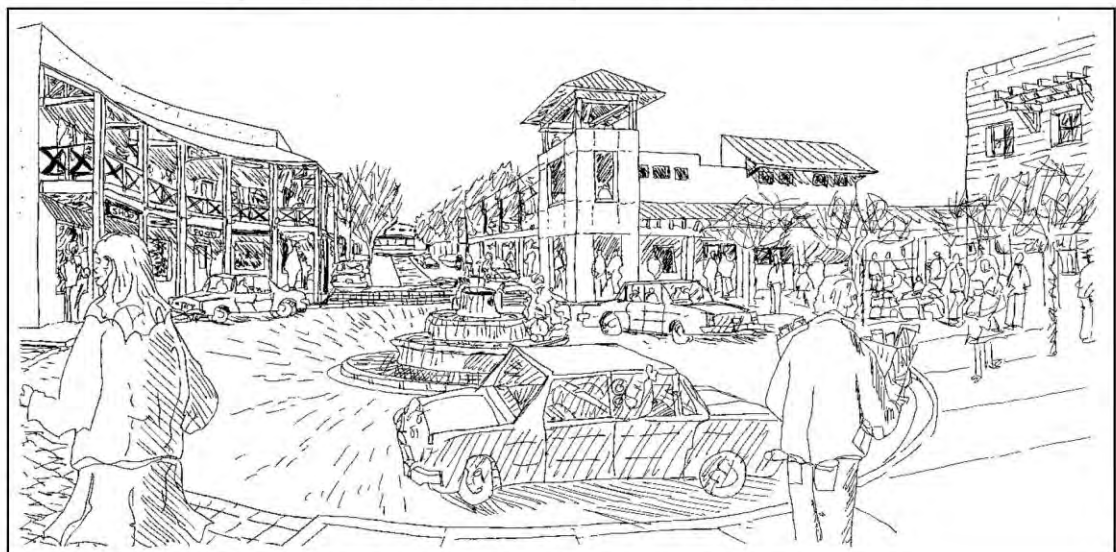
1.2 - North Shore Development Permit Area

NORTH SHORE TOWNE CENTRE - BUILDING HEIGHT

Required Elements	Recommended Elements
<ol style="list-style-type: none"> 1. Building height minimum is four (4) storeys. 2. Building height maximum is four (4) storeys. 	<ol style="list-style-type: none"> 1. Building height minimums can be reduced to two (2) storeys where buildings are constructed as a landmark or focal point building, showcasing the importance of the North Shore Towne Centre and where a combination of public art, landscaped and pedestrian-focused plazas, green building design standards, and eco-friendly storm water systems, or roofs are used. 2. Building height maximum can be increased, up to ten (10) storeys, with the addition of a combination of market housing and non-market housing units, public art, landscaped and pedestrian-focused plazas, green building design standards and eco-friendly storm water systems and roofs.



Examples of Infill Development in the North Shore Towne Centre; Source: NDLea Consultants Ltd.



Streetscape Example: Pedestrian Friendly, Visually Appealing; Traffic Calming; Mixed Uses
Original Image Source: *The Next American Metropolis*
Drawing: Carla Stewart

1.2 - North Shore Development Permit Area

TRANQUILLE COMMERCIAL DISTRICT

AREA SPECIFIC GUIDELINES

JUSTIFICATION

The Tranquille Commercial District is the original main street for the North Shore and continues to accommodate the largest and most diverse concentration of central functions for the community including retail, office, personal services, entertainment, government resources, institutional uses, residential and community activities. Supporting and expanding these uses is paramount to ensuring the success of this commercial core area. Community input during the preparation of the North Shore Neighbourhood Plan supported increased residential densities along this corridor, maintaining a pedestrian-focused environment, encouraging more office use and creating focal point developments at key intersections along the length of the street. Achieving these goals requires high design standards along with the use of innovative, creative and eco-friendly building and site design concepts that accommodate pedestrians over the vehicle.

GUIDELINES

The following Development Permit Guidelines are applicable to the area shown on the Map below and are intended to supplement the North Shore Development Permit Area Guidelines as well as the city-wide Multiple Family Development Permit Area Guidelines. Development Permits issued in this area shall be in accordance with the following Guidelines:



1.2 - North Shore Development Permit Area

TRANQUILLE COMMERCIAL DISTRICT - SITE LAYOUT AND PARKING

Required Elements	Recommended Elements
<ol style="list-style-type: none"> 1. Parking shall not be located in any front or side yard or along any street frontage. 2. Site layout and building design shall have building frontage extend along the street frontage and shall provide pedestrian- oriented development along Tranquille Road, consistent with the functions of a main street commercial area. 	<ol style="list-style-type: none"> 1. Parking locations within front and side yards and along street frontages can occur where a minimum of a three (3)m landscape buffer strip is provided between the parking area and a property line.

TRANQUILLE COMMERCIAL DISTRICT - BUILDING SETBACKS

Required Elements	Recommended Elements
<ol style="list-style-type: none"> 1. Front yard setback minimum is 0 m. 2. Front yard setback maximum is 1.0 m. 3. Side yard and/or side street yard setback minimum is 0 m. 4. Side yard and/or side street yard setback maximum is 1.0m. 5. Rear yard setback minimum is 0 m. 6. Rear yard setback minimum, where a lot is adjacent to a residential zone with no intervening lane or street, is 2 m. 	<ol style="list-style-type: none"> 1. Front yard setback minimum and maximums can be increased only with the use of a pedestrian plaza or front entry landscape feature or public area feature, particularly where development is located at intersections along the Tranquille Commercial District corridor.

1.2 - North Shore Development Permit Area

TRANQUILLE COMMERCIAL DISTRICT - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Landscaping shall cover a minimum of five (5) per cent of the total site area, not including the boulevard. 2. Auto-oriented, vinyl, back-lit podium or free standing signs shall not be permitted. 	<ol style="list-style-type: none"> 1. Landscaping minimum can be reduced, up to three (3) per cent of the total site area, with the use of eco-roofs and green infrastructure storm water systems.

TRANQUILLE COMMERCIAL DISTRICT - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Building design, for those properties fronting Tranquille Road at intersections, shall be designed and situated so as to create a focal point building for the Tranquille Commercial District. Focal point buildings shall incorporate significant architectural styles and features to distinguish them from adjacent properties and to create a positive impact on the landscape and in conjunction with the pedestrian movements and activities in the area. 2. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Tranquille Commercial District in the North Shore Neighbourhood Plan. 3. Building design shall contribute to a lively and useable pedestrian scale over and above auto-oriented designs. 	<ol style="list-style-type: none"> 1. Building design should provide a continuous street frontage to encourage a pedestrian-oriented community. 2. Building design should provide ground floor commercial uses that are accessible at grade. 3. Building design should include a number of separate street front shops with separate entrances.

TRANQUILLE COMMERCIAL DISTRICT - BUILDING MASSING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Building massing shall create focal points and landmark developments along Tranquille Road to be consistent with the vision, goals and policies identified in the North Shore Neighbourhood Plan. 	<ol style="list-style-type: none"> 1. Building massing should be consistent with development on adjacent properties. Where this is not the case, architectural elements should be incorporated into building design to off-set the difference in massing.

1.2 - North Shore Development Permit Area

TRANQUILLE COMMERCIAL DISTRICT - BUILDING HEIGHT

Required Elements	Recommended Elements
<ol style="list-style-type: none"> 1. Building height maximum is three (3) storeys. 2. Building height minimum is two (2) storeys. 	<ol style="list-style-type: none"> 1. Buildings at intersections should be constructed as a landmark or focal point building, showcasing the importance of the Tranquille Commercial District, consistent with the vision, goals and policies of the North Shore Neighbourhood Plan. 2. Building height maximum can be increased, up to six (6) storeys, with the addition of a combination of market housing and non-market housing units, public art, landscaped and pedestrian-focused plazas, green building design standards and eco-friendly storm water systems and roofs.



Examples of Infill Development in the Tranquille Commercial Area; Source: NDLea Consultants Ltd.



Streetscape Example: Pedestrian Friendly, Visually Appealing; Traffic Calming; Mixed Uses
 Original Image Source: *The Next American Metropolis*
 Drawing: Carla Stewart

1.2 - North Shore Development Permit Area

TRANQUILLE SOUTH

AREA SPECIFIC GUIDELINES

JUSTIFICATION

The Tranquille South area is a key component to the success and function of the Tranquille Commercial District. Representing a southern entry point to Tranquille Road, this area has high levels of both pedestrian and vehicle traffic and is slowly transitioning away from the original auto-oriented uses to more pedestrian-oriented businesses such as bistros, coffee shops, retail stores and restaurants. Community input during the preparation of the North Shore Neighbourhood Plan identified the Tranquille South area as having potential for increased multiple family and office use development in buildings that are significant in nature announcing the entry into a core commercial area. Achieving this goal requires high design standards along with the use of innovative, creative and eco-friendly building and site design concepts that accommodate pedestrians over the vehicle.

GUIDELINES

The following Development Permit Guidelines are applicable to the area shown on the Map below and are intended to supplement the North Shore Development Permit Area Guidelines as well as the city-wide Multiple Family Development Permit Area Guidelines. Development Permits issued in this area shall be in accordance with the following Guidelines:



1.2 - North Shore Development Permit Area

TRANQUILLE SOUTH - SITE LAYOUT AND PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Parking shall not be located in any front or side yard or along any street frontage. 2. Site layout and building design, for those properties at the entrance to the Tranquille South area and at intersections along Tranquille Road shall form or incorporate a landmark character feature and shall have building frontage extend along the street frontage. 3. Site layout and building design shall provide pedestrian-oriented development in order to create a built form that is consistent with the roles and functions of a main street commercial area. 4. Where drive-thru aisles and idle lanes are permitted, they shall be located at a minimum setback of 10 m from any front and/or side street property line or any property line abutting a street. 	<ol style="list-style-type: none"> 1. Parking locations within front and side yards and along street frontages can occur where a minimum of a three (3)m landscape buffer strip is provided between the parking area and a property line.

TRANQUILLE SOUTH - BUILDING SETBACKS

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Front yard setback minimum is 0 m. 2. Front yard setback maximum is 1.0 m. 4. Side yard and/or side street yard setback maximum is 1.0m. 5. Rear yard setback minimum is 0 m. 6. Rear yard setback minimum, where a lot is adjacent to a residential zone with no intervening lane or street, is 2 m. 	<ol style="list-style-type: none"> 1. Front yard setback minimum and maximums can be increased only with the use of a pedestrian plaza or front entry landscape feature or public area feature at the corner of any property adjacent to intersections at the entrance to the Tranquille South area.

1.2 - North Shore Development Permit Area

TRANQUILLE SOUTH - SITE LAYOUT AND PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Parking shall not be located in any front or side yard or along any street frontage.</p> <p>2. Site layout and building design, for those properties at the entrance to the Tranquille South area and at intersections along Tranquille Road shall form or incorporate a landmark character feature and shall have building frontage extend along the street frontage.</p> <p>3. Site layout and building design shall provide pedestrian-oriented development in order to create a built form that is consistent with the roles and functions of a main street commercial area.</p> <p>4. Where drive-thru aisles and idle lanes are permitted, they shall be located at a minimum setback of 10 m from any front and/or side street property line or any property line abutting a street.</p>	<p>1. Parking locations within front and side yards and along street frontages can occur where a minimum of a three (3)m landscape buffer strip is provided between the parking area and a property line.</p>

TRANQUILLE SOUTH - BUILDING SETBACKS

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Front yard setback minimum is 0 m.</p> <p>2. Front yard setback maximum is 1.0 m.</p> <p>4. Side yard and/or side street yard setback maximum is 1.0m.</p> <p>5. Rear yard setback minimum is 0 m.</p> <p>6. Rear yard setback minimum, where a lot is adjacent to a residential zone with no intervening lane or street, is 2 m.</p>	<p>1. Front yard setback minimum and maximums can be increased only with the use of a pedestrian plaza or front entry landscape feature or public area feature at the corner of any property adjacent to intersections at the entrance to the Tranquille South area.</p>

1.2 - North Shore Development Permit Area

TRANQUILLE SOUTH - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Landscaping shall cover a minimum of five (5) per cent of the total site area, not including the boulevard.</p> <p>1. Auto-oriented, vinyl, back-lit podium or free standing signs shall not be permitted.</p>	<p>1. Landscaping minimum can be reduced, up to three (3) per cent of the total site area, with the use of eco-roofs and green infrastructure storm water systems.</p>

TRANQUILLE SOUTH - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Building design, for those properties located at the entrance to the Tranquille South area, shall be designed and situated so as to create a landmark or focal point building. Landmark and focal point buildings shall incorporate significant architectural styles and features to distinguish them from adjacent properties and to create a positive impact on the landscape and in conjunction with the pedestrian movements and activities in the area.</p> <p>2. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Tranquille South area in the North Shore Neighbourhood Plan.</p> <p>3. Building design shall contribute to a lively and useable pedestrian scale over and above auto-oriented designs.</p>	<p>1. Building design should provide a continuous street frontage to encourage a pedestrian-oriented community.</p> <p>2. Building design, for buildings fronting Tranquille Road, should provide ground floor commercial uses that are accessible at grade.</p> <p>3. Building design, for buildings fronting Tranquille Road, should include a number of separate street front shops with separate entrances.</p>

TRANQUILLE SOUTH - BUILDING MASSING

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Building massing shall create focal points and landmark developments at the entrance to the Tranquille South Area, to be consistent with the vision, goals and policies identified in the North Shore Neighbourhood Plan.</p> <p>2. Building massing shall create pedestrian-oriented developments along street fronts and along major pedestrian thoroughfare routes, specifically along Tranquille Road.</p>	<p>1. Accent focal points at primary sightlines throughout the Tranquille South area.</p>

1.2 - North Shore Development Permit Area

TRANQUILLE SOUTH - BUILDING HEIGHT

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Building height minimum is two (2) storeys. 2. Building height maximum is two (3) storeys. 	<ol style="list-style-type: none"> 1. Building height should be constructed as a focal point building, showcasing the importance of the Tranquille Commercial District consistent with the vision, goals and policies of the North Shore Neighbourhood Plan. 2. Building height maximum can be increased, up to six (6) storeys, with the addition of a combination of market housing and non-market housing units, public art, landscaped and pedestrian-focused plazas, green building design standards and eco-friendly storm water systems and roofs.



Examples of Infill Development in the Tranquille South Area; Source: NDLea Consultants Ltd.



Streetscape Example: Pedestrian Friendly, Visually Appealing; Mixed Uses
 Artistic Image: See Sources and References Section
 Drawing: Carla Stewart

1.2 - North Shore Development Permit Area

BROCK SHOPPING CENTRE

AREA SPECIFIC GUIDELINES

JUSTIFICATION

The Brock Shopping Centre is the only main commercial focal point for one of the more heavily populated areas of the city. Designed as an auto-oriented shopping mall, the site comprises single-storey buildings sited to discourage pedestrian use with a predominant feature of hard surfaces including concrete and pavement. Community input during the preparation of the North Shore Neighbourhood Plan identified the Brock Shopping Centre as a potential village centre for Brocklehurst where pedestrian-oriented development could be located on site along with an increase in the number of neighbourhood commercial conveniences and with the introduction of multiple family residential uses. Achieving a village centre for Brocklehurst requires high design standards along with the use of innovative, creative and eco-friendly building and site design concepts that accommodate pedestrian activities.

GUIDELINES

The following Development Permit Guidelines are applicable to the area shown on the Map below and are intended to supplement the North Shore Development Guidelines as well as the city-wide Multiple Family Development Permit Area Guidelines. Development Permits issued in this area shall be in accordance with the following Guidelines:



1.2 - North Shore Development Permit Area

BROCK SHOPPING CENTRE - SITE LAYOUT AND PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Parking shall not be located in any front or side yard or along any street frontage. 2. Site layout and building design, for those properties fronting Tranquille Road and Desmond Street shall have building frontage extend along the street frontage. 3. Drive-thru aisles and idle lanes shall be located at a minimum setback of 30 m from any front and/or side street property line or any property line abutting a street. 	<ol style="list-style-type: none"> 1. Parking locations within front and side yards and along street frontages can occur where a minimum of a 4 m landscape buffer strip is provided between the parking area and a property line.

BROCK SHOPPING CENTRE - BUILDING SETBACKS

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Front yard setback minimum is 0 m. 2. Side yard and/or side street yard setback minimum is 0 m. 3. Rear yard setback minimum is 0 m. 4. Rear yard setback minimum, where a lot is adjacent to a residential zone with no intervening lane or street, is 2 m. 	

BROCK SHOPPING CENTRE - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Landscaping shall cover a minimum of ten (10) per cent of the total site area, not including the boulevard. 	<ol style="list-style-type: none"> 1. Landscaping minimum can be reduced, up to five (5) per cent of the total site area, with the use of eco-roofs and green infrastructure storm water systems.

1.2 - North Shore Development Permit Area

BROCK SHOPPING CENTRE - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Building design, for those properties fronting Tranquille Road, shall be designed and situated so as to create a landmark or focal point building for the Brock Shopping Centre. Landmark and focal point buildings shall incorporate significant architectural styles and features to distinguish them from adjacent properties and to create a positive impact on the landscape and in conjunction with the pedestrian movements and activities in the area.</p>	<p>1. Building design should provide a continuous street frontage to encourage a pedestrian-oriented community.</p>
<p>2. Building design shall complement, enrich and recognize the vision, goals and policies identified for the Brock Shopping Centre in the North Shore Neighbourhood Plan.</p>	<p>2. Building design should provide ground floor commercial uses that are accessible at grade.</p>
<p>3. Building design shall contribute to a lively and useable pedestrian scale over and above auto-oriented designs.</p>	<p>3. Building design should include a number of separate street front shops with separate entrances.</p>

BROCK SHOPPING CENTRE - BUILDING MASSING

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Building massing shall create pedestrian-oriented developments along street fronts and along major pedestrian thoroughfare routes, specifically along Tranquille Road.</p>	<p>1. Building massing shall accent focal points at primary sightlines along Tranquille Road.</p>

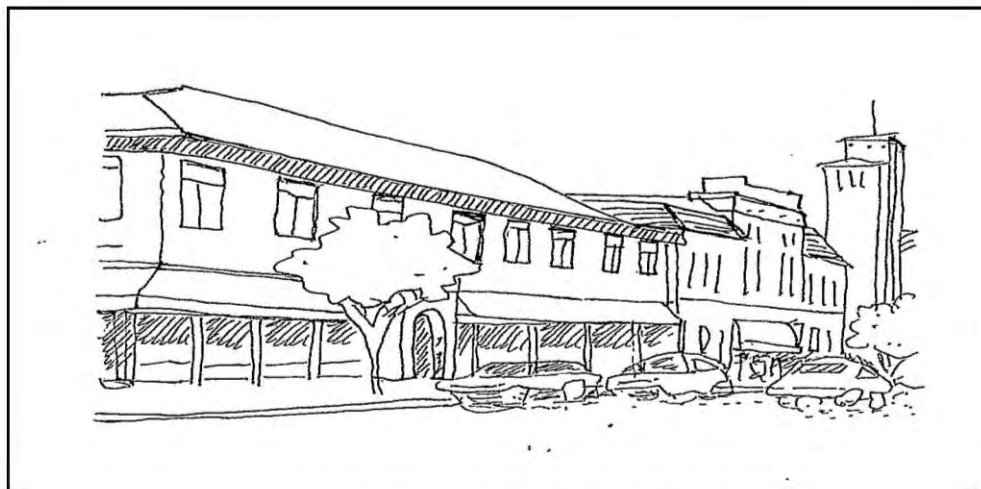
1.2 - North Shore Development Permit Area

BROCK SHOPPING CENTRE - BUILDING HEIGHT

Required Elements	Recommended Elements
<p>1. Building height minimum is two (2) storeys.</p> <p>2. Building height maximum is two (2) storeys.</p>	<p>1. Building height should be constructed as a landmark or focal point building, showcasing the importance of the Brock Shopping Centre, consistent with the vision, goals and policies of the North Shore Neighbourhood Plan.</p> <p>2. Building height maximum can be increased, up to four (4) storeys, with the addition of a combination of market housing and non-market housing units, public art, landscaped and pedestrian-focused plazas, green building design standards and eco-friendly storm water systems and roofs.</p>



Examples of Infill Development in the Brock Shopping Centre; Source: NDLea



Streetscape Example: Pedestrian Friendly, Visually Appealing; Mixed Uses
 Artistic Image: See Sources and References Section
 Drawing: Carla Stewart

1.2 - North Shore Development Permit Area

ADDITIONAL AREA SPECIFIC

DEVELOPMENT PERMIT GUIDELINES

JUSTIFICATION

Airport Entry Corridor:

The Airport Entry Corridor is a significant gateway to the North Shore from the Kamloops Airport and from the agricultural lands and proposed destination resort to the west. Vacant land, remnants of heavy industrial uses and a mix of residential densities are scattered along the corridor. Community input during the preparation of the North Shore Neighbourhood Plan identified the Airport Entry Corridor as one of the most significant entry points to the North Shore that, when developed, should showcase the North Shore with buildings that portray an attractive and positive image and site layout. Achieving this requires design standards and the use of creative building and site design concepts.

8th Street Corridor:

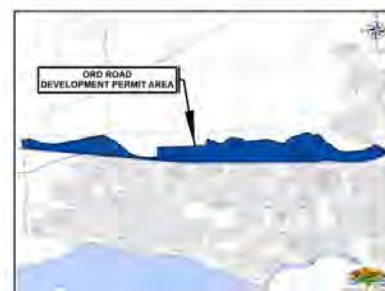
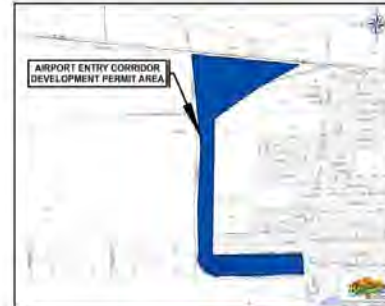
The 8th Street Corridor is a significant access point between the North Shore, Batchelor Heights, Westsyde, the Kamloops Indian Band and the nearby Yellowhead Highway. The current function of the 8th Street Corridor was established through an existing residential neighbourhood with a gradual transition toward commercial uses that are auto-oriented. Community input during the preparation of the North Shore Neighbourhood Plan identified the 8th Street Corridor as an area suited to auto-oriented uses but that when developed, also takes into consideration the high pedestrian traffic that exists, by increasing landscape buffer areas and by paying more attention to building design. Achieving this requires design standards and the use of creative site design.

Halston Entry Corridor:

The Halston Entry Corridor is the eastern entrance point for the North Shore with concentrations of medium industrial uses, varied commercial activities, vacant land and residential development. Community input during the preparation of the North Shore Neighbourhood Plan identified the Halston Entry Corridor as a significant area that, when developed, should project a positive image for the North Shore both in building design and in site layout. Achieving this requires design standards along with the use of creative site design.

Ord Road:

The Ord Road Corridor is located parallel to a significant railway line and is geographically separated from most of the North Shore. Comprising a variety of land uses such as single family residential, mobile home parks, heavy industrial uses, recycling depots and small auto-oriented uses, Ord Road has long been an area of transition devoid of one cohesive identity. Community input during the preparation of the North Shore Neighbourhood Plan identified the Ord Road Corridor as an area for service commercial and light industrial uses. Achieving this requires design standards to ensure eco-friendly and community appropriate development takes place.



1.2 - North Shore Development Permit Area

AIRPORT ENTRY CORRIDOR

AREA SPECIFIC GUIDELINES

AIRPORT ENTRY CORRIDOR SPECIFICATIONS - SITE LAYOUT AND PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Drive-thru aisles and idle lanes shall be located at a minimum setback of 30 m from any front and/or side street property line or any property line abutting a street. Drive -thru aisles and idle lanes shall be located a minimum setback of 2 m from any residential area.</p> <p>2. Parking areas located within front yards and along street frontages shall be situated with a minimum of a 3 m landscape buffer strip between the parking area and the property line.</p>	<p>1. Parking areas located within front yards and along street frontages can be situated with a minimum of a 2 m landscape buffer strip between the parking area and the property line where eco-friendly storm drainage systems and/or green roofs are used in the site and building design.</p>

AIRPORT ENTRY CORRIDOR SPECIFICATIONS - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Airport Entry Corridor in the North Shore Neighbourhood Plan.</p> <p>2. Building height maximum is two (2) storeys.</p> <p>3. Building design shall contribute to a lively and useable pedestrian scale over and above auto-oriented designs.</p> <p>4. For those buildings fronting Tranquille Road, shall be designed and situated so as to create a landmark or focal point building to reflect a major entrance to the North Shore and the city. Landmark or focal point buildings shall incorporate significant architectural styles and features to distinguish them from adjacent properties and to create a positive impact landscape and in conjunction with a pedestrian environment.</p>	<p>1. Building massing should avoid long, continuous and blank vertical surfaces throughout the entire development.</p> <p>2. Building design should provide ground floor commercial uses that are accessible at grade.</p>

AIRPORT ENTRY CORRIDOR SPECIFICATIONS - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Landscaping shall cover a minimum of ten (10) per cent of the total site area, not including the boulevard.</p> <p>2. Auto-oriented, vinyl, back-lit podium or free standing signs shall not be permitted.</p>	<p>1. Landscaping minimum can be reduced, up to five (5) per cent of the total site area, with the use of eco-roofs and green infrastructure storm water systems.</p>

1.2 - North Shore Development Permit Area

8TH STREET CORRIDOR

AREA SPECIFIC GUIDELINES

8TH STREET CORRIDOR SPECIFICATIONS - SITE LAYOUT AND PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Parking areas located within front yards and along street frontages shall be situated with a minimum of a 3 m landscape buffer strip between the parking area and the property line. 2. Drive-thru aisles and idle lanes shall be located adjacent to rear alleys and away from any street frontage. 	

8TH STREET CORRIDOR SPECIFICATIONS - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Building design shall consider the high level of pedestrian activity along this auto-oriented corridor. 2. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the 8th Street Corridor area in the North Shore Neighbourhood Plan. 3. Building height maximum is two (2) storeys. 	<ol style="list-style-type: none"> 1. Podium or free standing signs can occur where height and area are limited to a pedestrian scale and where natural materials and directional lighting is used, in conjunction with landscaping. 2. Building height maximum can be increased to four (4) storeys with the addition of market and non-market housing, a green building design standard and eco-friendly storm water systems.

8TH STREET CORRIDOR SPECIFICATIONS - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
<ol style="list-style-type: none"> 1. Landscaping shall cover a minimum of five (5) per cent of the total site area, not including the boulevard. 2. Podium and free standing signs can occur where height and area are limited to what is appropriate for a local road standard (50 km/hr) and where natural materials and directional lighting is used, in conjunction with landscaping. 	<ol style="list-style-type: none"> 1. Landscaping minimum can be reduced, up to three (3) per cent of the total site area, with the use of eco-roofs and green infrastructure storm water systems.

1.2 - North Shore Development Permit Area

HALSTON ENTRY CORRIDOR

AREA SPECIFIC GUIDELINES

HALSTON ENTRY CORRIDOR SPECIFICATIONS - SITE LAYOUT AND PARKING

<u>Required Elements</u>	<u>Recommended Elements</u>
1. Drive-thru aisles and idle lanes shall be located at the rear of the property setback from any front and/or side street property line or any property line abutting a street.	

HALSTON ENTRY CORRIDOR SPECIFICATIONS - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
1. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Halston Entry Corridor in the North Shore Neighbourhood Plan.	1. Building height maximum can be increased to four (4) storeys with the addition of market and non-market housing, the use of eco-roofs and green infrastructure storm water systems.
2. Building height maximum is two (2) storeys.	

HALSTON ENTRY CORRIDOR SPECIFICATIONS - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
1. Landscaping shall cover a minimum of ten (10) per cent of the total site area, not including the boulevard.	1. Landscaping minimum can be reduced, up to five (5) per cent of the total site area, with the use of eco-roofs and green infrastructure storm water systems.

1.2 - North Shore Development Permit Area

ORD ROAD CORRIDOR

AREA SPECIFIC GUIDELINES

ORD ROAD CORRIDOR SPECIFICATIONS - SITE LAYOUT AND PARKING

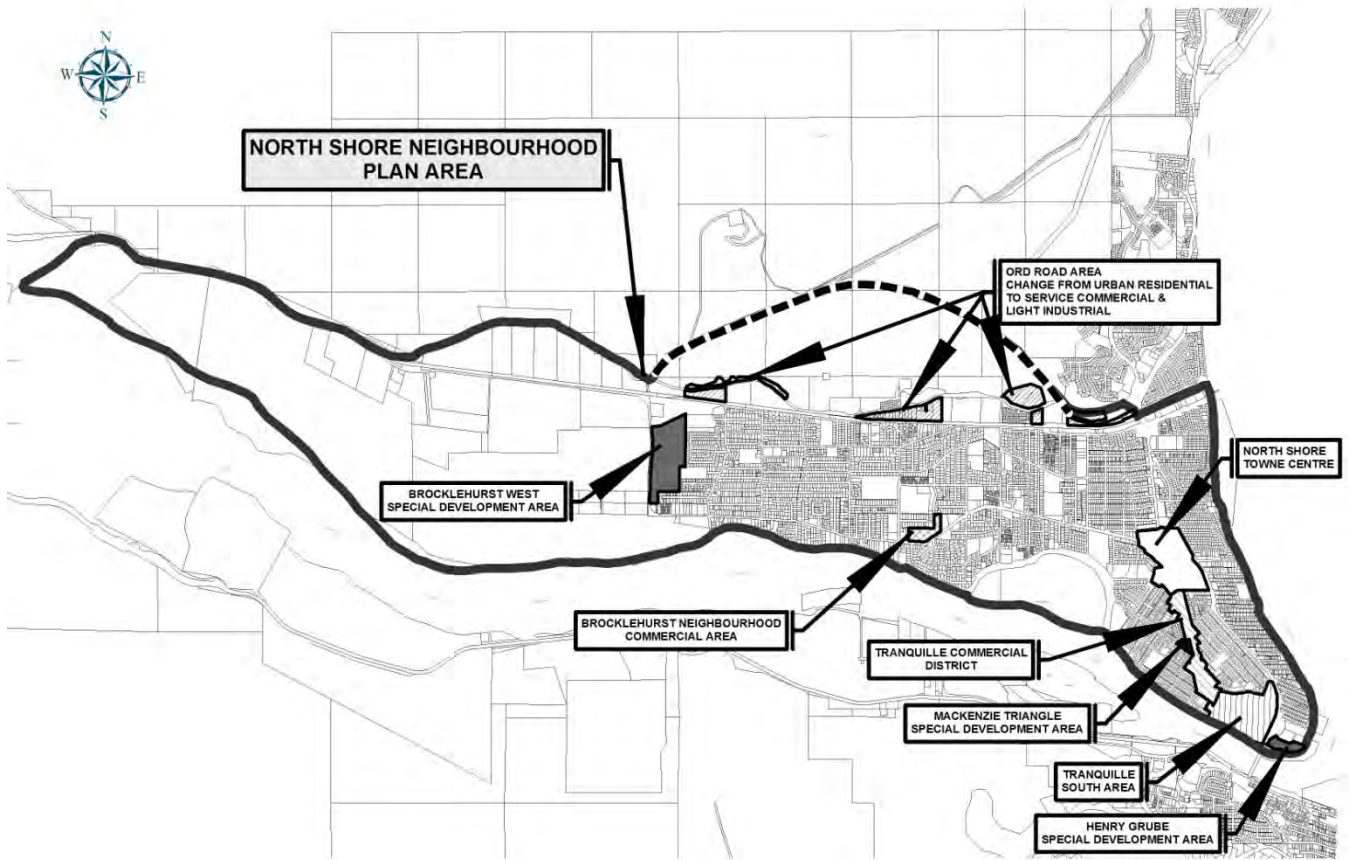
<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Site layout shall be accompanied by the submission of a geotechnical report and assessment that identifies any potential hazard due to rock fall from the steep slopes along the north side of Ord Road, west of McLean Street, and that also provides recommendations for mitigation of any potential rock fall hazard that may exist because of the steep slopes.</p>	<p>1. Site and parking layout should reflect the auto-oriented nature of the Ord Road Corridor.</p>

ORD ROAD CORRIDOR SPECIFICATIONS - BUILDING DESIGN

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Building design shall compliment, enrich and recognize the vision, goals and policies identified for the Ord Road Corridor in the North Shore Neighbourhood Plan.</p>	

ORD ROAD CORRIDOR SPECIFICATIONS - LANDSCAPING, SIGNAGE AND GREEN TECHNOLOGY

<u>Required Elements</u>	<u>Recommended Elements</u>
<p>1. Landscaping shall cover a minimum of ten (10) per cent of the total site area, not including the boulevard.</p> <p>1. Podium and free standing signs can occur where height and area are limited to what is appropriate for a local road standard (50 km/hr) and where natural materials and directional lighting is used, in conjunction with landscaping.</p>	<p>1. Landscaping minimum can be reduced, up to five (5) per cent of the total site area, with the use of eco-roofs and green infrastructure storm water systems.</p>



1.3 WESTSYDE CORE DEVELOPMENT PERMIT AREA

Application: Category (e) Form and character of intensive residential development.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: Westsyde Core Development Permit Area is the area shown shaded grey on the attached map.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations,
- external renovations which do not affect the form and character of the building, or
- single or two family dwellings.

Implementation:

In accordance with Bylaw No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Justification:

The Westsyde Shopping Plaza/Secondary School area is the primary retail commercial and institutional/recreational centre in Westsyde. Intensive land uses including higher density residential are considered more appropriate in proximity to major services and amenities. While the plaza and the school provide the necessary support, the designated area borders an established neighbourhood predominantly consisting of single family dwellings with some two family houses. The objective of the designation is to ensure that new development in the area is compatible with the form and character of the existing neighbourhood.

Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines:

- a) The shape, siting, roofline and exterior finish of buildings should be sufficiently varied to avoid a monotonous appearance.
- b) Continuity of building height with the adjacent neighbourhood is important in maintaining a compatible building scale and mass. New development building heights should generally conform to the heights of adjacent buildings. When new buildings will be taller than their neighbours, they should be internally located on the site such that they are not directly situated across from existing development.
- c) Buildings taller than two storeys should be sited such that shadows and views do not adversely impact the immediate neighbourhood.
- d) Buildings on the exterior of the development abutting local residential roads should be oriented to the adjacent street. Where this occurs, permitted fencing and landscaping shall not exceed 1.0 m in height.
- e) Large expanses of parking should be internally located. Where parking is sited adjacent to a local residential street, it should be provided in smaller clusters no greater than three stalls and screened from view.
- f) All areas not covered by buildings, structures or asphalt shall be landscaped. Internal paths and walkways should be provided to ensure ease of circulation to both on- and off-site services and amenities. The provision of landscaping should consider the use of xeriscaping and native landscape materials.
- g) The principal form of development should be detached dwellings, duplexes and triplexes. Should this be exceeded, the buildings should be located internally on the site and not abutting local residential streets.
- h) Building components housing publicly accessible office and/or customer service areas shall incorporate architectural details such as windows, entry features, distinctive rooflines, and exterior finish materials that distinguish such areas from more utilitarian warehousing, manufacturing, and processing areas. The use of vinyl siding is strongly discouraged in such publicly accessible areas, while the use of natural wood, rock, and stone details is encouraged.
- i) Landscaping islands shall be used to separate large expanses of parking into smaller subsections appropriate to the use. Landscaping islands shall be located at each end of every parking aisle. In the case of longer parking aisles, landscape islands shall be provided in the middle of the aisle, with a maximum separation of 12 to 15 parking stalls between landscape islands.
- j) Site plans must demonstrate the capacity for safe and efficient movement of people and traffic on the site. Site design must include pedestrian walkways along building frontages, as well as continuous pedestrian connections between walkways along building frontages, and on neighbouring sites. Pedestrian walkways shall be distinguished from driving surfaces by using varied treatments and by raising walkways to curb level.
- k) Entrances to buildings shall be located to maximize pedestrian safety.

- l) Raised curbs shall define entry driveways.
- m) Outdoor storage areas, garbage bins, or loading/unloading areas shall be situated in a side or rear yard in order to minimize exposure of storage areas to public roadways.
- n) All areas not covered by buildings or structures and not required for parking, loading, storage, assembly, processing, or manufacturing shall be suitably landscaped. The provision of landscaping shall consider the use of xeriscaping and native landscaping materials.
- o) Landscape design shall prioritize security and pedestrian safety. The use of Crime Prevention through Environmental Design (CPTED) principles such as enhancing lighting and minimizing dark areas, encouraging opportunities for natural surveillance, and using visual cues to control access to the site should be considered when designing landscaping.
- p) External lighting shall be incorporated into building and landscaping design.
- q) Landscape plans prepared and sealed by a landscape architect must be included in Development Permit applications; Development Permit approval will be contingent upon submission of security in the amount of 125% of the estimated value of landscape work and British Columbia Society of Landscape Architects (BCSLA) Schedules of Assurance (Schedules L-1 and L-2)."

1.4 PINEVIEW VALLEY DEVELOPMENT PERMIT AREA

Application: Category (e) Form and character of intensive residential development.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: Pineview Valley Development Permit Area is the area shown shaded grey on the attached map.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations,
- external renovations which do not affect the form and character of the building, or
- single or two family dwellings.

Implementation:

In accordance with Bylaw No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Justification:

The Pineview Valley area is a large tract of rezoned land with a high proportion of multiple family residential development. As development is expected to occur over a five to ten year time frame, development standards may change during this period. The objective of the designation is to ensure that new development in the area is compatible with the form and character of the adjacent single family neighbourhood and the linear park.

Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines:

- a) The shape, siting, roofline and exterior finish of buildings should be sufficiently varied to avoid a monotonous appearance.

- b) Continuity of building height with the adjacent neighbourhood is important in maintaining a compatible building scale and mass. New development building heights should generally conform to the heights of adjacent buildings. When new buildings will be taller than their neighbours, they should be internally located on the site such that they are not directly situated across from existing development.
- c) Buildings taller than two storeys should be sited such that shadows and views do not adversely impact the immediate neighbourhood.
- d) Buildings on the exterior of the development abutting local residential roads should be oriented to the adjacent street. Where this occurs, permitted fencing and landscaping shall not exceed 1.0 m in height.
- e) Large expanses of parking should be internally located. Where parking is sited adjacent to a local residential street, it should be provided in smaller clusters no greater than three stalls and screened from view.
- f) All areas not covered by buildings, structures or asphalt shall be landscaped. Internal paths and walkways should be provided to ensure ease of circulation to both on- and off-site services and amenities. The provision of landscaping should consider the use of xeriscaping and native landscape materials.
- g) The principal form of development should be detached dwellings, duplexes and triplexes. Should this be exceeded, the buildings should be located internally on the site and not abutting local residential streets.



PINEVIEW VALLEY DEVELOPMENT PERMIT AREA

1.5 MT. DUFFERIN DEVELOPMENT PERMIT AREA

Application: Category (e) Form and character of intensive residential development.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: Mt. Dufferin Development Permit Area is the area shown shaded grey on the attached map.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations,
- external renovations which do not affect the form and character of the building, or
- single or two family dwellings.

Implementation:

In accordance with Bylaw No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Justification:

The Mt. Dufferin area is a large tract of rezoned land with a significant proportion of multiple family residential development. As development is expected to occur over a long time frame, development standards may change. The objective of the designation is to ensure that new development in the area is compatible with the standards and principles established through the Mt. Dufferin Plan process and is in keeping with the form and character of the adjacent neighbourhood and the city-wide park.

The Mt. Dufferin Land Use Plan identified several significant natural features in the Development Permit Area worthy of special attention and consideration, including prominent knolls, view corridors, vegetation and mature stands of trees. The Development Permit Area Guidelines attempt to strike a balance between the negotiated density expectations and the importance and value of these natural features to the city-wide park and the existing neighbourhood.

Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines:

- a) The Development Permit Area is located within the urban/wildland interface and hazard area. Applications for development are subject to the Urban/Wildland Interface policies.
- b) The layout and design of access roads, internal streets, building sites and overall grading patterns should demonstrate an effort to preserve and protect significant natural features, including prominent knolls, vegetation, view corridors and mature stands of trees.
- c) The following information and drawings may be required upon submission of a Development Permit application:
 - Site photographs and/or drawings showing the subject area and proposed buildings in relation to the surrounding area.
 - A grading plan showing existing and proposed grades.
 - A tree inventory plan indicating existing tree cover and trees proposed to be removed, retained or replaced.
- d) The shape, siting, roofline, height, facade and exterior finish of buildings should be sufficiently varied to avoid a monotonous appearance.
- e) All areas not covered by buildings, structures or asphalt shall be landscaped. Internal paths and walkways should be provided to ensure easy circulation to both on- and off-site services and amenities. The provision of landscaping should consider the use of xeriscaping and natural landscape materials.
- f) Parking:
 - Parking within the individual unit will be encouraged.
 - Where surface parking is planned, it should be provided in small clusters no greater than three stalls and should be visually separated by curbing, lighting, directional signage, landscaping or any combination of these elements.
- g) Large areas of building walls should be avoided. Where extensive wall areas are planned, this should be visually relieved by a combination of windows, colour, material, textures and mature landscaping.
- h) Site staking may be required indicating the proposed location of:
 - corners of buildings,
 - access and internal roads, and
 - rooflines of buildings.

1.6 SOUTH THOMPSON SILT BLUFFS RED ZONE DEVELOPMENT PERMIT AREA

Application: Category (b) Protection of development from hazardous conditions.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: South Thompson Development Permit Area is the area shown shaded grey on the attached map.

Exemptions: Applications for the following shall not be required to apply for a Development Permits:

- Internal renovations to an existing building or structure, or
- New buildings or structures less than 10 m² in size.

Justification:

Detailed geotechnical analysis and study have identified significant and extensive silts in and adjacent to the bluffs bordering the South Thompson River. The hazardous zone encompasses an area extending approximately from Rose Hill Road to Campbell Creek at the eastern boundary of the City. Substantial harm and/or damage could result should development occur in this area. The objective of the Development Permit Area designation is to regulate development in the area in such a manner as to protect it from the identified hazardous condition.

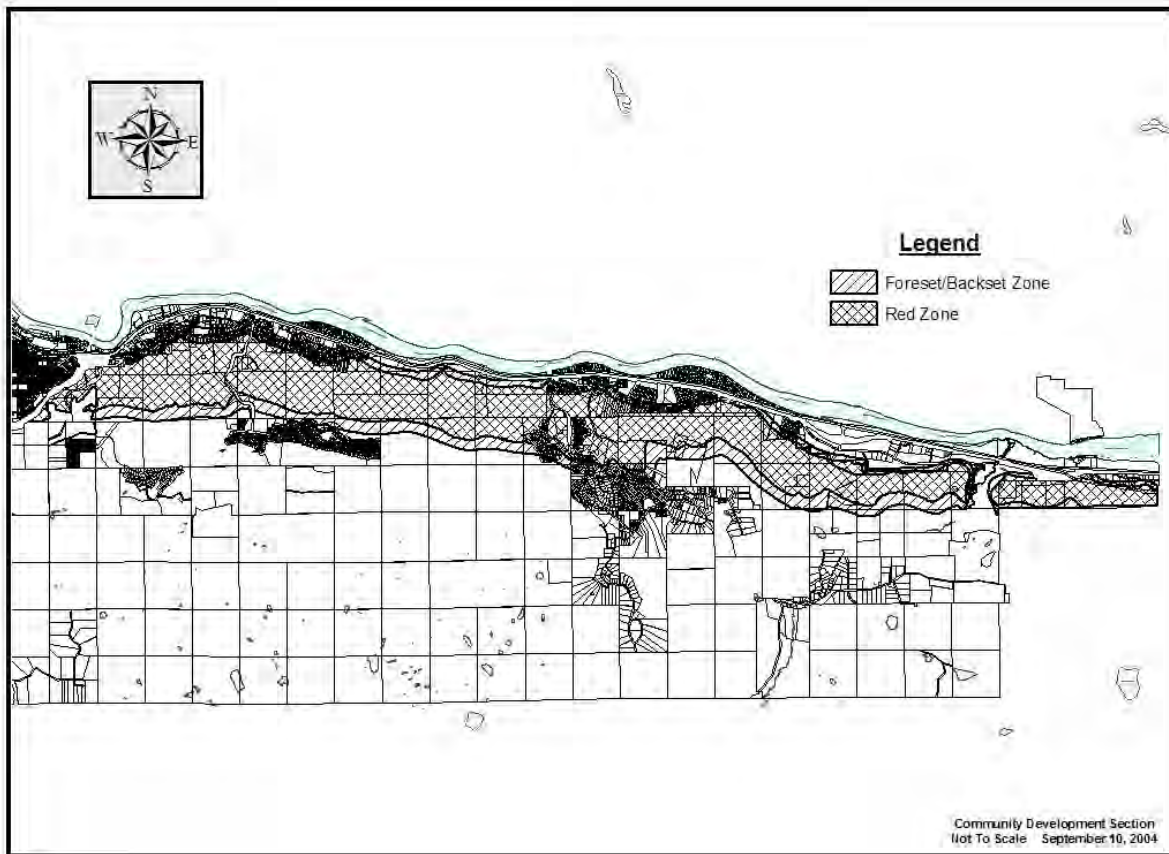
Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines:

- a) No development shall be permitted in the area designated as the Red Zone on the attached map except that accessory structures and buildings may be permitted on parcels of land having an existing residential dwelling where it can be demonstrated by a professional engineer that the proposed accessory structure or building is able to meet the special conditions required for the development in the Foreset/Backset Zone and that the accessory structure/building is not used as a dwelling unit.
- b) Development may be permitted in the area designated as the Foreset/Backset Zone on the attached map subject to the following special conditions:
 - Every application for development shall be accompanied by a geotechnical report prepared by a professional engineer qualified to practice in the field of geotechnical engineering. The geotechnical report shall:
 - define the extent of all surficial geologic materials and the ground water regime in the area proposed for development;

- define the extent of hazard zones within the area; and
- recommend specific criteria for development such that potential hazards are eliminated or accommodated.
- All development within the Foreset/Backset Zone shall be under the direct supervision of a professional engineer.
- At the option of the City, a "save harmless" covenant shall be registered against each legal parcel.

(5-1-2762)



SOUTH THOMPSON SILT BLUFFS RED ZONE DEVELOPMENT PERMIT AREA

1.7 DALLAS COMMERCIAL CORE DEVELOPMENT PERMIT AREA

Application: Category (e) Form and character of intensive residential development.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: Dallas Commercial Core Development Permit Area is the area shown shaded grey on the attached map.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations, and
- external renovations which do not affect the form and character of the building.

Implementation:

In accordance with Bylaw No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Justification:

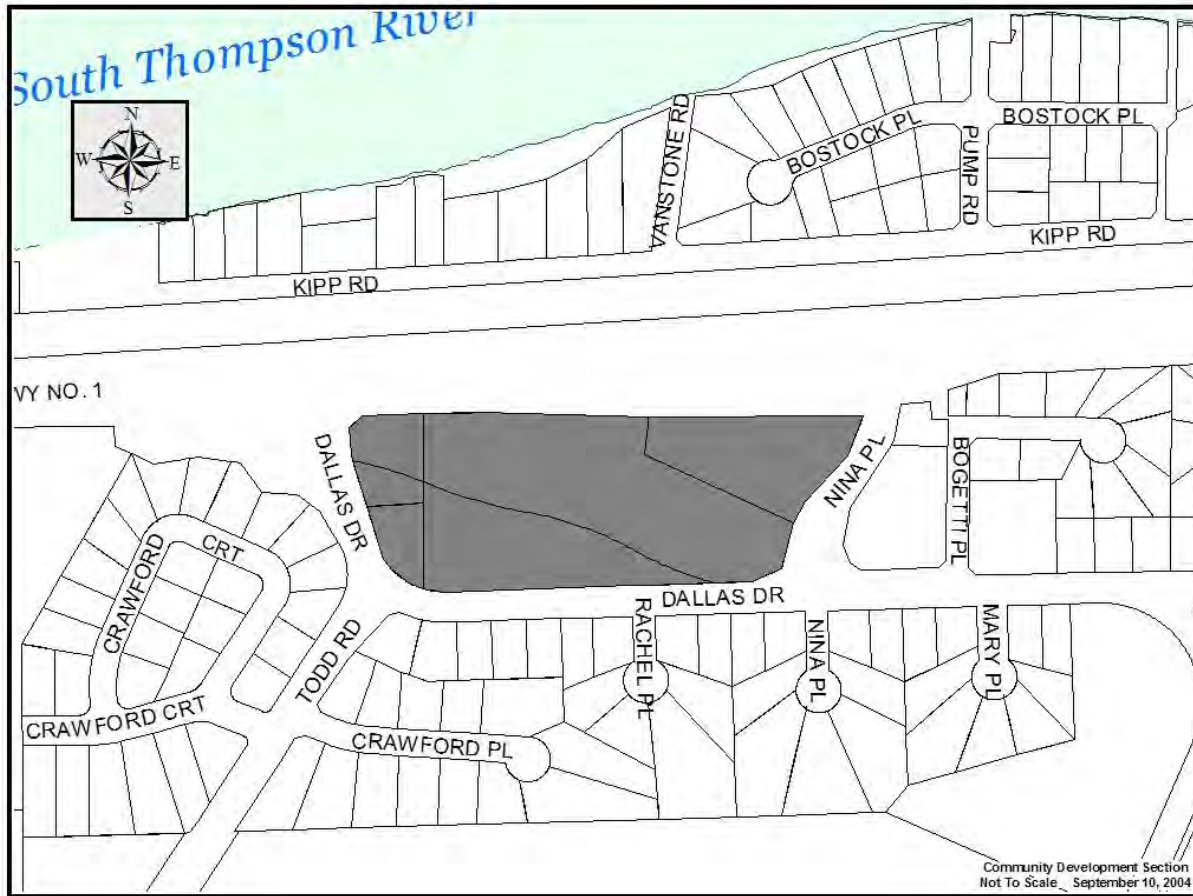
The designated permit area borders an established single family neighbourhood, a service station/convenience store and the East Trans Canada Highway. The site is intended to be developed as a Neighbourhood/District commercial centre servicing Dallas, Barnhartvale, Campbell Creek, areas east beyond the city boundary, as well as the travelling public. The range of Shopping Centre commercial uses which could be concentrated in this location may include retail, personal services, office, convenience and food outlets.

The objective of the Development Permit Area designation is to ensure that the impact of new development in the area is minimized for adjacent residential areas, as well as to ensure an attractive commercial area adjacent to the highway.

Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines:

- a) The site must be developed in a comprehensive manner to ensure the form and character of the building(s) have a common architectural scheme/design.
- b) All principal buildings shall be sited adjacent to the Trans Canada Highway. Satellite buildings may be located adjacent to Dallas Drive to break up large expanses of parking and to provide screening of the parking areas.
- c) Large expanses of parking must be separated into smaller sections by curbing, light standards, signage, landscaping or a combination of these elements. Parking immediately adjacent to Dallas Drive should be provided in smaller clusters no greater than ten stalls.
- d) Lighting shall be directed away from residential uses. No freestanding or animated signs shall be permitted on Dallas Drive. The maximum sign height permitted on Dallas Drive shall not exceed 1.5 m.
- e) Buildings facing residential uses shall be visually relieved by a combination of windows, building materials and textures, and architectural features.
- f) Continuous perimeter landscaping and supplemental landscaping adjacent to the residential areas must be provided. This landscaping is to include street trees of sufficient height to provide effective screening.
- g) Rooflines should relate to adjacent building forms and styles. Varied rooflines must be used to avoid the appearance of a long, flat building facade. Rooftop mechanical equipment must be screened from view by purpose built structures or the inherent form of the roof.
- h) Outdoor storage areas, garbage bins or loading/unloading areas shall be visually screened and landscaped from adjacent residential areas.
- i) Building height should not exceed one storey and the form and character of development should relate to, and appear compatible with adjacent residential development.
- j) Access points shall be located in a manner which does not adversely impact the safe use of the adjoining highway.
- k) Site plan must demonstrate the capacity for safe and efficient movement of people and traffic on the site.
- l) Sidewalks along Dallas Drive and Nina Place extension must be provided.



DALLAS COMMERCIAL CORE DEVELOPMENT PERMIT AREA

1.8 CAMPBELL CREEK INDUSTRIAL PARK DEVELOPMENT PERMIT AREA

Application: Category (e) Form and character of intensive residential development.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: Campbell Creek Industrial Park Development Permit Area is the area shown shaded grey on the attached map.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations,
- external renovations which do not affect the form and character of the building,
- external renovations or additions which are not visible from a public right-of-way, commercial, or residentially zoned property, and
- single or two-family dwellings.

Implementation:

In accordance with Bylaw No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Justification:

The Campbell Creek Industrial Park is located at the eastern gateway to the City adjacent to the Trans Canada Highway, serving as the primary access route. The City's Official Community Plan encourages the preparation of highway corridor guidelines to ensure a high standard of development and further encourages the protection of significant public view corridors.

Campbell Creek is a highly visible tract of land that is appropriate for the development of service commercial and light to general industrial uses. Care must be taken to ensure a positive impression of the City is transmitted to the travelling public. The area has significant exposure as the Trans Canada Highway runs in an east/west direction through the full length of the industrial park area. The environmentally sensitive silt bluffs provide a visually impressive border to the north and south. To provide safe highway access to the future development, a portion of the Trans Canada Highway was elevated to accommodate an overpass. The elevated portion creates additional need to ensure the site development is managed from an aesthetic perspective. The substantial landscaping and screening associated with a major industrial plant (Moly-Cop) at the park's eastern edge establishes the development standard envisaged for the remaining undeveloped parcels in the park. The well-established residential area of Dallas is located along the western boundary of the Development Permit area.

The objectives of the Development Permit Area are:

- To achieve a positive, identifiable, and coordinated image of the Campbell Creek Industrial Park which conveys the impression of high quality development through the implementation of architectural, landscaping, and sign guidelines unique to this neighbourhood;
- To ensure that new development exemplifies the high development standard established by an existing major industrial use (Moly-Cop);
- To maintain the carrying capacity of Dallas Drive;
- To ensure compatibility with the existing residential neighbourhood and with future uses; and
- To serve as a visually appealing entranceway to the City.

Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines:

Commercial Development

- a) Large areas of building walls shall be articulated by a combination of windows, entry features, building materials, textures, architectural features, and landscaping.
- b) Varied rooflines are encouraged to avoid the appearance of long, flat building facades wherever possible.
- c) Rooftop mechanical equipment shall be screened from view by the inherent form of the roof or by purpose built structures constructed from durable all-weather materials.

Industrial Development

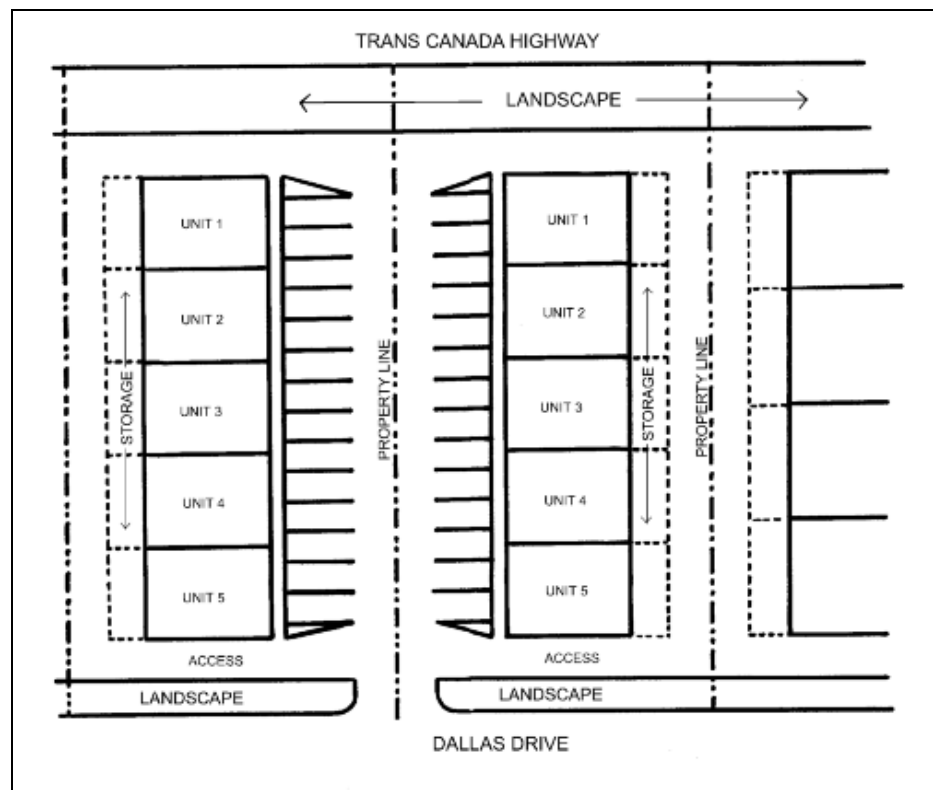
- a) Building components housing publicly accessible office and/or customer service areas are encouraged to incorporate architectural details such as windows, entry features, distinctive rooflines, and exterior finish materials which distinguish such areas from more utilitarian warehousing, manufacturing, and processing areas.

- b) There shall be continuous perimeter landscaping along road frontages. Landscaping will include shrubs and trees of sufficient height and quality acceptable to the City to provide effective screening. Where fencing is proposed along road frontages, the landscaping shall be placed between the fence and the property line.

Commercial and Industrial Development

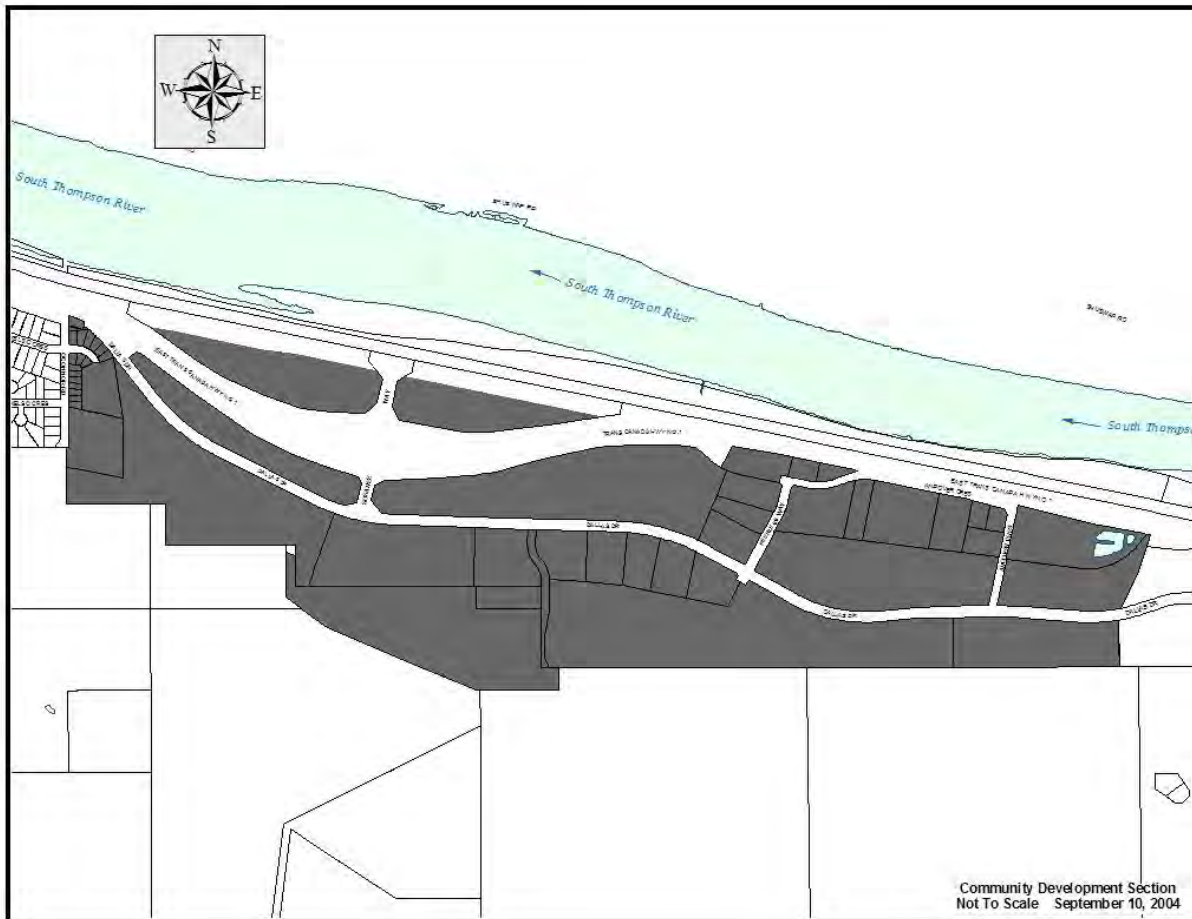
- a) Buildings on corner sites or with double road frontage should be designed to acknowledge the building's visibility from each street through continuity of design, colour, materials, exterior finish, signage, and landscaping.
- b) The use of unfinished and untextured concrete and unfinished metal or aluminum as a final building finish is discouraged. Buildings should be finished in natural, earth tone colours. Extensive use of bold, obtrusive colour is discouraged.
- c) Lots with double road frontage shall be developed such that the longest building elevation is generally perpendicular to the road frontages.
- d) Parking areas are encouraged to be shared between neighbouring properties and should give the appearance of a common parking area.
- e) Storage areas are encouraged to be situated in a side yard in order to minimize exposure of storage areas to the public rights-of-way and appropriately screened.
- f) Access driveways are encouraged to be shared in order to minimize the number of, and to provide maximum spacing (a minimum 30 m on centre) between, entry/exit points along the public right-of-way (see example diagram below).

Example - Double Road Frontage Site Plan (NOT TO SCALE)



- g) Access to and from parking and loading areas shall not impede traffic flows on public roadways.
- h) Landscaping islands shall be used to separate large expanses of parking into smaller subsections appropriate to the use.
- i) Parking, outdoor storage areas, garbage bins, or loading/unloading areas shall be visually screened by landscaping from adjacent residential areas, streets, and the Trans Canada Highway.
- j) Where visual screening is desirable, the use of evergreen shrubs, trees, and other plantings forming an opaque visual barrier is encouraged.
- k) Where security is required, the use of dark coloured chain-link is acceptable as a fencing material.
- l) Fencing along the front property line is discouraged if not essential to site security.
- m) A comprehensively designed fenced and landscaped buffer strip 3 m in width shall be provided to screen the mobile home park from commercial or industrial use. Design and plant materials utilized in this buffer strip should be consistent along the entire length of the mobile home park interface.
- n) There shall be perimeter landscaping within a minimum 3 m buffer along road frontages. Where fencing is proposed along road frontages, the landscaping shall be placed between the fence and the property line.
- o) Installation of raised landscaped berms is encouraged where appropriate to enhance the effect of visual screening. Berms required for on-site drainage control shall be landscaped.
- p) All plant material shall be suited to the local climate, healthy, free of disease and pests, showing good growth characteristics, and shall be No. 1 Grade Canadian grown plant material suited to the site's soil types and moisture availability.
- q) All areas not covered by buildings or structures and not required for parking, loading, storage, assembly, processing, or manufacturing shall be suitably landscaped. The provision of landscaping shall consider the use of xeriscaping and native landscaping materials.
- r) A landscaped buffer strip a minimum of 6 m in width shall be provided parallel to the Trans Canada Highway. Within this buffer strip, massed plantings shall be spaced at minimum 50 m intervals. For the purposes of these guidelines, a massed planting is defined as a grouping of shrubs and trees, one-third of which shall have a minimum height of 3 m at maturity, planted densely yet in accordance with sound landscaping practice and not less than 20 m².
- s) The landscape plan shall be drawn to scale and include footprints of all existing and proposed buildings, parking, loading, driveway, and walkway areas; existing and proposed plantings including common and botanical names, quantity, caliper, root ball size, height at planting, and maturity; height and location of berms; garbage receptacles; a plan of the irrigation system; location of underground utilities; property lines; easements; rights-of-way; drainage storage and/or retention areas; and direction of drainage and catchment systems.

- t) Lighting and illuminated signage shall be oriented so as not to create a glare or reflection on the street, neighbouring buildings, or residential areas. Monument style freestanding signs should be unlit or externally lit.
- u) South of the Trans Canada Highway, freestanding signs shall be no higher than 2 m and shall have a sign area not exceeding 10 m².
- v) No freestanding sign shall contain more than the corporate name, logo, and street number on its face.
- w) For multi-tenant buildings, all lettering and logos on signs shall be of similar size.



CAMPBELL CREEK INDUSTRIAL PARK DEVELOPMENT PERMIT AREA

1.9 MCGILL CORRIDOR DEVELOPMENT PERMIT AREA

Application: Category (e) Form and character of intensive residential development.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: The McGill Corridor Development Permit Area has been designated along McGill Road from Columbia Street through the industrial park. The designated areas include portions of the Thompson Rivers University and the Southgate Industrial Park as shown on the attached map of the McGill Corridor Development Permit Area.

The guidelines apply either generally to all lands within the designated area or they are specifically referenced to the "Main Street" or "Technology Park" land uses designated on the Urban Design Concept Plan map.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations, and
- external renovations that do not change the form and character of the building.

Implementation:

In accordance with Bylaw No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Justification:

In 2002, the City of Kamloops in association with the Thompson Rivers University undertook the McGill Corridor/Southgate Concept Plan. The participants felt that the interface between the Southgate Industrial Park and the University was not well defined and that there was a potential to enhance the relationship through better land use planning. The planning process set out to more clearly define preferred land uses and development options along the McGill Corridor and in Southgate generally. Specifically, the Concept Plan designated the south side of McGill Road between Summit Drive and Hillside Drive "Main Street" with the intent to encourage mixed use development consisting of ground floor commercial uses with residential or office uses above.

The remainder of the McGill Corridor is designated for light industrial and technology related uses.

The Concept Plan included a design strategy, which recognized that design issues would be an important consideration along the McGill Corridor because of the need to treat the corridor as a unified place, to build on the established architectural heritage of the Thompson Rivers University, and to recognize the role of quality streetscapes in successful retail and technology areas.

Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines:

Built Form and Appearance

The McGill Corridor Development Permit Area is a critical interface between the Thompson Rivers University and the Southgate Industrial Park, where design standards are required to ensure long-term land use compatibility.

- a) Buildings within the McGill Corridor shall maintain the high design standards and use similar exterior finish materials as found on buildings on the Thompson Rivers University campus such as the Campus Activity Centre and the Applied Trades and Technology building.
- b) Principal facades should be finished with brick and glazing. The brick should use colour tones that are similar to or compatible with buildings on the TRU campus such as the Campus Activity Centre and the Applied Trades and Technology building.
- c) A minimum of 50% of the ground floor of the principal facade shall be glazed.
- d) The principal facades of buildings shall be oriented to McGill Road.
- e) Awnings and canopies a minimum 1.5 m wide shall be used to provide pedestrian weather protection and add visual interest.
- f) Metal cladding may be used to complement brick finishes.
- g) Secondary facades or accessory buildings may use split face concrete block or stucco painted in a colour tone complementary to the principal facade.
- h) The front setback from McGill Road, between Summit Drive and Hillside Drive/University Drive, should provide a minimum setback of 3 m and a maximum setback of 6 m to promote a relatively continuous street facade.
- i) A minimum of 50% of the frontage shall be occupied by buildings.

- j) The Urban Design Concept Plan identifies two major gateway nodes and one minor gateway node along the McGill Corridor. Corner buildings at these intersections should be designed as landmark corner buildings to identify their strategic location within the Urban Design Concept Plan. Landmark corner buildings should:
- consist of a dominant form that can be carried along the street;
 - occupy a significant terminus of view;
 - consider all street frontages equally and provide equal quality of design, orientation, and detail;
 - respond to intersection focal points and pedestrian orientation with corner setbacks (4.5 m), building articulation, and landscape treatment; and
 - should incorporate peaks, gables, turrets, clock towers, and other distinctive architectural features to emphasize that these intersections act as gateways or focal points to the university and to the adjacent technology park areas.
 - should incorporate peaks, gables, turrets, clock towers, and other distinctive architectural features to emphasize that these intersections act as gateways or focal points to the university and to the adjacent technology park areas.
- k) Buildings shall provide ground floor retail or office uses accessible at grade along the "Main Street" portion of McGill Road between Summit Drive and Hillside Drive/University Drive.

- (5-1-2416) l) Buildings having a minimum height of three storeys shall be encouraged. There is a maximum height of 12 storeys on Thompson Rivers University lands within the Development Permit area.

Vehicle and Pedestrian Circulation

The urban design concept accommodates vehicular traffic in rear yards and encourages the development of "Main Street" as a visually pleasing pedestrian environment.

- a) Forecourts, landscaped islands, and patios are encouraged for commercial and public purposes to enhance the streetscape for pedestrians.
- b) Provide a continuous 3 m wide sidewalk between curb line and the principal facade of the building, designed to complement and enrich the pedestrian character, amenities, and commercial activity along the "Main Street" segment of McGill Road.
- c) In the "Main Street" segment of McGill Road, no parking or circulation roadways shall be provided between the principal facade and McGill Road. Parking shall be located at the rear of the buildings and will be accessed by internal circulation or maneuvering aisles. A continuous major circulation aisle shall be provided at the rear of the buildings to facilitate on-site circulation in the "Main Street" segment of McGill Road. Reciprocal parking agreements may be required between adjoining lots to better facilitate on-site circulation.
- d) Parking areas will be landscaped with planted islands a minimum of 2 m in width. The planted islands will separate major circulation aisles from parking access aisles.
- e) Entrances to buildings will be located to maximize pedestrian safety, particularly in relation to crossing McGill Road.

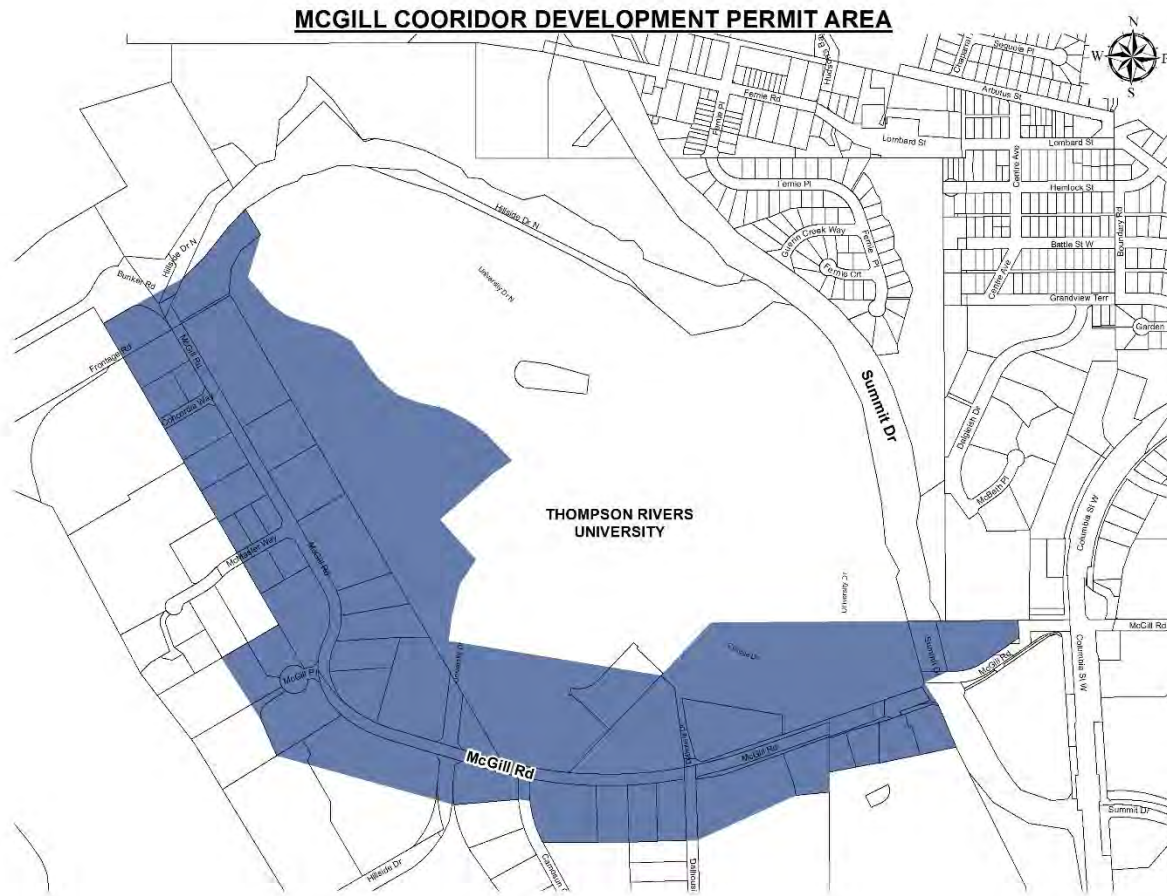
- f) Mid-block entrances to residential buildings will be discouraged, except where they provide access to private outdoor patio space. Access to residential buildings will be encouraged from side yards.

Landscaping

Landscaping is a major visual element that has the ability to provide a unified development along McGill Road. The corridor will be landscaped with street trees and amenity spaces.

- a) The area between the principal facade and McGill Road shall be landscaped with street trees.
- b) Landscape design should complement landscape treatments on the Thompson Rivers University campus, including street trees along McGill Road.
- c) Landscape design should address security and pedestrian safety by enhancing lighting and minimizing dark areas.
- d) Site planning in the "technology park" segment of McGill Road should protect natural landscape elements including existing trees and rock knolls.
- e) Xeriscape and native landscape treatment similar to that used on the Terasen building at the westerly end of McGill Road will be encouraged in the "technology park" portion of McGill Road.
- f) Buildings located on corner lots should include a minimum 4.5 m corner cut or similar pedestrian treatment to expand the sidewalk adjacent to these intersections.

(5-1-2631)



(5-1-2316)

Site Specific Development Permit Area Guidelines

The following development permit guidelines are applicable to Lot A, Plan KAP76624, K.D.Y.D. and are intended to supplement the McGill Corridor Development Permit Area guidelines and the City-wide Multi-Family Development Permit Area guidelines.

Application

Upon application for a subdivision or building permit for lands within the area shown on Map 1, a development permit shall be required. The development permit submission shall detail the following:

- Parking, access, and vehicular circulation;
- Pedestrian circulation;
- Landscaping;
- Building elevations;
- Site layout; and
- Streetscape enhancement.

Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines. The illustrative examples shown as Sketches 1-6 will be used for additional detail and guidance to assist in interpreting the guidelines:

General Design Principles

The City is encouraging adherence to LEED (Leadership in Energy and Environmental Design) standards as a fundamental part of programming, design, life-cycle analysis, and budgeting processes for development. Initiatives may include reduced water consumption, reduced demand on non-renewable resources, lowered energy use, and reduced impacts associated with eco-friendly maintenance, and operational practices.

The following "Green" building design principles, as expressed in the LEED program of the Canada Green Building Council, are to be used, where feasible, to ensure the development is environmentally responsible and sustainable.

- a) Water efficiency in landscaping, reduced water usage features in the commercial and residential units, and reduced wastewater enhancements will be encouraged.
- b) Energy and atmosphere impacts should be minimized through building systems, commissioning, energy performance, CFC reductions in equipment, optimized performance and energy consumption, and ozone protection.
- c) Material and resource impacts should be minimized through recycling, reuse of materials on site, construction waste management, landfill diversion, and use of recycled materials, regional materials, and renewable materials.
- d) Indoor environmental quality should be addressed with effective ventilation, quality management, low emitting materials, systems controls, and daylight and view enhancements.

Built Form and Appearance

- a) All buildings shall be designed to exemplify and utilize Thompson Rivers University (TRU) campus related materials and forms.
- b) All principle facades (i.e. facades facing/having street frontage) shall include brick and have extensive glazing at the ground floor for retail uses.
- c) Awnings shall be incorporated into all ground floor commercial facades oriented to McGill Road and to Summit Drive.
- d) Upper and secondary facades shall incorporate exterior finishes that complement the brick in order to maintain the character. The use of vinyl siding and finished concrete facing shall be discouraged.
- e) Variation of street frontage setbacks shall be used to provide visual interest and to address grade issues, with building forms related to provide continuity along street facades.
- f) Off-street parking will not be permitted in street frontages.
- g) Buildings oriented at the intersection of McGill Road and Summit Drive shall form or incorporate a landmark character feature, and building frontage shall extend along the street frontage. Additional features to create interest and focal points, such as public art, are required.
- h) The streetscape shall be enhanced to encourage pedestrian use by creating wide sidewalks, retail uses at grade, landscape, and planting features. The streetscape shall be of sufficient width so as to be functional.
- i) Brick features shall incorporate complimentary colors to Thompson Rivers University campus colors including, but not limited to, "Light Tweed", "Dark Tweed", and "Kootenay Brown".
- j) Glass and frames for commercial spaces shall complement the Thompson Rivers University campus plan.
- k) Roofing on sloped roofs shall use colors complementing the existing Thompson Rivers University campus colors.
- l) Roofing styles shall be a blend of sloped (Minimum 4:12 pitch) and flat pitched. In all instances the majority of roof pitches shall be sloped.
- m) For buildings greater than three storeys in height, a minimum 3.0 m setback shall be provided for all storeys above the third storey to create a podium. To mitigate for additional building height, higher levels must be stepped back and articulated.

Landscaping

- a) Sidewalk treatment along McGill Road and Summit Drive street frontages shall be in accordance with Paving Option A, as shown on the detailed sketch appended to these guidelines.
- b) Landscaped feature details (e.g. paving patterns, sitting areas, artwork) shall be required at all access point, both vehicle and pedestrian, to the site.
- c) Bus stop locations shall be required along Summit Drive and McGill Road.
- d) The Summit Drive street frontage boulevard space shall be developed as park like green space to create a pedestrian corridor and linkage, complete with shade opportunities.
- e) Landscaped pods and green spaces shall be maximized in public parking areas.

Site Specific Development Permit Area Guidelines

SKYLINE AND ELEVATION OPTION



LANDMARK FEATURE OPTION



Site Specific Development Permit Area Guidelines

**VIEW TO RESIDENTIAL
ENTRY CORRIDOR**



Site Specific Development Permit Area Guidelines

**VIEW TO SUMMIT/MCGILL
INTERSECTION**

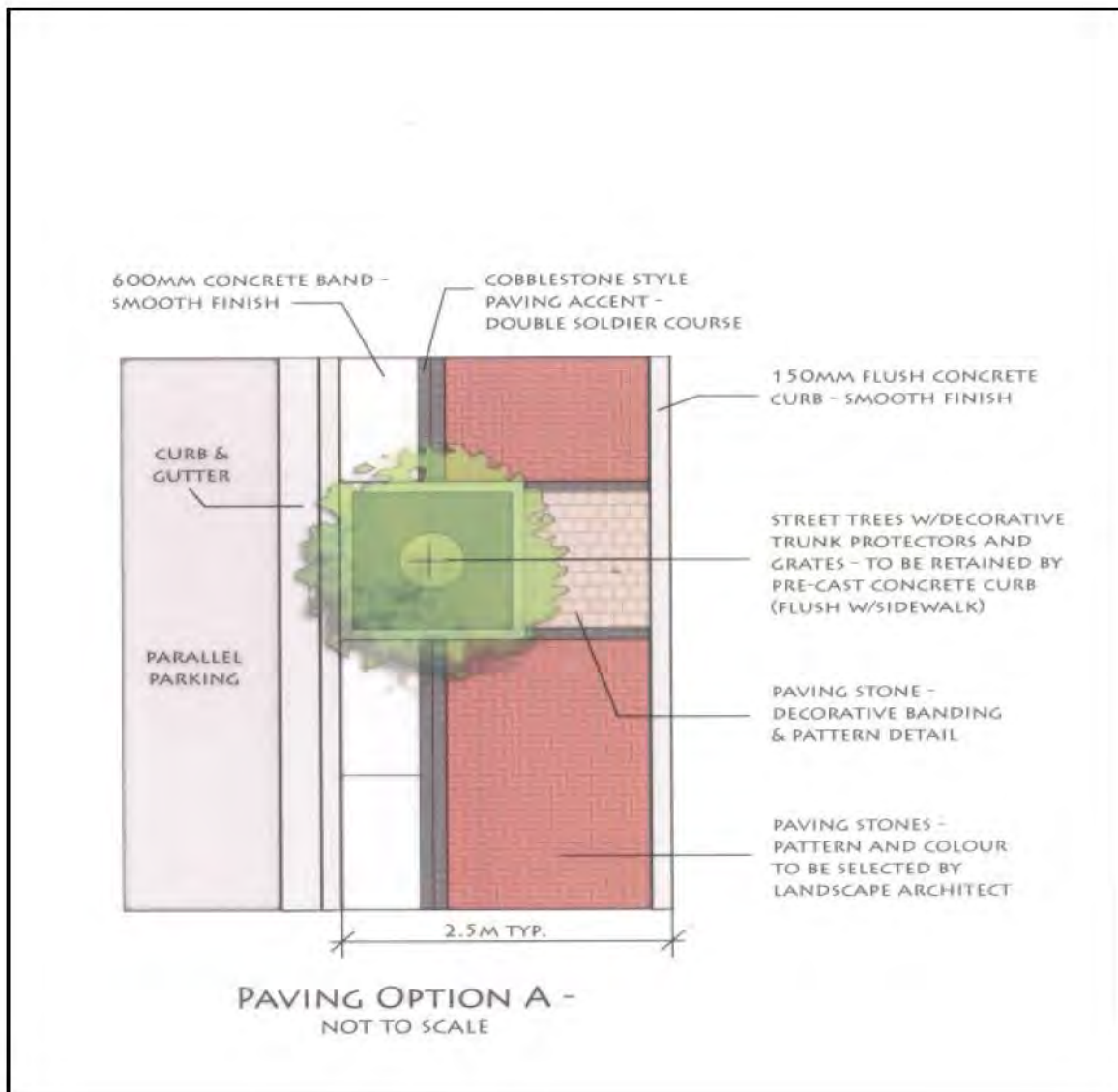


VIEW TO COMMERCIAL ENTRY



Site Specific Development Permit Area Guidelines

SIDEWALK LANDSCAPE TREATMENT OPTION



1.10 NOTRE DAME DRIVE DEVELOPMENT PERMIT AREA

Application: Category (b) Protection of development from hazardous conditions.
(e) Form and character of intensive residential development.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: The Notre Dame Drive Development Permit Area Guidelines are applicable to all properties within the designated Development Permit Area as shown on the following map.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations, and
- external renovations that do not change the form and character of the building, or
- external renovations which do not extend into hazardous areas.

Implementation:

In accordance with Bylaw No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Justification:

The Notre Dame Drive corridor is part of the Southgate Industrial Park and was developed in the 1970s and 1980s to accommodate traditional light industrial uses in large warehouse format buildings. Strong growth in the Southwest Sector has recently generated interest in the redevelopment of properties along Notre Dame Drive for arterial commercial use. Commercial development at the intersection of Notre Dame and Dalhousie Drives, followed by the construction of the Wal-Mart Centre, has been a major event marking the progress of land use change.

The Notre Dame Drive Development Permit Area is established to ensure that the form and character of new development is consistent with principles of good urban design and the goals and objectives of the Official Community Plan. The guidelines promote design standards and concepts which support the redevelopment of an existing industrial area into a vibrant arterial commercial corridor. Attractive designs and efficient site layouts will ensure that new commercial developments evolve as a functional commercial node while minimizing impacts on the adjoining industrial neighbourhood and road network. Guidelines encourage site planning to address the functional relationship between adjoining properties, particularly in terms of vehicular and pedestrian movement.

Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines:

Built Form, Appearance, and Sanitary Landfill

The Notre Dame Drive Development Permit Area is located along a major urban arterial through the Southgate Industrial Park. Attention to building design will ensure a high development standard is maintained throughout the corridor.

- a) The principal building shall address the street frontage in a direct relationship, with main public entrances clearly articulated and identified. Articulation shall be achieved through variation in building planes and materials. The resulting design shall provide visual interest along the street frontage.
- b) The architectural style of buildings containing multiple tenants shall remain cohesive over the entire frontage. Each individual entry point shall be identifiable without detracting from the building's overall appearance.
- c) Canopies and awning style shall complement building architectural style and shall remain consistent over the entire building.
- d) Signage shall be designed as an integral element of the building's facade.
- e) Rooflines shall contain architectural elements that add visual interest. Where flat roofs are used, the roofline shall be no more than 25 m without modulation (e.g. change of elevation, projection, or architectural feature).
- f) Corner lot buildings shall be designed with regard for the high priority of public exposure and shall incorporate specific corner lot design features such as landmark architectural elements.
- g) Corner lot site planning and building design shall consider all street frontages equally and provide equal quality of design, orientation, and detail.
- h) Corner lot buildings shall be separated from street frontages by landscaping and pedestrian walkways. Corner lot setbacks should not contain parking areas or circulation aisles.
- i) The maximum front yard setback for all buildings shall be 23 m.
- j) Building exteriors shall use earth tone colours with complementary accents. Secondary facades shall be in a colour tone that is the same as or complementary to the principal facade.

- k) Guerin Creek Sanitary Landfill: The development permit area also contains portions of the former Guerin Creek Sanitary Landfill. The development permit area guidelines provide an opportunity to address concerns relating to hazardous conditions caused by mitigating gasses and differential soil settlement.
- l) Site planning must include an environmental impact assessment relating to the hazardous conditions imposed on this site by the location of the former Guerin Creek Sanitary Landfill.
- m) Site development plans must implement recommendations of environmental impact assessment to address the hazardous conditions imposed on this site by the location of the former Guerin Creek Sanitary Landfill.

Circulation and Parking

Automobiles will long remain the primary means of access to properties on Notre Dame Drive. Careful site planning is required to address this automobile orientation. Planning must also recognize the arterial function of Notre Dame Drive in the City's overall transportation network.

- a) Loading and other ancillary services shall be located at the rear of the building.
- b) Designated staff parking areas shall be located at the rear of the building.
- c) Pedestrian walkways shall be provided along the length of the building frontage. The walkway shall have a minimum width of 1.5 m. Walkways located adjacent to parking areas shall be 0.6 m wider than the minimum to allow for vehicle overhang. Where walkways have been widened for vehicle overhang, the adjacent parking spaces may be shortened by 0.6 m.
- d) Well defined, continuous pedestrian walkways shall be provided to connect:
 - pedestrian walkways along building frontages,
 - walkways on street frontages, and
 - walkways on neighbouring sites.
- e) Access points (driveways) are to be shared with adjacent development to minimize the number of driveways on Notre Dame Drive.
- f) The driveway spacing shall be a minimum of 70 m measured between the centre of adjacent driveways.
- g) The driveway throat width shall be a minimum of 9 m to a maximum of 12 m.
- h) Driveway locations shall be determined by the Development and Engineering Services Department in accordance with the Notre Dame Drive Access Management Plan.
- i) All entry drives from Notre Dame Drive shall be flanked on both sides by a landscaped area a minimum 2 m wide and extending 4 m into the property.
- j) Raised curbs shall define entry drives.

- k) Parking areas at the rear of the buildings shall be designed with circulation aisles that connect to circulation aisles on adjacent properties. Circulation aisles shall accommodate two-way traffic with a minimum width of 7.3 m to provide access to multiple properties.
- l) Circulation aisles shall be treated (e.g. signs or speed humps) to manage operating speeds every 100 m and where circulation aisles intersect.
- m) Circulation aisles connecting adjoining properties shall be protected through reciprocal agreements.
- n) Bollards or signage are encouraged to define safe pedestrian crossings. Placement of bollards should address turning radii of large vehicles and should also consider snow removal needs.
- o) Public access to the rear of buildings may be required to support and encourage the use of large parking areas in rear yards.

Landscaping

Landscaping is a major visual element that has the ability to provide a unified look along Notre Dame Drive and to contribute to an attractive business setting.

- a) Landscaped areas should include amenity spaces (e.g. benches).
- b) Required landscaping shall be concentrated in the front yard.
- c) High branching deciduous trees shall be incorporated into landscape plans as a design feature. Trees shall be located in the front yard, ensuring there are no conflicts with traffic visibility.
- d) A minimum of 2 m landscaped area shall be developed along street frontages.
- e) Landscaped areas should be located at key points along the pedestrian walkway at the building frontage (e.g. to accentuate building entrances).

Lighting and Signage

Design guidelines for lighting and signage will ensure consistent quality and image along Notre Dame Drive.

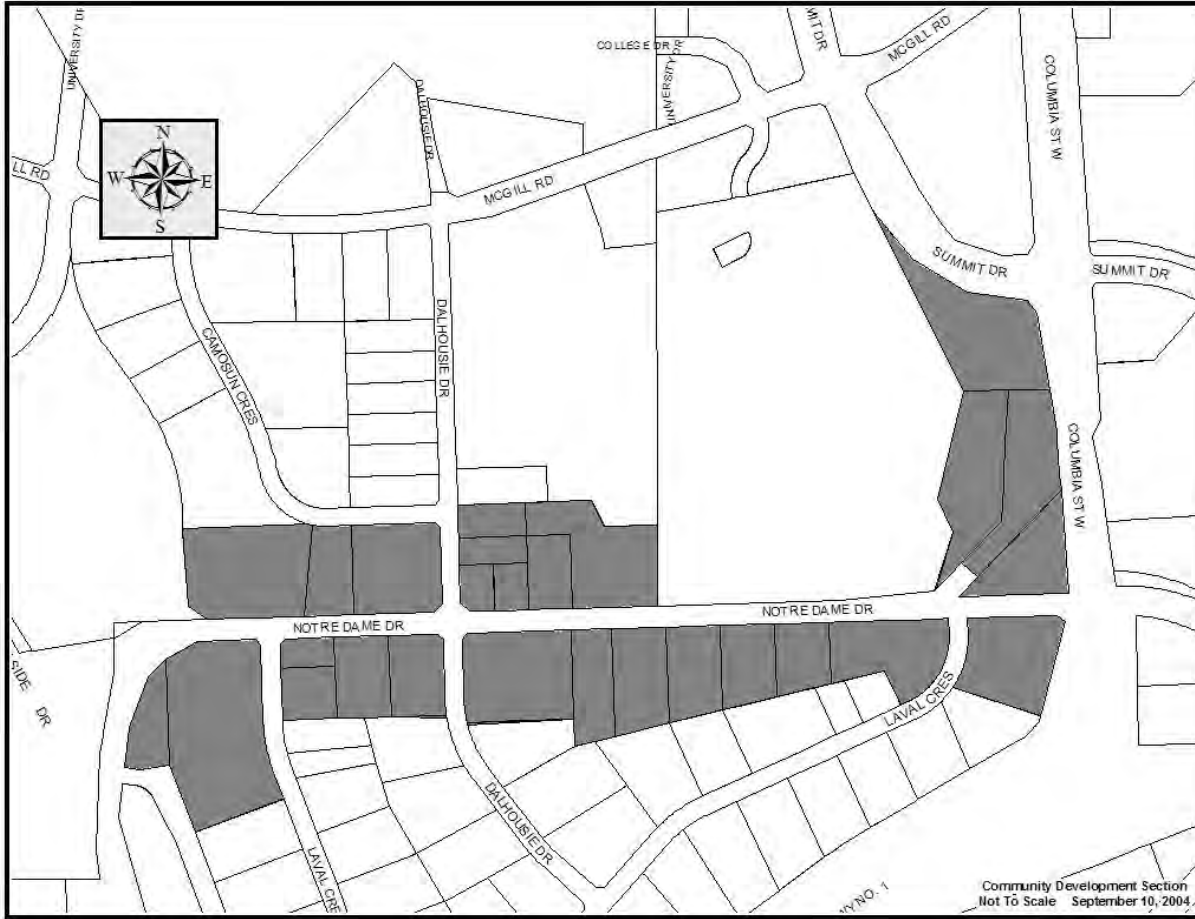
- a) Lighting design should complement the design of the development and provide for a safe, attractive environment for pedestrian and vehicular circulation.
- b) Exterior lighting should not spill over onto adjacent properties and streets.
- c) Floodlighting of buildings or their focal points is encouraged, particularly at major corners.
- d) The preferred design for lighting within front yards is by ground-mounted units located discreetly within landscaped areas. This approach limits the number of light standards in the streetscape zone.
- e) Lighting and light standards in all public areas, including parking lots, should relate to the pedestrian and shall be limited to a maximum 6 m height.

- f) To improve safety, adequate lighting shall be provided throughout developments, particularly in isolated areas.
- g) Signage will be designed as an integral element of the building's facade.
- h) Sandwich board signs are not permitted.
- i) Monument signs or ground signs with solid bases are encouraged rather than pole mounted signs. Ground signs should be integrated in terms of design and size into development projects. They should relate to the architecture, landscaping, and materials. For example, building identification may be incorporated into a planter wall.

Loading, Servicing, and Storage

Servicing functions can be placed in accessible locations and designed to contribute to overall site development standards.

- a) All loading, storage, and servicing areas shall be located in rear yards and be effectively screened with materials that are consistent with the exterior finishes of the principal building.
- b) A storage area screened with solid material is required for garbage collection services. The screened area shall contain sufficient space for at least one standard commercial garbage bin and shall also provide storage areas for ancillary materials such as pallets.
- c) All storage areas shall be finished with a hard surface such as asphalt, concrete, or interlocking stone.
- d) Utility installations shall be integrated into landscaped areas.



NOTRE DAME DEVELOPMENT PERMIT AREA

1.11 SAHALI COMMERCIAL DEVELOPMENT PERMIT AREA

Application: Category (b) Protection of development from hazardous conditions.
(e) Form and character of intensive residential development.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: Sahali Commercial Development Permit Area are applicable to all properties within the designated Development Permit Area as shown on the following map.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations, and
- external renovations that do not change the form and character of the building, or
- external renovations which do not extend into hazardous areas.

Implementation:

In accordance with Bylaw No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Justification:

The Development Permit Area is located on Columbia Street in the lower Sahali neighbourhood. The Sahali Commercial Area has developed as one of the City's major commercial districts. As well, some of the City's major transportation corridors converge upon this area. The City's long-term plan is to ensure the viability of the commercial district while protecting the efficiency of the transportation system.

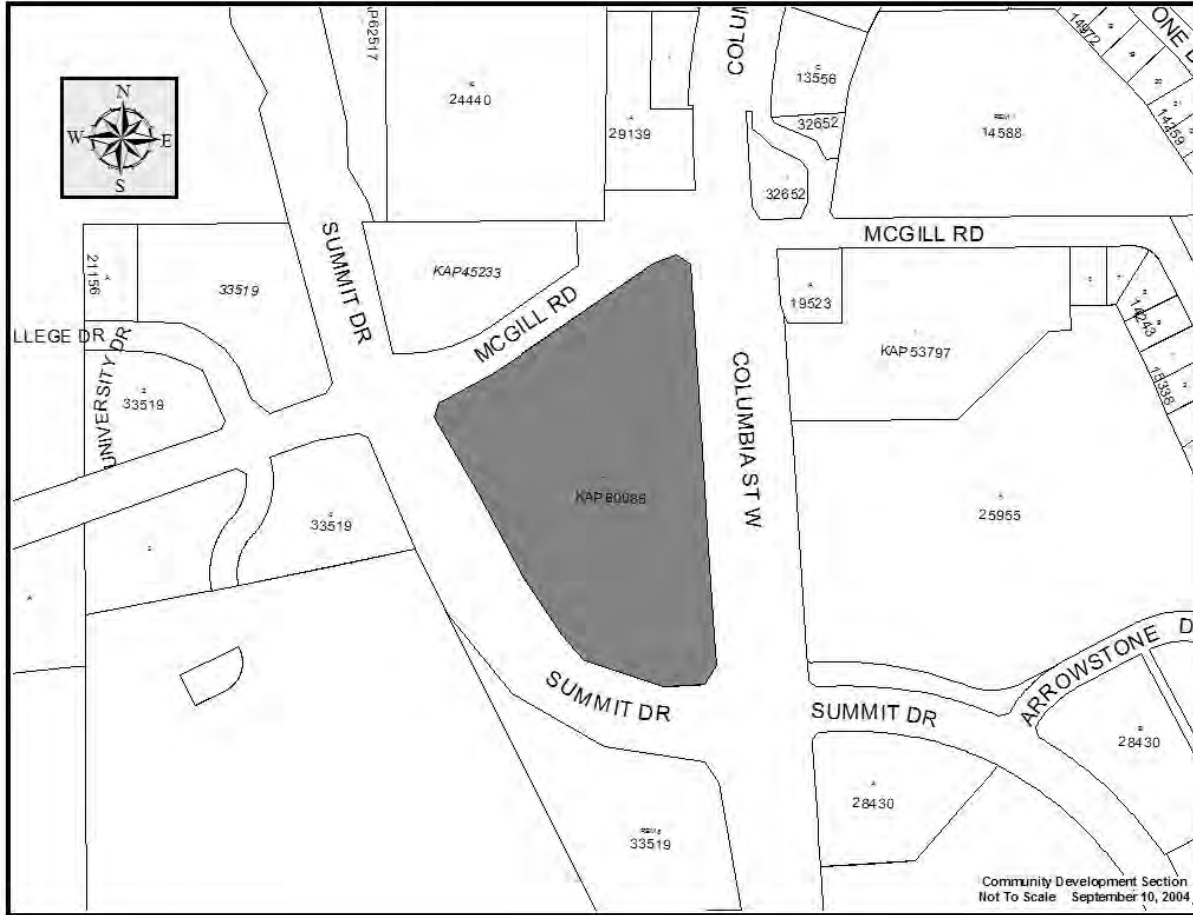
New developments must provide careful site planning to ensure that the transportation network functions at a high standard. New development is also to be of a high development standard to provide a positive impression of this gateway to the City centre and Thompson Rivers University and to reflect the existing development standard of the Sahali Commercial District. The subject properties also link major development cells such as the Thompson Rivers University, Columbia Place, and the Sahali Centre Mall. Development of the subject properties should recognize pedestrian links between existing development cells by providing pedestrian corridors, particularly along McGill Road.

The Development Permit Area also contains a portion of the former Guerin Creek Sanitary Landfill. The Development Permit Area Guidelines provide an opportunity to address concerns relating to hazardous conditions caused by migrating gases and differential soil settlement.

Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines:

- a) Buildings fronting on McGill Road should be comprised of numerous commercial retail units, or the building frontage should give the impression of multiple stores (i.e. the building should not have continuous window or wall areas with no public access).
- b) Blank walls fronting onto or easily viewed from streets should be avoided.
- c) Blank walls (surfaces over 40 m² lacking an architectural feature) should be screened with landscaping. Landscaping must include trees that cover 50% of the blank wall surface (width and height) within five years of planting.
- d) Flat or shed style roof types should be avoided. Where flat roofs are used, the length of any roofline should be no more than 25 m without modulation (e.g. change of elevation, projection or architectural feature).
- e) Rooftop mechanical equipment must be screened from view by purpose built structures or the inherent form of the roof.
- f) There should be continuous perimeter landscaping along all road frontages. Existing mature landscaping must be retained wherever possible.
- g) Large expanses of parking must be separated into smaller sections by curbing, light standards, signage, landscaping or a combination of these elements.
- h) Adjoining properties should make use of reciprocal access agreements to minimize the number of access/egress points. Site plans must demonstrate the capacity for safe and efficient movement of vehicles and people. Site planning must be accompanied by a traffic impact analysis.
- i) Site plans must demonstrate the capacity for safe and efficient movement of people. Sidewalks with adjacent landscaping must be provided along McGill Road and Columbia Street. Sidewalks must also be provided for the safe movement of people through the site.
- j) Outdoor storage areas, garbage bins or loading/unloading areas shall be visually screened or landscaped from adjacent roadways.
- k) Where more than one building is to be constructed on the site, the buildings shall share common architectural features.
- l) Buildings shall be set back to minimize massing impact along streets.
- m) Site planning must include an environmental impact assessment relating to the hazardous conditions imposed on this site by the location of the former Guerin Creek Sanitary Landfill.
- n) Site development plans must implement recommendations of environmental impact assessment to address the hazardous conditions imposed on this site by the location of the former Guerin Creek Sanitary Landfill.



SAHALI COMMERCIAL DEVELOPMENT PERMIT AREA

1.12 ORCHARDS WALK DEVELOPMENT PERMIT AREA

(5-1-2212)

Application: Category (e) Form and character of intensive residential development.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: The Orchards Walk Development Permit Area is applicable to all properties within the designated Development Permit Area as shown on the following map.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations, and
- external renovations that do not affect the form and character of the building, or
- result in increased lot coverage.

Implementation:

In accordance with Bylaw No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Justification:

Orchards Walk is a comprehensively planned mixed-use pedestrian-oriented development that is situated on a highly visible tract of land adjacent to the Trans-Canada Highway East. The objective of the designation is to ensure that the form and character of development within Orchards Walk respects the unique natural qualities of the area, welcomes both residents and visitors alike to the Kamloops region, and reinforces the compact, walkable, pedestrian-oriented vision for the site.

Guidelines

Development Permits issued in this area shall be in accordance with the following guidelines:

(i) General Design Guidelines

- The site must be designed and developed in a comprehensive manner to maximize view corridors towards the river and towards the bluffs and to ensure that streetscape elements such as lighting, furniture, paving treatments and tree plantings display a unified design theme.
- The streets shall be designed developed to provide a safe and pleasant environment for both pedestrians and vehicles. Major road corridors should be designed to safely accommodate on or off-road bike lanes whenever possible.
- The site will accommodate a variety of building types, forms and heights to create visual interest and to appeal to a variety of income levels and age groups.
- The site must create a centrally located pedestrian-oriented village centre that, not only serves the daily needs of adjacent residents, but also serves as a neighbourhood gathering space and focal point for the community.
- Consider opportunities to share parking with adjacent complimentary uses (e.g. meet retail demand during the day and residential demand at night) wherever possible, to reduce space dedicated to parking and to encourage walking within the community.
- Ensure quality materials are used to evoke a sense of permanence and to reflect the dramatic natural setting.
- Encourage development entrance signage that is visually appealing to the travelling public and is consistent with the scale, character, quality and image of the development.
- Encourage signage within the development that is pedestrian-oriented, small scale and is consistent with the architectural character of the building or use that it is advertising. To this end, hanging and window signs will be encouraged within the Village Centre Commercial area with monument style free-standing signs and directional signs located at key entrances and at gateways throughout the development.
- On-site parking will be discouraged in any yard abutting Valleyview Drive or the Grand Boulevard.

(ii) Multiple Family Residential in Single and Two Family Residential Form

Development Permits, including Multiple Family Residential development in Single and Two Family Residential form, shall be issued in accordance with the following guidelines:

- Ensure buildings are oriented toward the street. This can be accommodated by having the main entrance face the internal road or public street.
- Encourage an urban form of housing that strongly related to the street and maximizes the potential of the site by respecting the following guidelines:

- Ensure a minimum lot size of 200 m².
- Permit variations in the front yard setback, from a minimum of 1.5 m to a maximum of 6.0 m (or the foreset line, whichever is greater) to provide transition space from public to private space and to provide opportunity for decks and balconies.
- Ensure a minimum side yard setback of 1.2 m from interior lot lines and 3.0 m from exterior lot lines.
- Ensure a minimum rear yard setback of 3.0 m, except where an attached garage faces a rear lane where the minimum rear yard setback is reduced to 1.5 m.
- Accessory buildings are not permitted in the front yard and shall have a minimum side yard setback of 0.0 m, or 1.2 m where an accessory building accommodates a secondary suite, and a rear yard setback of 1.5 m.
- Permit a maximum site coverage of 60%.
- Permit a maximum building height of 2 storeys, not to exceed 10 m.
- Permit a secondary suite within the principal dwelling or within an accessory building, provided the secondary suite is clearly incidental in size and scale to the primary single family use of the site and meets other design guidelines (e.g. building height, lot coverage).
- Permit fences to a maximum height of 1.0 m in the front yard, except that the maximum fence height in the front yard may be increased to 1.5 m for a length not to exceed 25% of the width of the lot.
- Permit fences to a maximum of 2 m in the side and rear yard, except that the maximum fence height in the side yard shall be 1.0 m where the side yard abuts a street.
- Encourage the creation of usable outdoor amenity space in the form of front and rear courtyards that provide shade in hot summer months and wind protection in cool winter months.
- A minimum of 25% of the site shall be landscaped.

(iii) Multiple Family Residential in Semi-Detached and Townhouse Form

Development Permits, including Multiple Family Residential development in Semi-Detached and Townhouse forms, shall be issued in accordance with the following guidelines:

- Ensure buildings are oriented toward the street which includes an internal access roadway. This can be accomplished by having the main entrance face the street.
- Encourage an urban form of housing that strongly relates to the street and maximizes the potential of the site by respecting the following siting guidelines:

- Permit variations in the front yard setback, from a minimum of 1.5 m to a maximum of 6.0 m (or the foreset line, whichever is greater) to provide transition space from public to private space and to provide opportunity for decks and balconies.
- Ensure a minimum side yard setback of 0.0 m from interior lot lines and 3.0 m from exterior lot lines and 2.4 m between detached buildings.
- Ensure a minimum rear yard setback of 3.0 m, except where an attached or detached garage faces a rear lane where the minimum rear yard setback is reduced to 1.5 m.
- Permit a maximum site coverage of 60%.
- Permit a maximum building height of 3 storeys, not to exceed 12 m.
- Building forms shall ensure visual variety through articulation, modulation, varied setbacks, indentations, building separation, roof modulation, varied roof pitch, finish and the use of various natural building materials (e.g. brick, wood, stone) and colours to reflect the dry Kamloops landscape.
- Facilitate strong street edges by ensuring general consistency in the front yard setbacks within the same street block.
- Parking will be discouraged in the front yard on ground oriented multiple family residential development fronting Valleyview Drive or the Grand Boulevard.
- RV parking shall be intensively screened and landscaped and shall not be located adjacent to Valleyview Drive.
- Common parking areas should be broken down into small clusters and should be visually separated by curbing, lighting, directional signage, landscaping, or any combination of these elements.
- Permit fences to a maximum height of 1.0 m in the front yard, except that the minimum fence height in the front yard may be increased to 1.5 m for a length not to exceed 25 % of the width of the lot.
- Permit fences to a maximum height of 2 m in the side and rear yard except that the maximum fence height in the side yard shall be 1 m where the side yard abut a street.
- Encourage the creation of usable outdoor amenity space in the form of front and rear courtyards that provide shade in hot summer months and wind protection in cool winter months.
- A minimum of 25% of the site shall be landscaped.

iv) Multiple Family Residential in Multiple Storey Walk-Up Apartment Form

Development Permits, including Multiple Family Residential development in Multiple Storey Walk-Up Apartment form, shall be issued in accordance with the following guidelines:

- Ensure buildings are oriented towards the street. This can be accomplished by having the main entrance face the street.

- Encourage an urban form of housing that strongly relates to the street and maximizes the potential of the site by respecting the following siting guidelines:
 - Permit variations in the front yard setback, from a minimum of 1.5 m to a maximum of 6.0 m (or the foreset line, whichever is greater) to provide transition space from public to private space and to provide opportunity for decks and balconies.
 - Ensure a minimum side yard setback of 0.0 m interior lot lines and 3.0 m from exterior lot lines and 2.4 m between detached buildings.
 - Ensure a minimum rear yard setback of 6.0 m, except where an attached or detached garage faces a rear lane where the minimum rear yard setback is reduced to 1.5 m.
 - Permit a maximum lot coverage of 60%.
 - Permit a maximum building height of 4 storeys, not to exceed 18.0 m, with the exception that no more than 20% of the roof area may be constructed to a maximum height of 20.0 m to accommodate various vertical architectural elements.
- Building forms shall ensure visual variety through articulation, modulation, varied setbacks, indentations, building separation, roof modulation, varied roof pitch and finish, and the use of various natural building materials (e.g. brick, wood, stone) and colours to reflect the dry Kamloops landscape.
- Parking will be discouraged in the front yard of multiple storey walk-up apartments fronting Valleyview Drive or the Grand Boulevard.
- RV parking shall be intensively screened and landscaped and shall not be located adjacent to Valleyview Drive.
- Common parking areas should be broken down into small clusters and should be visually separated by curbing, lighting, directional signage, landscaping, or any combination of these elements.
- Permit fences to a maximum height of 1.0 m in the front yard, except that the maximum fence height in the front yard may be increased to 1.5 m for a length not to exceed 25% of the width of the lot.
- Permit fences to a maximum height of 2 m in the side and rear yard except that the maximum fence height in the side yard shall be 1 m where the side yard abuts a street.
- A minimum of 25% of the site shall be landscaped.

v) Village Centre Commercial

Development Permits, including commercial and mixed-use development, shall be issued in accordance with the following guidelines:

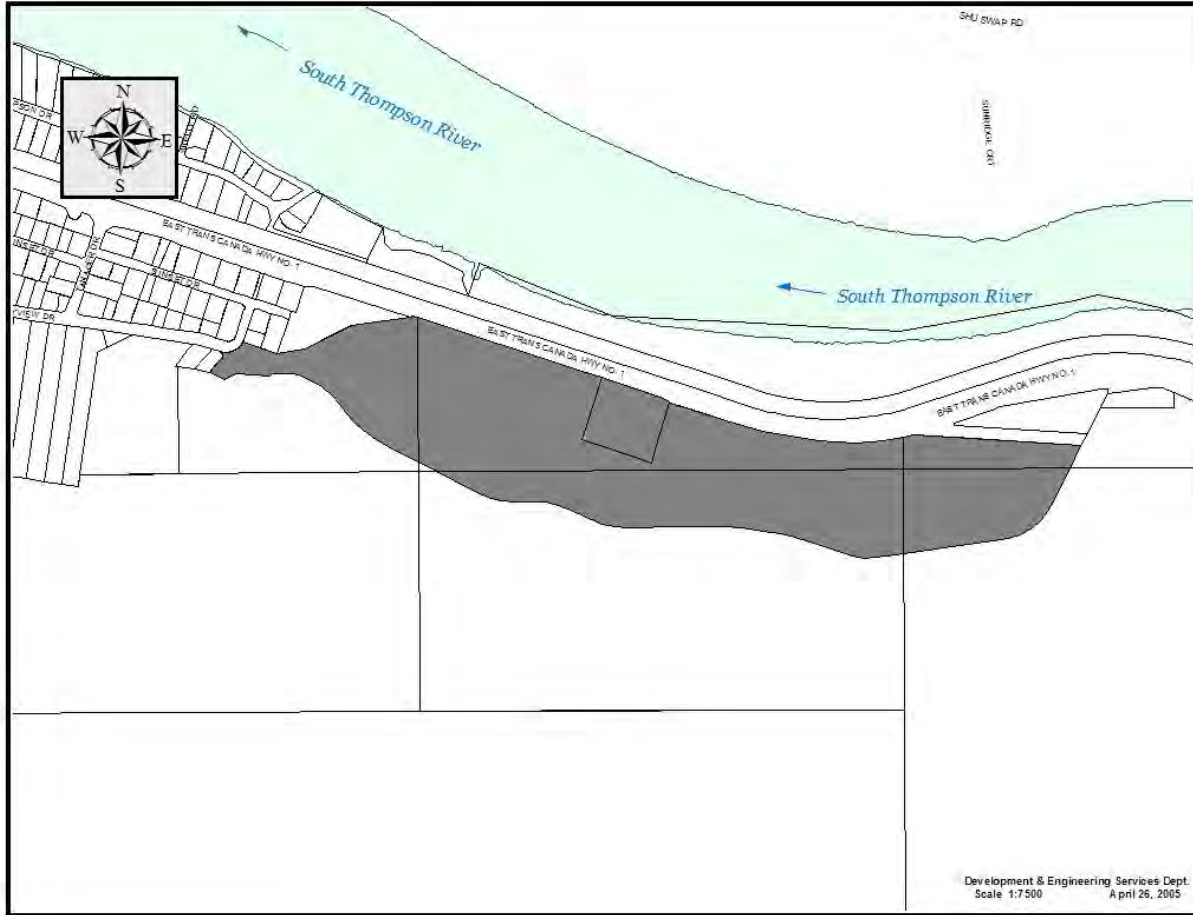
- Encourage a highly pedestrian-oriented village centre by:
 - ensuring buildings are oriented toward the street;

- ensuring a higher level of architectural detail that provides visual interest at street level;
 - accommodating transparent ground-level commercial to animate the street space;
 - clearly identify the primary entrance to a building by using special architectural treatments, signage and landscaping;
 - discouraging stand-alone buildings surrounded by parking;
 - providing visual variety through articulation, modulation, varied setbacks, indentations, building separation, roof modulation, varied roof pitch and finish, the use of various natural building materials (e.g. brick, wood, stone) and colours that reflect the dry Kamloops landscape.
- Encourage an urban village centre that strongly relates to the street, is pedestrian-oriented and has a human scale by respecting the following siting guidelines:
 - Encouraging commercial development to locate close to the street edge to enliven the street and to reinforce an urban character.
 - Setbacks from the street are encouraged where outdoor courtyards, seating areas or plaza space is provided.
 - Permitting a maximum lot coverage of 90% and a maximum FAR of 3.0.
 - Permitting a maximum building height of 4 storeys, not to exceed 16.0 m, with the exception that no more than 20% of the roof area may be constructed to a maximum height of 18.0 m to accommodate various vertical architectural elements.
 - Encourage a landmark vertical architectural element at a highly visible location within the Village Centre Commercial area to reinforce a sense of place and to visually identify the centre of Orchards Walk.
 - Permit mixed-use multiple family residential and commercial development within the Village Centre Commercial area, provided the ground floor is preserved for commercial uses.
 - Pedestrian-level commercial uses shall be distinguished architecturally from attached residential units using horizontal architectural features, building indentations and varying colors and material types.
 - Ground level entries to residential development must be separate from the commercial entrance and shall be clearly visible from the street.
 - Encourage upper level outdoor activity spaces adjacent to residential uses, including decks, balconies and patios.
 - Common parking areas should be broken down into small clusters and should be visually separated by curbing, lighting, directional signage, landscaping or any combination of these elements.

- Encourage the development of a central plaza area to function as both the village centre and the community gathering space.
- Ensure the pedestrian bridge is integrally linked to the village centre and is designated to reflect the character and quality of the village centre and surrounding community.
- A minimum of 15% of the site shall be landscaped.
- Encourage low level monument, fascia or hanging projecting signage that is of a scale and level of detail that reflects the pedestrian orientation of the village centre.
- Ensure site lighting is directed to minimize light pollution and potential impact on adjacent residential areas.

vi) Landscaping and Screening

- Ensure public open space is usable, is located in highly visible areas, and where possible, includes trees that provide necessary shade in hot summer months and an open canopy to facilitate sun access in cool winter months.
- Preserve and creatively integrate existing trees and topographical features into the development where feasible.
- Use tree species and vegetation commonly found within the Kamloops area.
- Use landscaping to define public and private areas and to provide visual and physical breaks in hard urban surfaces (e.g. parking areas, facades, courtyards, patios, etc.).
- Garbage containers, utility connections, roof top mechanical equipment and the like, should be appropriately located and screened from the view of building tenants, the street and other prominent viewpoints.
- Use additional landscaping, tree planting, signage and lighting to emphasize neighbourhood entrance points.
- Encourage a wide variety of sound attenuation facilities adjacent to the Trans Canada Highway East, including fences, landscaped walls, berms and other similar techniques, designed to reduce noise impacts and to complement the character and quality of the adjacent development.



ORCHARDS WALK DEVELOPMENT PERMIT AREA

(5-1-2205) 1.13 JUNIPER CORNER DEVELOPMENT PERMIT AREA

Application: Category (e) Form and character of intensive residential development.

A Development Permit shall be required prior to any improvements to a property, including alterations, additions, site design, landscaping, new construction, the issuance of a building permit or approval of a subdivision application, for all commercial, industrial, institutional, or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: Development Permit Area 13, as shown on the attached map.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations;
- external renovations that do not affect the form and character of the building; or
- single or two-family dwellings.

Implementation:

In accordance with Bylaw No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Justification:

The Juniper Corner area is a 1.98 ha site located at the intersection of the only access road to the Juniper Ridge neighbourhood. A combination of residential and commercial development is expected to occur and is required to integrate within the existing neighbourhood. The objective of the designation is to ensure that new development on the site is compatible with the standards and principles established through the rezoning process.

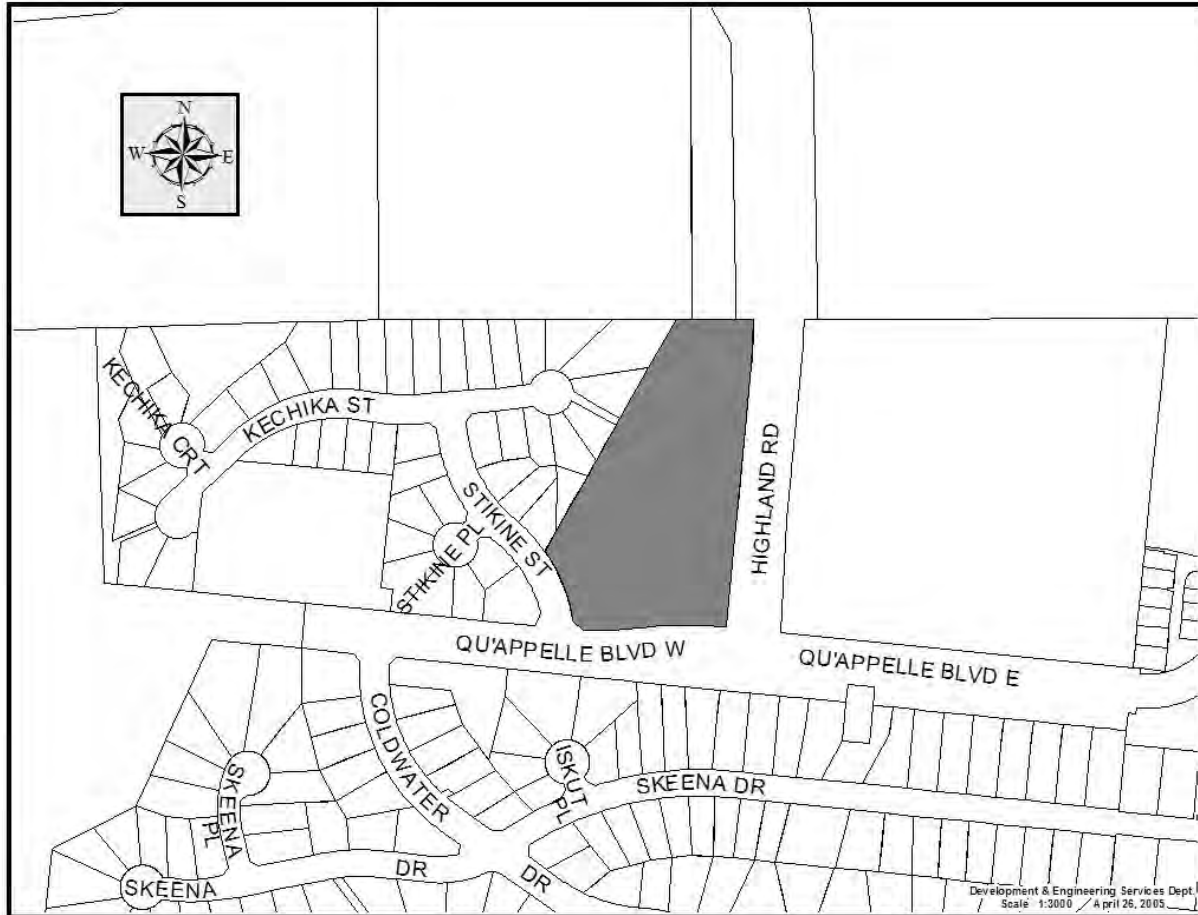
The Juniper Corner Development Permit Area establishes comprehensive urban design guidelines for this prime gateway site. The guidelines incorporate crime prevention principles and support a quality residential design that is consistent for both residential and commercial uses.

Guidelines:

Development Permits issues in this area shall be in accordance with the following guidelines:

- a) Rooflines shall retain a residential character, using exposed trusses, dormers and a pitched roof modulation.

- b) Exterior building finishes and facades shall integrate natural wood wherever possible, particularly on decks, eaves and window trim.
- c) Commercial buildings shall be designed to project a residential appearance through careful use and positioning of windows, doors and patios.
- d) On-site parking shall not be located in the yards fronting Qu'appelle Boulevard or Highland Road.
- e) All areas not covered by buildings, structures or asphalt shall be landscaped. Internal pathways and walkways shall be provided to ensure ease of circulation to both on and off-site services and amenities. The provision of landscaping shall consider the use of xeriscaping and native landscaping materials.
- f) Landscaping shall maintain a residential character with a minimum of a 6 m setback for landscaping along Qu'appelle Boulevard and Highland Road.
- g) Landscaping and fencing oriented to the street shall not exceed 1 m in height.
- h) Residential units above commercial ground-floor units are to instill a feeling of security by providing opportunities for increased visibility to the street.
- i) Public, private, and semi-private areas are to be clearly defined. Public and semi-private spaces are to be designed so as to maximize surveillance.
- j) Buildings are to provide maximum opportunity for surveillance of sidewalks, entryways, circulation routes and/or parking areas.
- k) Any recessed entryways and/or blind corners are to be avoided. Exterior stairwells should be visible.
- l) Lighting shall be set up in such as way as to ensure clear visibility, both during the day and night, of all access routes, landscaped areas, and parking areas, without causing excessive lighting levels or glare.



JUNIPER CORNER DEVELOPMENT PERMIT AREA

1.14 APPENDIX 1 - RIPARIAN AREAS REGULATION DEVELOPMENT PERMIT AREA

Application: Category (a) Protection of the natural environment, its ecosystems, and biological diversity.

A Development Permit shall be required for all activity defined as development under the Riparian Areas Regulation (RAR) that is located within 30 m of the high watermark or top of a stream's ravine bank. The Riparian Areas Regulation and all associated documentation, including the Implementation Guidebook should be referenced for greater detail. Development is defined as follows:

- a) removal, alteration, disruption, or destruction of vegetation;
- b) disturbance of soils;
- c) construction or erection of buildings and structures;
- d) creation of non-structural impervious or semi-pervious surfaces;
- e) flood protection works;
- f) construction of roads, trails, docks, wharves, and bridges;
- g) provision and maintenance of sewer and water services;
- h) development of drainage systems;
- i) development of utility corridors; and
- j) subdivision as defined in Section 872 of the Local Government Act.

Area: This Development Permit Area applies to all privately owned land abutting the North and South Thompson Rivers, Kamloops Lake, all their tributaries, and all waterbodies containing fish habitat within the City of Kamloops. The RAR Development Permit Area map identifies most watercourses and water bodies subject to this development permit area. However, the map is not inclusive of all watercourses or water bodies within City limits. Should a stream or other watercourse not identified on the map be identified, it is the responsibility of the proponent to contact the City of Kamloops to determine whether or not this RAR Development Permit Area applies.

Guidelines:

Prior to undertaking any of the activities defined as development within the Riparian Areas Regulation Development Permit Area, an owner of property or appropriate delegate shall apply to the City of Kamloops for a Development Permit, and the application shall meet the following guidelines:

- a) A Qualified Environmental Professional (QEP) will be retained at the expense of the applicant, for the purpose of preparing a report on the riparian area pursuant to the RAR Assessment Methodology Guidebook. The report will be submitted to the Ministry of Environment, Fisheries and Oceans Canada, and the City of Kamloops.

- b) Where the QEP report proposes a Harmful Alteration, Disruption, or Destruction (HADD) to fish habitat pursuant to the Canada Fisheries Act and/or other applicable federal regulations, the Development Permit shall not be issued unless the HADD is subsequently approved by Fisheries and Oceans Canada (FOC). In order to apply for a HADD, the City must express support in principle for the HADD and identify the context as identified by the QEP report.
- c) Where the QEP report describes an area designated as Streamside Protection and Enhancement Area (SPEA), the Development Permit will not allow any development activities to take place therein, and the owner will be required to provide a plan for protecting the SPEA over the long term through measures to be implemented as a condition of the Development Permit, such as:
- A dedication back to the Crown, Province, or the City of Kamloops;
 - Gifting to a nature protection organization such as a conservation authority; and
 - The registration of a restrictive covenant over the SPEA securing it as a riparian buffer to remain free of development.
- d) Where the QEP report describes an area as suitable for development with mitigating measures, the Development Permit shall allow the development in strict compliance with the measures described in the report. Monitoring and regular reporting by professionals retained by the applicant may be required, as specified in the Development Permit.
- e) Variances to the Zoning Bylaw may be granted in compliance with the QEP report.
- f) If the nature of a proposed project in a riparian assessment area changes, the QEP will be required to re-assess the proposal with respect to the SPEA.

Exemptions: Applications for the following shall be exempt from requiring a Development Permit:

- a) Renovations, repairs, and maintenance to existing buildings and structures that are determined to be legally non-conforming under Section 911 of the Local Government Act;
- b) Reconstruction of a building or structure damaged beyond 75 per cent of its value, as described by Section 911(8) of the Local Government Act, provided it remains on its original foundation;
- c) **Farming activities;**
- d) Minor interior and exterior renovations to existing buildings, excluding any additions or increases in building volume;
- e) Developments that have been approved but not yet constructed are honoured provided the approval has not changed;
- f) Mining activities, hydroelectric facilities, and forest management, including the logging of land; and
- g) Institutional developments.

Existing permanent structures, roads, or other development within the riparian area are considered legal and conforming. Consequently, landowners are entitled to continue using their land in the same manner as they have historically, even if a SPEA is designated on it.

CONCURRENT DEVELOPMENT PERMIT AREAS

Approval under the Riparian Areas Regulation Development Permit Area does not constitute approval under other development permit areas, which may apply to the same land.

1.15 MULTIPLE FAMILY DEVELOPMENT PERMIT AREA

Section 1.15

Multiple Family
Development

Permit Area





A Application

Category (f)

Establishment of objectives for the form and character of multi-family residential development.

(S-1-2398)

- A development permit shall be required for multiple family residential development on properties zoned for multiple family or comprehensive residential development for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout and street enhancement.
- Zoning by-law variances may be considered:
 - If they do not affect use or density.
 - If the variance complies with the intent of the Development Permit Area Guidelines.
 - If the variance enhances the proposed development.
 - If the variance is necessary in order to avoid a physical constraint.
 - If the variance does not adversely impact adjacent land; and
 - If the applicant demonstrates a benefit to the proposed development such as an enhanced amenity space area.

A Area

(S-1-2416)

- The entire City of Kamloops is designated a development permit area except:
 - Thompson Rivers University (housing on campus aimed primarily at non-university users does not fall within this exemption and requires a Multiple Family Development Permit).
- This development permit area applies to all multi-family development within the City of Kamloops and overlaps with some existing development permit area. In those cases where the multi-family development permit area overlaps with existing development permit areas, refer to the Implementation Section for how the guidelines will apply.

E Exemptions

- A Development Permit shall not be required for the following:
 - Internal renovations;
 - External renovations which do not affect the form and character of the building or site (to be determined by Development and Engineering Services Department); and
 - Subdivisions.

J Justification

- Future growth and demographic trends suggest that there will be increased interest in multi-family developments. KAMPLAN encourages the distribution of multiple family developments throughout the community in a variety of location to avoid large concentrations of higher density housing and to encourage more diversity within neighbourhoods. A high standard of design is important in the long-term as it can ensure that new development are appropriate to their surroundings, contribute to a sense of neighbourhood, and are attractive and functional for future residents.
- The objectives of these guidelines are to:
 - ensure the multi-family developments achieve high quality design and residential livability;
 - Ensure that new developments are compatible with and contribute positively to neighbourhood character and urban form;
 - Encourage multi-family projects that residents can take pride in and have a sense of ownership of;
 - Enhance the public interface of multi-family developments with attention to structural placement, building orientation, and landscape treatment;
 - Ensure that multi-family projects incorporate common open space and amenities in keeping with residential needs; and
 - Ensure that multi-family projects are designed with adequate setbacks, landscaping, and massing to address privacy, solar access, and compatibility with adjacent single-family residential development.



I mplementation

- This section contains a development permit area for multi-family residential developments in Kamloops. This development permit area applies to all multi-family development within the City of Kamloops and overlaps with some existing development permit areas. In those cases where the multi-family development permit area overlaps with existing development permit areas, the following table outlines how the guidelines will apply.
- In accordance with By-law No. 5-1-2277, delegated authority to issue a Development Permit can fall to the Development and Engineering Services Department for applications of a construction value of \$250,000 or less, multiple family development of eight or fewer units, where variances to zoning regulations are not requested; where Development Permits only pertain to site planning or landscaping; and/or where existing Development Permits are being amended. Any other Development Permit applications that fall outside of these parameters will require the approval of City Council.

Development Permit Area	Relationship to Multi-Family DPA
1. City Centre	City Centre guidelines take precedence, but these multi-family guidelines will be considered for multi-family developments.
2. Tranquille Road	Tranquille Road and these multi-family guidelines will both be considered for multi-family developments. Tranquille Road guidelines will take precedence after a neighbourhood plan is completed, but these multi-family guidelines will still be considered.
3. Westsyde Core	These multi-family guidelines will take precedence for multi-family development except where there are any guidelines respecting the form of multi-family development in the Westsyde Core guidelines, in which case the Westsyde Core guidelines will take precedence.
4. Pineview Valley	These multi-family guidelines will take precedence for multi-family development except where there are any guidelines respecting the form of multi-family development in the Pineview Valley guidelines, in which case the Pineview Valley guidelines will take precedence.
5. Mt. Dufferin	Mt. Dufferin and these multi-family guidelines will be considered for any multi-family developments.
6. South Thompson Silt Bluffs	These multi-family guidelines are not applicable.
7. Dallas Commercial Core	Dallas Commercial Core and these multi-family guidelines will be considered for any multi-family developments.
8. Campbell Creek Industrial Park	These multi-family guidelines are not applicable.
9. McGill Corridor	McGill Corridor guidelines take precedence, but these multi-family guidelines will be considered for any multi-family developments.
10. Notre Dame Drive	Notre Dame Drive guidelines take precedence, but these multi-family guidelines will be considered for any multi-family developments.
11. Sahali Commercial	Sahali Commercial guidelines take precedence, but these multi-family guidelines will be considered for any multi-family developments.
12. Orchards Walk	These multi-family guidelines are not applicable due to the level of detail of residential design guidelines in this development permit area.
13. Juniper Corner	Juniper corner guidelines take precedence, but these multi-family guidelines will be considered for multi-family developments.

G uidelines

- Development permits issued for multiple family developments shall take the following guidelines in consideration. Planning staff shall determine which guidelines apply after consulting the applicant and researching the subject property. The research shall include a site visit.

Multiple Family Development Permit Area



1.0 Context

DESIGN PRINCIPLE:

Development should respond to existing site conditions and opportunities, reinforce the street as the foundation of the urban structure, and respect adjacent sites.

T Topography

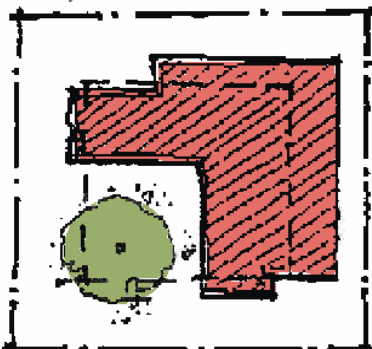
- Buildings and structures should reflect, rather than obscure, natural topography. Particularly buildings and structures on slopes should be designed to have the hillside step up in a gradual manner whereby buildings and retaining walls are integrated into a slope, reducing visual impact. For example, wherever possible, the first floor of a building should be below grade, retaining walls should be low in height, and one or more terraces are encouraged in favour of installing one large wall, and landscaping should be used to connect and blend terrace levels.



A multiple family development that steps up the hillside to match the topography and reduce the impact on neighbouring buildings

E Existing Vegetation & Natural Features

- Opportunities should be considered to carefully locate buildings around significant on-site trees or tree clusters, existing vegetation, or other important natural features that could be preserved to reinforce a 'green' image and provide a natural and visual amenity.



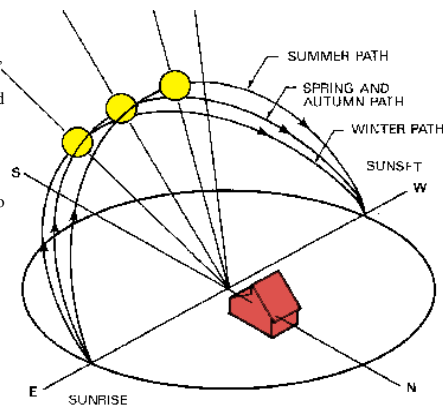
A development that is aligned to preserve a significant on-site tree. In this case, setback variances have been permitted to allow for the unique site design.

V Views

- Developments should be planned to optimize existing views and vistas, and create new ones where possible. Property owners, however, are entitled to zoning rights such as maximum height and floor area ratios.
- Site planning and building design should consider the view and shadow impact onto adjacent developments and, wherever possible, preserve views through creative building placement and modified building forms.
- On hillsides or in other prominent locations, developers may be required to provide a detailed visual or height impact assessment to illustrate the impact of the development on adjacent properties.

S Solar Orientation

- Properties should be designed to minimize shadow impacts on adjacent buildings and public areas. Wherever possible, solar exposure should be maximized





Streetscape Compatibility

- Where possible, buildings should front onto a street or, at minimum, appear to front onto a street. Buildings should be designed to encourage natural surveillance of the street.
- Apartment buildings should have at least one common entrance that is clearly visible from the street. Emphasis and interest for entrances can be provided by special detailing or architectural features such as ornamental glazing, railings, awnings, canopies, decorative pavement and lighting, seats, planter boxes, or art work near the doorway.
- Where possible, first-floor units should provide individual entrances to the street to enhance the level of vitality, activity, and pedestrian comfort on the street.
- Buildings on corner lots should be oriented both to the corner and to the street fronts, and design should add significance or prominence to the corner. This can be accomplished by providing visual and physical access to the building from the corner, expressing height or additional building mass at the corner, or providing other distinctive architectural elements at the corner.



A multiple family development that defines and overlooks the public right-of-way.

- In buildings with little or no front yard setback, the first floor should be elevated higher than the street to enable better supervision of the street and to increase the privacy of first floor units. Notwithstanding this point, convenient access should be provided for people with mobility problems.



In this suburban townhouse development, the units in the foreground relate well to the public street, whereas the units in the distance turn their back to the street and are screened by the site landscaping.

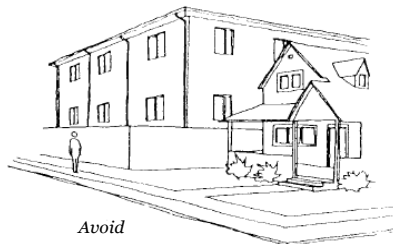
- Where buildings are set back a significant distance from the street, they should have a sufficient presence to contribute to that street. Such buildings should be appropriately scaled to relate to the street and surrounding building lines, consider opportunities for porches, picture windows, and garden space, and incorporate suitable edge treatments, such as fencing or landscaping, that contribute to the enclosure of the street.



This development contributes to the vitality of the street through the use of appropriate building height, picture windows, balconies, gardens, and a transparent, low-level fence at the property line.

Integration with Adjacent Sites

- To achieve harmonious integration with surroundings, development should be sensitive to the scale, mass, and form of adjacent buildings.
- Continuity of frontage should be provided by ensuring the use of a common building line amongst new and existing development.
- Building height should consider proportion and the relationship to adjacent buildings and public open space. Where proposed building height is lower or higher than that of adjacent development, consideration should be given to step up or step down to adjacent development or to reflect similar roof treatments, horizontal and vertical building articulation, varying materials, and different colours to provide visual relief.



Avoid



Prefer

In the example above, the multiple family development respects the adjacent site by matching the form of the adjacent single detached home, using a similar roof treatment, and maintaining a relationship to the street.

Safety

- Site design should encourage natural surveillance and reinforce a sense of ownership over place. The boundaries of a place should be accomplished through landscaping, fencing, accentuated building entrances, and other architectural features. Consideration may be given to the guidelines and principles of Crime Prevention through Environmental Design (CPTED).



2.0 Form and Character

DESIGN PRINCIPLE:

Development should be designed so that its form and character is at a human scale, compatible with existing adjacent development and reflective of Kamloops' natural setting.

B Building Mass & Scale



A multiple family development that creates visual interest by breaking the building mass down into volumes using both horizontal and vertical articulation.

- In larger buildings, building mass or bulk should be broken down into a number of volumes to improve the human scale, enhance visual interest, and create building rhythm. This can be accomplished through the use of horizontal and vertical articulation, varying materials, and different colour treatments. Repetitious forms without significant articulation will be discouraged, and buildings should avoid having large, blank walls, especially when facing the street. If blank walls are unavoidable, they should receive a design treatment to increase visual interest and pedestrian comfort.
- In mixed-use commercial and residential developments, pedestrian-level commercial development should be distinguished from the residential development above it by using horizontal architectural features, building indentations, and by varying colours and building materials.

B Building Height

- Decisions about building height should be made to create a functional and attractive street.
- Zoning measures such as "Floor Area Ratios", and in some instances "Building Height", determine a maximum building height within a zone. Depending on the circumstances, taller and narrower buildings, exceeding height maximums, may be better suited to a particular site. The advent of steeper pitched roofs may also exceed height restrictions.
- In order to facilitate good integration with neighbourhoods and adjacent properties, building height variances may be considered. Development that proposes to exceed maximum building height may be varied by a development permit upon submission of a visual impact assessment to determine the shadow impact of building height on adjacent property and important view areas. To mitigate for additional building height, higher levels should be stepped back. Building height variances shall not increase habitable floor area beyond limits identified in zoning.

R Roof Design

- Roof design should be similar to that of surrounding residential areas. Sloped and varied roof lines are encouraged, as are secondary roof forms such as dormers and varied roof pitches. Elements such as these can add visual interest and break up long expanses of roof.



An example of varied roof forms and secondary elements such as dormers, which are used to break up a long expanse of roof.

B Building Form

Materials & Colours

- Building form should be varied in developments with two or more buildings. Varied roof forms, building shape, and site layout may all contribute to a visually interesting design. Repetition of a building form is strongly discouraged.
- Building materials should be durable and contribute to an appearance of quality construction that evokes a sense of permanence. The use of natural products such as stone, brick, and wood may enhance buildings in this regard.
- Opportunities to use more than one material type are encouraged, to provide visual variety in built form. Where possible, changes in material, building articulation, texture, or colour should be used to break up large expanses of smooth material.



In this development, visual interest is enhanced through the use of a variety of materials, including stone detailing at the base.

- Exterior cladding should be varied by material and colour to create visual diversity and avoid a uniform appearance. Building colours should induce a sense of richness and liveliness. To reflect Kamloops' natural setting, earth tones and warm colours should be considered, while bolder colours may be used as accents or trim.

Multiple Family Development Permit Area



3.0 Site Landscaping and Screening

DESIGN PRINCIPLE:

High quality landscaping should reinforce Kamloops' natural setting and positively contribute to the overall character of multiple family development.

Landscape Materials & Location

- All areas not covered by buildings, structures, and parking must be fully landscaped, and development permit applications must be accompanied by a detailed landscape plan for the site and boulevard areas. This plan shall be consistent with the requirements of the zoning bylaw and the "Landscape Guidelines for Development." If the development is 10 units or greater, the plan shall be prepared by a registered landscape architect.
- Street-facing elevations should have landscaping adjacent to their foundations. Landscaping on other elevations may be required on a case-by-case basis.
- In boulevard areas, street trees are encouraged. Boulevard treatments should be consistent with that of adjacent properties.
- Landscaping is encouraged as a means of defining public and private areas and providing breaks in hard urban surfaces (e.g. parking areas, blank walls, patios).
- Whenever possible, the retention of existing trees is strongly encouraged.
- Development is encouraged to use tree species and other forms of vegetation commonly found in the Kamloops area.
- Xeriscaping is encouraged as an important means of conserving water and reflecting Kamloops' dry climate.
- Landscaping should reinforce the character of the neighbourhood and use plant materials similar to those on adjacent properties.
- Landscape features should be used to enhance and define gateways and entrances into multiple family developments. However, entrance landscaping should be low-level so as not to detract from pedestrians' security.
- All lawns and landscaped areas should contain an underground irrigation system as required to maintain the integrity of the landscaped features.

- Landscaping should be used to soften and embellish units, to buffer units from noise and undesirable views, and to provide visual screening.



Use landscaping on the ground level to soften and embellish units and provide visual screening.

Utilities & Services (5-1-2618)

- Practical access for servicing garbage bins, recycling bins, and outdoor or rooftop equipment including mechanical, electrical, or other service equipment should be appropriately located and screened from the view of building tenants, the street, adjacent properties, and other prominent viewpoints; and
- Garbage and recycling bins can be effectively screened using landscaping, fencing, a trellis, or other design features.



Garbage and recycling bins can be effectively screened using landscaping, fencing, a trellis, or other design features.

Fencing

- Maximum fence height in any yard shall be 1 metre. In a rear or side yard, fences to a maximum height of 2 metres shall be permitted, except that maximum fence height shall be 1 metre in a yard that abuts any street.
- Street-side fencing should not provide a continuous wall or barrier. Rather, it should provide regular access to the property and be low in profile, somewhat transparent, and supplemented by landscaping.



Avoid a continuous wall or barrier. Rather, provide regular access to the property and be low in profile, somewhat transparent, and supplemented by landscaping.



- Chain-link fences are discouraged adjacent to the street or other residential properties.

Signage

- Signage should reflect the architectural character of the development and not be visually obtrusive or present a cluttered image. Entry signs should be placed at or below eye level and be integrated with landscaping or other features.
- Street address numbers should be clearly identified on buildings or units.



4.0 Open Space and Amenity Areas

DESIGN PRINCIPLE:

Open Space and Amenity areas should provide a sanctuary for residents and be comfortable, safe and clearly defined

Amenity Areas

- Common amenity areas, required through zoning, must be provided to allow for active recreation or passive activities such as lounging or socializing. Examples of amenity areas include activity centres, daycare facilities, swimming pools, tennis courts, clubhouses, playgrounds, and similar recreational/social facilities. Outdoor amenity areas like gardens and courtyards are also encouraged.
- The size and type of amenity areas should be consistent with the requirements set up in the Zoning By-law and meeting a minimum of 30 per cent of the total site area.
- Outdoor amenity areas should be usable and located in highly visible areas that are overlooked by the development. They should incorporate seating areas, an entry court to the building, or other features to foster social interaction and a sense of community. Such amenity areas should be oriented to receive the maximum sunlight possible, using trees or overhangs to provide shade from sun or relief from snow as necessary.
- Notwithstanding the amenity space requirements of the zoning bylaw, where multiple family developments are located in close proximity to neighbourhood parks, schools, or open space areas, opportunities to reduce the required amenity area will be considered on a case-by-case basis. In these situations, the City will explore partnership/funding opportunities with the developer to provide recreational equipment in the neighbourhood park space for the use of all neighbourhood residents.



Entries facing the street provide a clear distinction between public and private space. Changes in grade provide privacy while allowing for a small semi-private open space within the front yard setback.

Definition

- There should be a clear definition between public, semi-private, and private spaces. This transition helps users to define what is 'mine' versus 'ours,' and it can be accomplished through changes in grade, landscaping, or other design features.



An example of a well-overlooked pedestrian route that is well-defined through the use of alternate paving materials and landscaping.

Comfort, Safety & Crime Prevention

- Where possible, pedestrian routes should be provided to connect to neighbouring commercial properties or open spaces. Dead-end pathways within the development should be avoided.
- Major pedestrian walkways should have a hard, slip-resistant surface, and borders should be well-defined through the use of alternate materials, textures, or landscaping.
- To enhance outdoor safety, blind spots should be avoided, particularly near dumpsters, parking areas, and other open spaces.
- Active recreational areas such as playgrounds must be safely accessible without vehicular traffic interference, and shall be located in areas that permit visual supervision by residents.
- Dark or shadowy places should be avoided. All entrances, pedestrian pathways, open spaces and parking areas should have adequate lighting at a human level. Light fixtures that complement the character of the development and surrounding area are encouraged, and light pollution onto adjacent properties should be avoided.



5.0 Parking and Circulation

DESIGN PRINCIPLE:

Access points and parking areas should be designed to minimize impact of the vehicle and maintain a pedestrian character on the street and within the development

Location of Parking

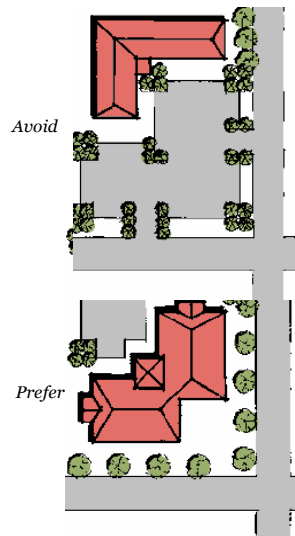
- Parking areas should be located close to main building entrances or have a direct access route to the building. As appropriate, signage should be used to assist in locating pedestrian routes and building entrances.
- Large expanses of parking should be internally located rather than being adjacent to the length of the street front. If parking areas must be sited adjacent to a street, they should be provided in small clusters and be partially screened from view with durable, attractive materials.



The multiple family development orients itself to the street front to fit in with the existing neighbourhood. Parking is provided in small clusters in the interior of the block.

- High-density multiple family developments are encouraged to have underground parking, be gated, and lit for safety and security. Entrances to underground parking should have landscaped edges and be screened from adjacent residential units. In most cases, on-street visitor parking is encouraged, particularly as a means of both calming traffic and reducing off-street parking demands.
- Any outdoor parking for apartments is encouraged to locate to the side or rear of the building, and in clusters, away from public open amenity space.
- In townhouses and other cluster forms of development, parking spaces and garages should be located to the side or rear of principal buildings, or recessed behind the main building line.

- On corner lots, parking lots should be directed away from the corner, which should be defined instead by a building.



Parking is best located internally, as opposed to being adjacent to the street front.

- Parking spaces should be located such that casual surveillance is possible from a number of locations.

Paving Treatment

- The use of special paving, e.g. brick or stamped concrete, is encouraged to enhance project design and accent entryways, pedestrian crosswalks, walkways, and common areas.

Site Access

- Vehicular entrance points should be defined with additional landscaping, tree planting, signage, lighting, or other design features.
- The number and width of curb cuts and driveways should be minimized. Shared access points with adjacent properties are encouraged when possible.

Pedestrians & Cyclists

- Bicycle storage should be provided in a secure location.
- Developments should be designed with an internal pedestrian and bicycle system providing access to all units, common areas, and off-site connections as appropriate. Off-site connections are to provide access to schools, parks, and other community amenities located adjacent to or in immediate vicinity of the site.
- Pedestrian access should not be limited to vehicle access routes only. All connections should be designed with a priority on personal safety and the deterrence of vandalism.

Internal Circulation

- Development applications for multiple family developments will be required to submit a traffic circulation plan to ensure ease of access and off-street circulation within driveways and parking areas.



Parking Spaces & Lots

- Large expanses of contiguous, outdoor parking will be discouraged. Large parking areas should be broken up into clusters or smaller areas using landscaped islands to create a pedestrian-friendly, park-like environment. Within large parking areas, defined movement corridors should be provided for pedestrians.



Landscaped islands can break up parking lots and provide shade for both vehicles and pedestrians. Low-level planting can also be used to screen vehicles from views without compromising safety.

- Assigned parking spaces should not be marked with unit numbers, but should be identified in an attractive and subtle manner and should not be painted directly onto the asphalt.

- Visitor parking should be adequately identified and provided for and should be located in such a manner that is convenient for visitors and that is preferably integrated into the residences parking area.
- In addition to the requirements of the zoning bylaw, the number of required parking spaces may be varied by a development permit on a case-by-case basis for development near Thompson Rivers University or major transit exchanges, or for development that is designed specifically for users with lower parking needs.



The visual impact of headlights adjacent to units can be effectively mitigated through the use of low-level planting.

- Parking areas have the potential to be a source of noise and light that may affect adjacent residential areas, as well as dwelling units. To reduce this potential impact, areas between dwelling units and parking areas shall be landscaped with berming and/or foliage to achieve a minimum 0.9 metre screen.



6.0 Residential Highrise Development

The following residential highrise guidelines are provided as a supplement to the general multiple family development permit area guidelines. The intent is not to replace these guidelines, but to provide guidance that is unique to residential highrise development. For the purposes of these guidelines, residential highrise development shall be defined as development of 6 or more storeys.

DESIGN PRINCIPLE:

Residential highrise development should benefit the public realm, be compatible in scale and massing with surrounding development, enhance street character and the street-level pedestrian environment, and provide a high-quality, livable environment for residents.

The Street

- Residential highrise development should enhance the on-street environment. At street-level, a pedestrian character should be evident. To accomplish this, podium and tower development is encouraged, with row housing or retail space provided at street-level.



In this development, street-level row housing creates a comfortable pedestrian environment and softens the visual magnitude of the tower. The row housing provides a 3-storey podium for the tower, which is set back from the street.

- At sidewalk eye-level, building elements should be attractive and in scale with the pedestrian. Edges should be animated with frequent doors and windows, few or no blank walls, articulation of facades, and projections such as porches or stoops.
- Street-level residential units should be elevated approximately 1.5 metres higher than the street. This allows for better supervision of the street, and it increases the privacy of first floor units.

Open Space

- Residential highrise development should provide on-site open space that represents a significant addition to neighbourhood green and plaza spaces.



Although there are no entries to this development from the street frontage shown, the public realm has been enhanced through the use of a water feature, public art, trees, and seating areas for pedestrians.

- Public art is encouraged as a permanent element of the building or public realm.



Highrise projects need not turn their back to major streets. This development provides a significant green court directly adjacent to an arterial street. A water wall and lush landscaping provide a foil to the busy street.

Building Mass & Scale

- To preserve views and adequate light onto adjacent sites, slender point towers are preferred over bulkier, horizontal towers with a large apparent mass.
- The apparent bulk and mass of towers should be reduced through the use of articulation. Flat building facades are generally discouraged.
- On sites with more than one highrise, the towers should not be flush with one another. Rather, the tall buildings should be massed at either end of the site to permit light penetration and to add diversity to the skyline.

Relationship to Surroundings

- Towers should be compatible with surrounding structures, contribute to the general skyline, and sensitively consider colour, reflectivity, shape, fenestration, materials, and detailing. In general, lighter-coloured buildings are preferred.
- Building design should soften mass to minimize contrast with surroundings.
- Towers should be located to minimize the shadows they may cast on adjacent buildings and public areas. Developers may be required to provide an analysis showing the impact of a proposal on sun and shade conditions.

Multiple Family Development Permit Area

(5-1-2529)

1.16 SITE SPECIFIC COMMERCIAL AND INDUSTRIAL DEVELOPMENT PERMIT AREA

Application: Category (e) Form and character of commercial, industrial, or multiple family residential development

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all commercial industrial, institutional or multiple family residential development. Development Permits shall detail: parking, access and vehicular circulation, pedestrian circulation and access, landscaping, building elevations, site layout, and street enhancements.

Area: The Site Specific Commercial and Industrial Development Permit Area applies to all appropriately zoned lands identified on the attached map, as amended from time to time. In those cases where the Site Specific Commercial and Industrial Development Permit Area overlaps with existing multi-family or area specific Development Permit Areas, the guidelines of those other Development Permit Areas will take precedence over these guidelines, although these guidelines may also be considered.

Justification: The majority of Kamloops' commercial and industrial areas are located either on or near highways such as the Trans Canada Highway or major arterials such as Columbia Street, Summit Drive, and Westsyde Road. These guidelines are intended to encourage a high standard of built form, site design, and landscaping in these highly visible areas; ensure that new development compliments existing development on adjacent properties; and balance the desire for efficient traffic flows with the need to move towards a more pedestrian-friendly environment.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- internal renovations;
- external renovations that do not change the form and character of the building;
- single or two-family dwellings;
- small storage sheds that are not visible from a public road; and
- temporary buildings and structures permitted by a Temporary Commercial or Industrial Use Permit.

Delegation of Authority:

In accordance with the Kamloops Development Permit Procedure Bylaw, Council approval is not required for the following Development Permit applications:

- Development Permits pertaining to only site planning or landscaping;
- amendments to existing Development Permits; and
- any new improvement to a property or building equalling \$250,000 or less in building permit value, where that development is in compliance with all applicable regulations and bylaws pertaining to the subject property.

Design Guidelines**i) Built Form**

- Buildings shall be designed with architectural features and/or variation in treatment to avoid the appearance of long, homogenous façades and to reduce apparent building mass.
- Rooflines shall relate to adjacent building forms and styles. Varied rooflines shall be used to avoid the appearance of a long, flat building façade. Rooftop mechanical equipment must be screened by rooflines or well designed architectural details.
- Building exteriors shall use subdued rather than bright hues as the dominant colour tones and employ bold, complimentary accents. Secondary façades shall be treated in a colour tone that is the same as or complimentary to that of the principal façade. Bright, contrasting colours should be used for trims and accents only.
- Large expanses of highly reflective treatments and glazing (i.e. mirror glass) shall be avoided on exterior walls so as to prevent heat and glare impacts on adjacent properties and roads.
- Buildings on corner sites or with double road frontage shall be designed to acknowledge the buildings' visibility from each street through continuity of design, colour, materials, exterior finish, and signage.
- Buildings on corner sites shall be designed with regard for the high priority of public exposure and shall incorporate specific corner lot design features such as landmark architectural elements.
- Where feasible, buildings shall be oriented towards the street with parking located in the side or rear yard. Required front yard and side street yard setback regulations may be varied through a Development Permit to facilitate street oriented development.
- The architectural style of buildings containing multiple tenants shall remain cohesive over the entire frontage. Each individual entry point shall be identifiable without detracting from the building's overall appearance.

- Fascia signs shall be integrated into the building's façade, compliment existing signage, and comply with the City of Kamloops Sign Regulations Bylaw.
- Building components housing publicly accessible office and/or customer service areas shall incorporate architectural details such as windows, entry features, distinctive rooflines, and exterior finish materials that distinguish such areas from more utilitarian warehousing, manufacturing, and processing areas. The use of vinyl siding is discouraged in such publically accessible areas, while the use of natural wood, rock, and stone details is encouraged.
- New buildings shall be designed so that their form, character, and massing compliment existing adjacent development.
- Buildings taller than two storeys should be sited such that shadows and views do not adversely impact adjacent properties. Shadow and/or view impact studies may be required.
- Buildings taller than two storeys that are adjacent to a street may require upper storeys to be stepped back a minimum of 3 m to reduce the apparent mass of the building and reduce negative shadow and view impacts.
- Buildings facing residential uses shall be visually relieved by a combination of windows, building materials and textures, and architectural features.

ii) Site Design, Parking, and Circulation

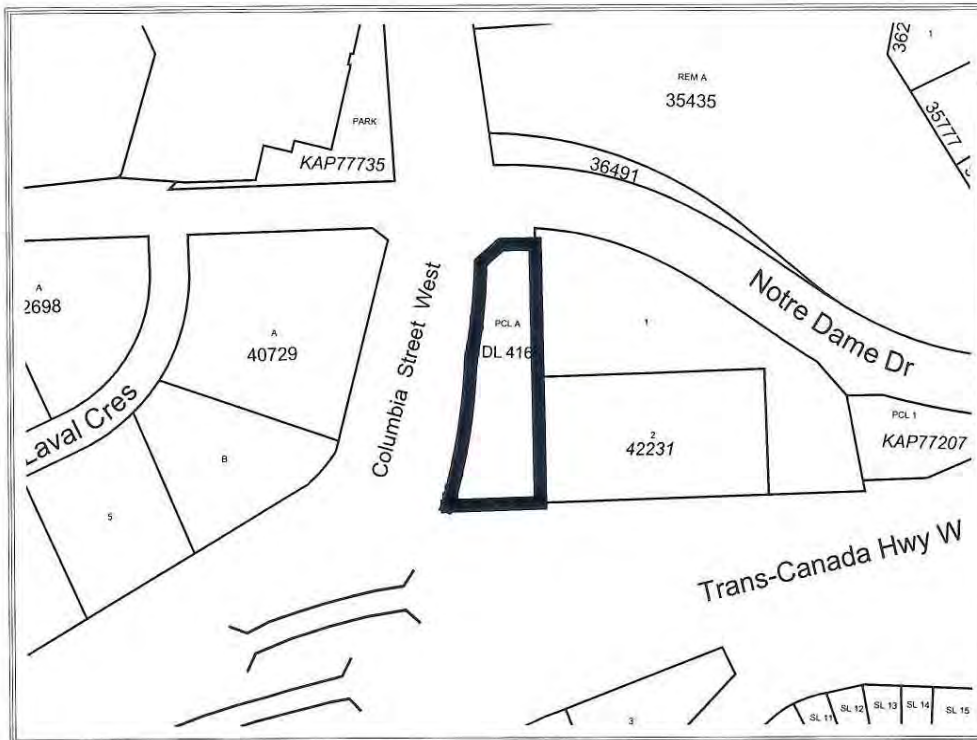
- On-site parking shall be located underground or at the side or rear of the property. Parking lots shall be screened and landscaped.
- Landscaping islands shall be used to separate large expanses of parking into smaller subsections appropriate to the use. Landscaping islands shall be located at each end of every parking aisle. In the case of longer parking aisles, landscape islands shall be provided in the middle of the aisle, with a maximum separation of 12 - 15 parking stalls between landscape islands.
- Landscaping islands shall have a minimum width 2.5 m and include one tree per parking aisle.
- Site plans must demonstrate the capacity for safe and efficient movement of people and traffic on the site. Site design must include pedestrian walkways along building frontages as well as continuous pedestrian connections between walkways along building frontages, street frontages, and on neighbouring sites.
- Pedestrian walkways shall be distinguished from driving surfaces by using varied treatments and by raising walkways to curb level.
- Entrances to buildings shall be located to maximize pedestrian safety.
- Bicycle racks shall be located near building entrances.

- Where drive-thrus are used, they shall be located at the side or rear of the property and away from sensitive adjacent uses such as residential and outdoor amenity areas in order to prevent conflicts between vehicles and pedestrians and encourage pedestrian safety. Drive-thrus shall not be located adjacent to the street frontage.
- Drive-thru stacking lanes shall include an adequate number of queuing spaces, as well as escape lanes, and shall be separated from parking areas and driveways by landscape islands. Stacking lanes shall be separated from pedestrian walkways so as to allow safe and comfortable pedestrian access to the building.
- Site layout shall incorporate Crime Prevention Through Environmental Design (CPTED) strategies such as encouraging the use of pedestrian thoroughfares to and from adjacent properties and avoiding the creation of dead ends.
- Raised curbs shall define entry driveways.
- Corner lot site design and landscaping shall consider all street frontages equally and provide equal quality of design, orientation, and detail.
- Outdoor storage areas, garbage bins, or loading/unloading areas shall be visually screened by landscaping from adjacent residential areas and streets.
- Outdoor storage areas, garbage bins, or loading/unloading areas shall be situated in a side or rear yard in order to minimize exposure of storage areas to public roadways.
- Access driveways are encouraged to be shared in order to minimize the number of entry/exit points along the road and to provide maximum spacing between them.
- Circulation aisles connecting adjoining properties shall be protected through reciprocal access agreements.
- Circulation aisles shall use signs or speed humps to manage operating speeds every 100 m and where circulation aisles intersect.
- Except where the property fronts a highway, all free-standing signage shall be monument-style and pedestrian-oriented, with a maximum sign area of 4 m²; shall relate to the façade of the building; and shall comply with the City of Kamloops Sign Regulations Bylaw (the Sign Regulations Bylaw requires that the number of signs on site shall not exceed the lesser of one for every two businesses or one for each street frontage, provided that the total number of free-standing signs shall not exceed three).
- Parking structures shall be built to a high standard of design that is consistent with the "Built Form" guidelines detailed above in terms of colour, treatment, massing, rooflines, view protection, and respect for adjacent development, but shall be oriented towards the rear of the property.

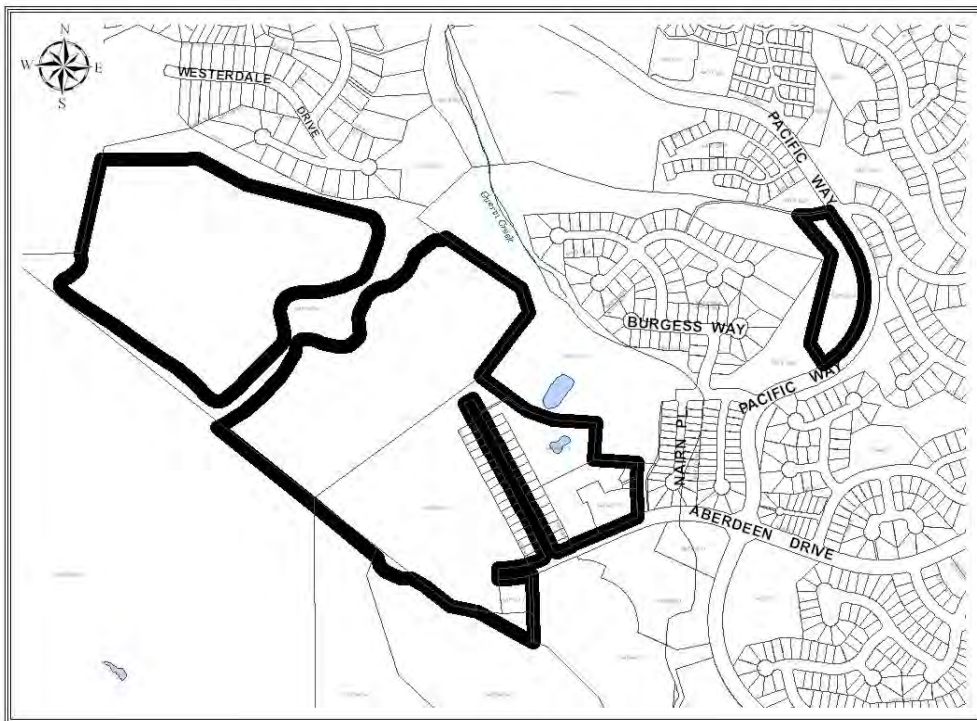
- Detailed traffic analysis shall be required where the proposed development will generate in excess of 100 vehicle trips during peak hours or otherwise significantly impact traffic flows and transportation infrastructure, as determined by the Development and Engineering Services Department.

iii) Landscaping

- All areas not covered by buildings or structures and not required for parking, loading, storage, assembly, processing, or manufacturing shall be suitably landscaped. The provision of landscaping shall consider the use of xeriscaping and native landscaping materials.
- A 4 m wide continuous perimeter of landscaping and supplemental landscaping adjacent to the residential areas must be provided. This landscaping is to include street trees of sufficient height to provide effective screening.
- Required landscaping shall be concentrated in the front yard.
- Where visual screening is required, the use of evergreen shrubs, trees, and other plantings forming an opaque visual barrier is encouraged.
- Landscape design shall prioritize security and pedestrian safety. The use of CPTED principles such as enhancing lighting and minimizing dark areas, encouraging opportunities for natural surveillance, and using visual cues to control access to the site should be considered when designing landscaping.
- The preferred design for lighting within front yards is by ground mounted units located discreetly within landscaped areas. This approach limits the number of light standards in the streetscape zone.
- Lighting and light standards in all public areas, including parking lots, shall relate to the pedestrian, be limited to a maximum 6 m height, and be directed toward the ground so as to avoid light pollution on neighbouring properties. Lighting plans may be required.
- External lighting shall be incorporated into building and landscaping design.
- Utility installations shall be integrated into landscaped areas and shall not be located in the front yard or side street yard.
- Landscape plans prepared and sealed by a landscape architect must be included in Development Permit applications; Development Permit approval will be contingent upon submission of security in the amount of 125% the estimated value of landscape work and British Columbia Society of Landscape Architects (BCSLA) Schedules of Assurance (Schedules L-1 and L-2).
- Parking and building height regulations may be varied through the Development Permit process in order to meet landscaping guidelines.

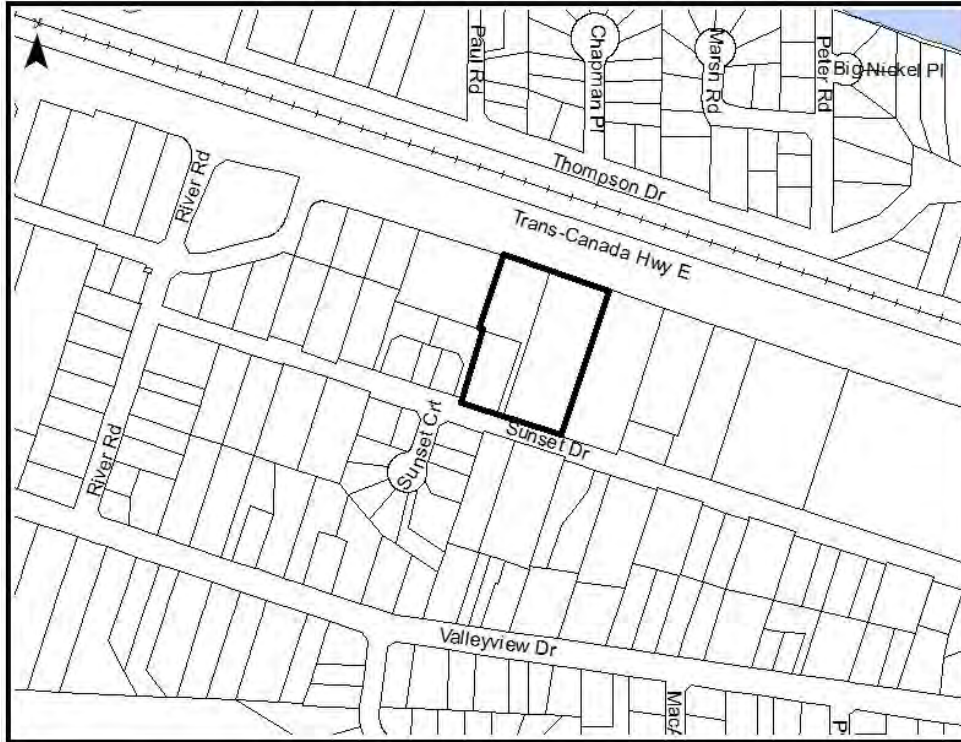


SITE 1
565 NOTRE DAME DRIVE



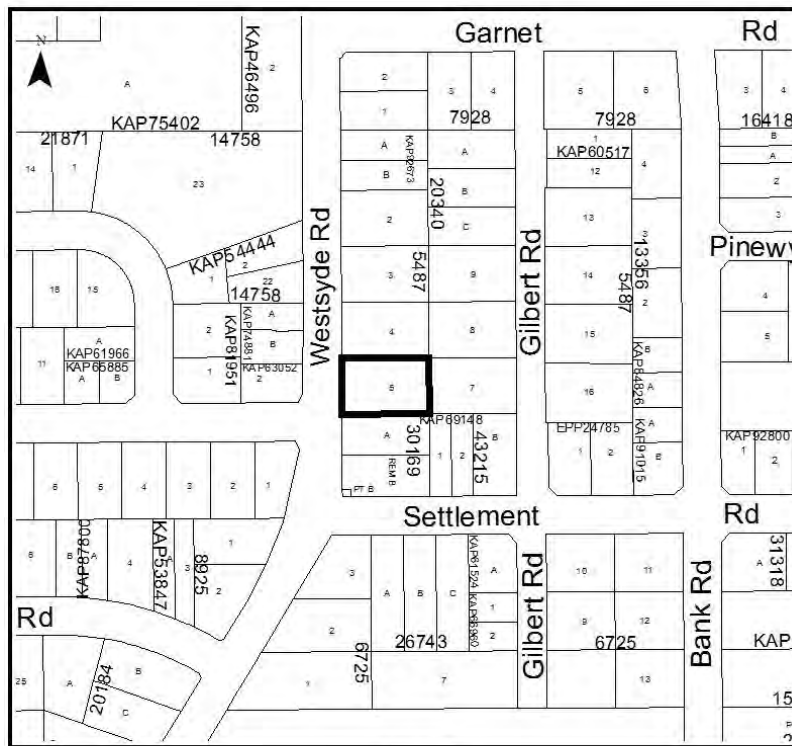
SITE 2
ABERDEEN HIGHLANDS

(5-1-2672)



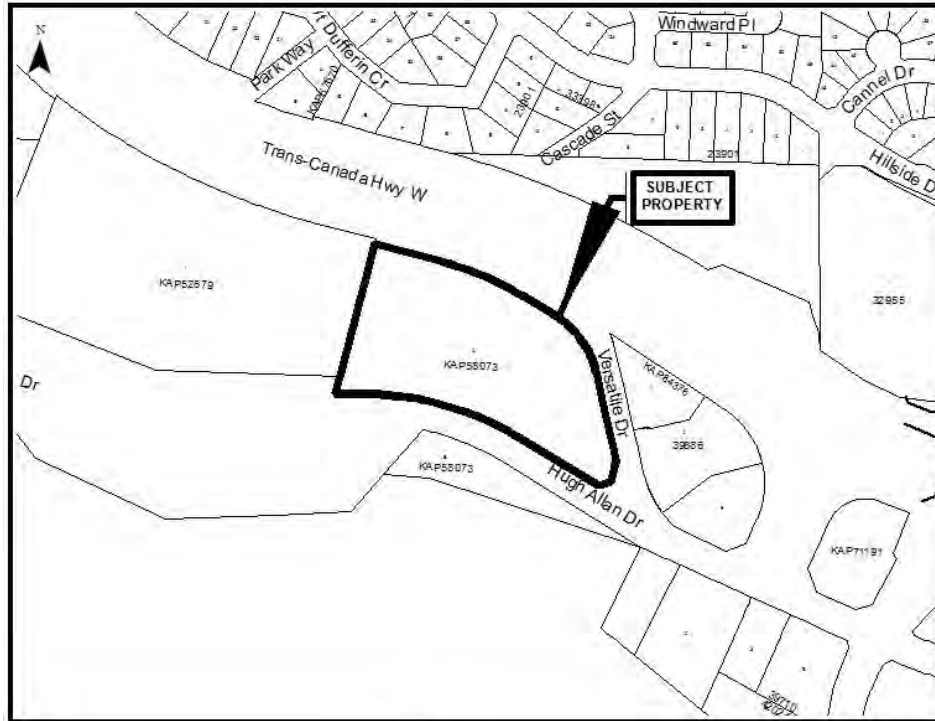
SITE 5
2466 SUNSET DRIVE, 2469 AND 2477 TRANS CANADA HIGHWAY EAST

(5-1-2725)



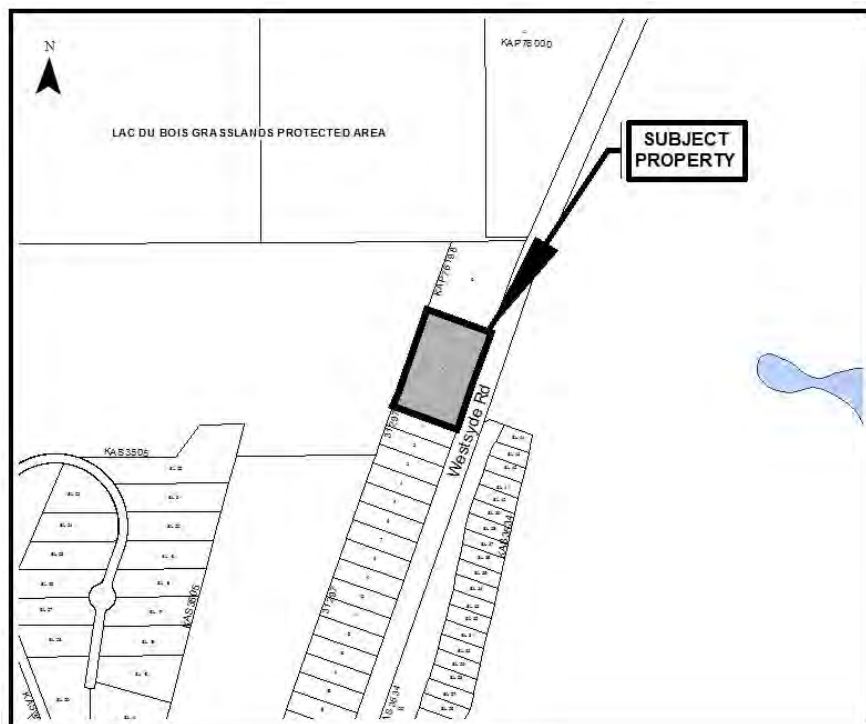
SITE 6
2955 WESTSYDE ROAD

(5-1-2729)



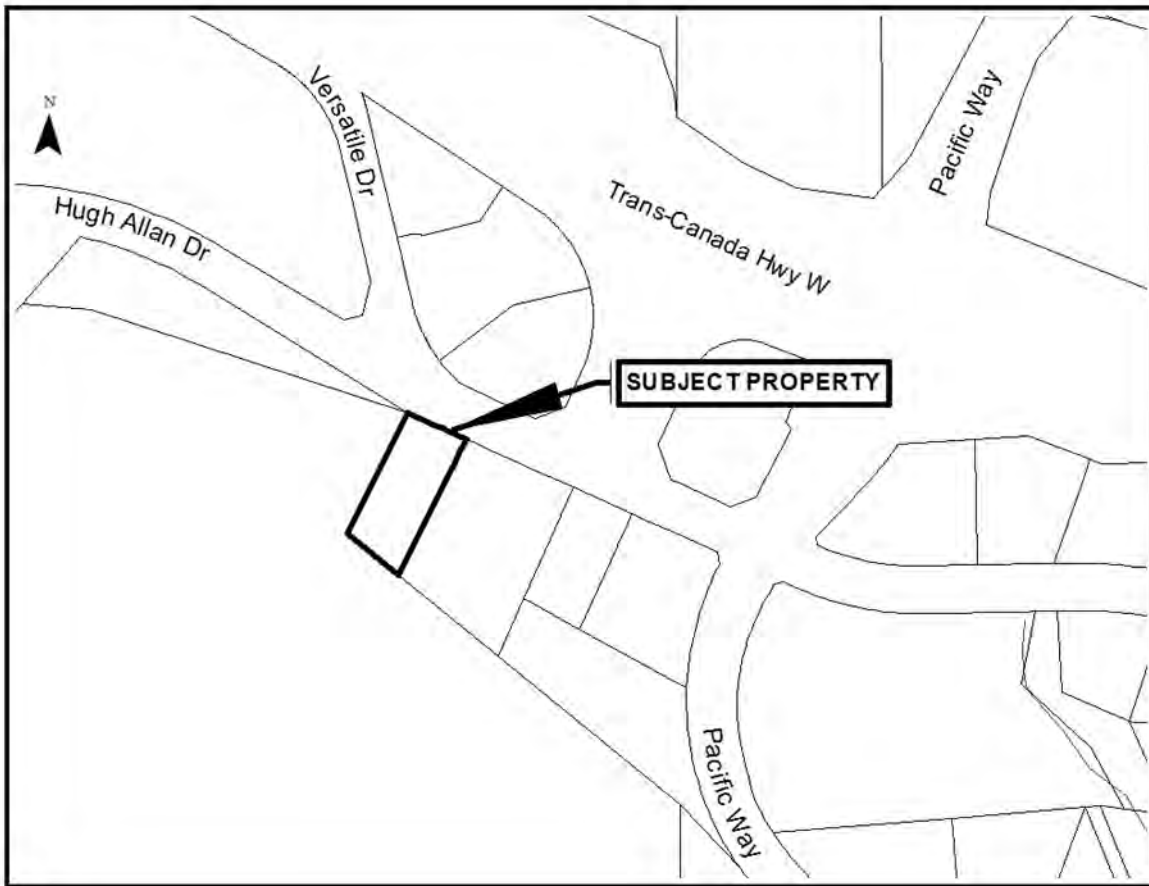
SITE 7
1555 Versatile Drive

(5-1-2731)



SITE 8
3808 WESTSIDE ROAD

(5-1-2794)



SITE 9
1529 HUGH ALLAN DRIVE

(5-1-2563) **1.17 INTENSIVE RESIDENTIAL DESIGN GUIDELINES**

INTENSIVE Residential Design Guidelines (section I)

Garden and Carriage Suite Design Guidelines



Application

- A development permit for form and character shall be required for development defined as intensive residential. For the purpose of this Development Permit Area, Intensive Residential shall include property containing a carriage or garden suite as defined by the Zoning By-law. An intensive residential development permit shall be required prior to the issuance of a building permit authorizing construction of a carriage or garden suite. The entire City of Kamloops is designated a Development Permit Area.

Justification

- Increasing disparities between income and cost of living combined with rising land and construction costs requires the provision of new, low-cost rental housing opportunities. KAMPLAN encourages the development of carriage and garden suites within serviced areas as a mechanism to promote sustainable infill opportunities. However, high quality design is important to improve solar access, privacy, and increase the outdoor enjoyment of neighbours and to ensure the sensitive integration of this housing style within established neighbourhoods.
- The objectives of these guidelines are to:
 - Ensure that carriage and garden suites achieve a high degree of residential livability;
 - Ensure that this type of development considers site specific characteristics;
 - Provide guidance for the sensitive integration of carriage and garden suites into existing neighbourhoods;
 - Promote a style of development that is compatible with existing single family dwellings in terms of setbacks, landscaping, and massing;
 - Encourage developments that reduce the impact on adjacent property owners; and
 - Promote a unique way to accommodate and encourage the construction of quality rental housing.



Definitions

- Carriage Suite - A self-contained, two storey dwelling unit that is separate, subordinate in size, and accessory to the principal dwelling. A carriage suite shall have a footprint no greater than 80 m² and shall not have more than 95 m² of residential living space. Total height, as measured from the lowest finished grade to the peak of the roof, is not to exceed 7 m.
- Garden Suite - A self-contained, one storey dwelling unit that is separate, subordinate in size, and accessory to the principal dwelling. A garden suite shall have a total floor area of not more than 80 m² in area and a building height of not more than 5 m as measured from the lowest finished grade to the peak of the roof.

Exemptions

- A development permit shall not be required for the construction of or alterations to carriage and garden suites approved for construction prior to adoption of these guidelines.



Building Mass and Modulation

- To achieve harmonious integration with its surroundings, development should be sensitive to the scale, mass, and form of adjacent buildings. Single storey garden suites are encouraged as a primary building form. Carriage suites will be considered for flood prone areas and in other areas based on a site specific evaluation of the characteristics of the surrounding neighbourhood.
- Where a carriage suite is considered appropriate, building height should be mitigated by incorporating habitable space into the truss system of the suite. Flat roofed buildings will only be permitted where the principal building has a comparable design.
- Buildings exceeding 5 m in height must increase side yard setback requirements.
- Roofs are encouraged to be varied and/or pitched to reduce the bulk of the building and to emphasis a human scale. A minimum 4/12 pitched roof is considered desirable.
- Large, blank walls are to be limited through the incorporation of dormer or bay style windows. Where windows are not considered desirable, landscaping should be used.



Sample Garden Suite



Sample Carriage Suite



Building Colours and Materials

- Garden and carriage suites should use building materials that reflect the character of the principal dwelling unit and should be built using similar architectural features.
- The use of vinyl should be avoided, but may be considered if complimentary to the principal building.
- A combination of both materials and colours should be used to emphasis prominent features.
- Contrasting colours should be used in the design to highlight window trim, soffits, and gables.



Building materials should be consistent between a garden or carriage suite and the principal dwelling unit.

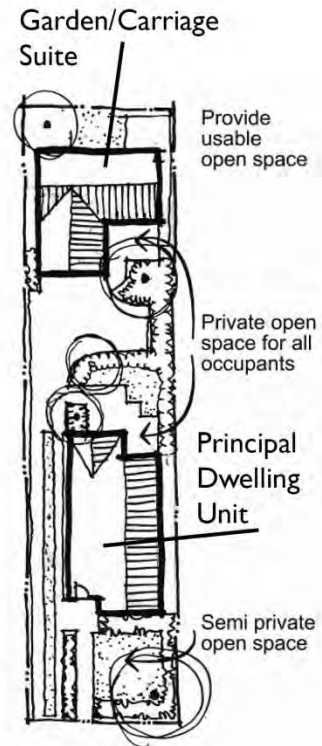
Window Treatments and Orientation

- Large windows will be encouraged to face the lane or shared yard to reduce the impact on adjacent properties.
- Gable-style windows are considered desirable.
- Skylights and similar features should be utilized to maximize interior sunlight.
- Window trim and exterior casings should be used as architectural features.



Landscaping and Fencing

- In conjunction with a development permit application for a garden or carriage suite, submission of a landscape plan may be required.
- Enhancement and greening along an adjacent lane is encouraged for areas not containing parking or driveways. In effect, the lane will become the street on which the garden suite or carriage suite is located.
- Landscaping is encouraged as a means of distinguishing and defining private outdoor space for both the principal dwelling unit and the suite.
- The position of shrubs and other dense vegetation should be strategically located around parking areas to reduce the amount of light pollution caused by night time traffic.
- A garden or carriage suite should be located in such a manner as to retain mature vegetation.
- Chain link fences are discouraged.



Front site plan demonstrating the relationship between the principal dwelling unit and the garden or carriage suite.



Parking and Access

- Where possible, additional parking for a garden or carriage suite should be accessed via a second street frontage or lane to disperse traffic associated with a single property.
- Parking areas should be screened with landscaping and/or fencing to reduce the effects of vehicle lights on adjacent properties.
- One parking stall must be provided for the exclusive use of the suite.
- A second access for a garden or carriage suite on an interior lot not serviced by a lane will only be considered where front yard landscaping is sufficient and subject to Engineering approval.

Outside Amenity

- The provision of an outside amenity area is desirable. This can be provided in the form of a deck, dedicated yard, garden, or similar feature.
- The amenity space should be no smaller than 10 m².
- Natural surveillance of the amenity area shall be promoted through the provision of large windows that are positioned to overlook the amenity area.



Garden and carriage suites should have access to outside recreational space.



Entrances and Balconies

- Entrances to the garden or carriage suite must be easily accessible and, where possible, should be visible from the primary street frontage.
- To enhance the livability and neighbourliness, upper level decks are allowed, but are required to face the lane or the shared yard between the principal dwelling and the garden or carriage suite. Balconies overlooking adjacent properties will not be permitted.

Safety Features

- The City may, at its discretion, permit an increase in the permissible distance between municipal road dedication and the garden or carriage suite entrance as established by the Zoning By-law subject to the installation of a sprinkler system.
- All garden or carriage suites must contain an outside strobe light interconnected with the smoke detector to help guide emergency response.
- Addressing for a garden or carriage suite must be clearly visible from the primary street frontage and must be done in accordance with the City's addressing policy.
- At a minimum, a dedicated, unobstructed, hard surfaced path of 1.5 m in width must be provided linking the garden or carriage suite to the municipal road dedication. This must be a lighted path, but the light should not impact neighbouring properties. Vertical clearances as deemed appropriate by Kamloops Fire Rescue must be maintained along the access path, and the average grade of the path cannot exceed 10% measured from the front property line to the entrance to the accessory building. Stairs may be considered subject to approval by Kamloops Fire Rescue.





INTENSIVE Residential Design Guidelines (Section II)

Small Lot Residential Development Permit Guidelines



Application

A development permit for form and character shall be required for development defined as intensive residential. For the purpose of this Development Permit Area, an Intensive Residential Development Permit shall be required prior to the issuance of a building permit for development of a small lot as defined in the Zoning By-law. Generally, a small lot is considered one of the following:

- A lot with an overall area of less than 370 m²; or
- A single family lot with less than 9 m in lot frontage, which includes panhandle lots; or
- A lot which has an overall area of less than 464 m² and which contains a single-family home with secondary suite

The entire City of Kamloops is defined as a Development Permit Area.

Justification

KAMPLAN: The Official Community Plan promotes the infill and intensification of land use as a mechanism to encourage cost-effective and environmentally sustainable development practices. The purpose of this Development Permit Area is to ensure that infill development occurs in a manner that is sensitive to and reflective of the existing built form. This Development Permit Area encourages new development to consider local characteristics and incorporate high quality design into the siting configuration, landscaping treatments, and overall building aesthetics.

The objectives of these guidelines include:

- Integrating small lot residential development into existing neighbourhoods through the establishment of general design criteria for building form and character;
- Providing a form of sustainable development by utilizing existing service infrastructure;
- Undertaking development that recognizes and emphasizes the unique characteristics of a neighbourhood; and
- Encouraging a more affordable homeownership opportunity.



5-1-2713

5-1-2713

Definition

- Small Lot Residential Development - A single family dwelling on a lot with less than 370 m² in area or less than 9 m in lot frontage, or a single-family dwelling with a secondary suite on a lot with less than 464 m².

Exemptions

- Minor alterations and repair to single family dwellings on legal, non-conforming lots where the footprint of the building is not increased more than 10% of the existing footprint.



Building Height and Massing

- To ensure a proposed single family dwelling does not impact adjacent residences, building height should be limited to a single storey, except where the predominant building form is two stories.
- Shadow impacts on adjacent properties should be reduced by stepping the building inward or by reducing the overall building footprint.
- Building height should be reduced, where possible, through the use of dormers and by accommodating residential living space within the truss system of a proposed dwelling.
- Buildings utilizing variations in the character of rooflines, steep sloping rooflines, gables, hips, and dormers should be encouraged. Upper floors should be proportionally smaller than the lower floors to reduce the appearance of a box.
- Infill housing must be designed to fit into the immediate surroundings and not appear out of scale or character to the adjacent homes. Building mass, scale, and height should compliment and not disrupt the rhythm of adjacent buildings and the overall neighbourhood pattern.



Setbacks

- Minimum and maximum front yard setbacks should be determined based on a review of the setbacks of adjacent properties and should reflect the general character of the neighbourhood.
- As a mechanism to promote the interaction of residential development with the street, small front yards are considered desirable.
- Side yard setbacks should be determined using building code, but should be no smaller than 1.5 m. A variance to 1.2 m may be considered based on a site specific evaluation.
- Rear yard setbacks should be utilized as a mechanism to provide outside recreational space.



Small front yard setbacks are considered desirable.



Rooflines

- Buildings should have strong primary roof forms. Secondary roofs, gables, sheds, or integrated skirt roofs should be incorporated into the building form to express a hierarchy and to visually support the primary roof line.
- Roof pitches should be reflective of the neighbourhood. Flat roofs should be avoided.
- Garage roofing should reflect the general character of the principal building.
- Roofing materials should complement the overall building design and be consistent with adjacent properties.

Windows

- Windows are to be visually prominent and are to be emphasized through the use of colour, exterior casings, and trim or similar architectural feature.
- Windows should be oriented toward the primary street frontage to allow the natural observation of the street.
- Windows are to be of a simple configuration, strategically located and, if possible, positioned uniformly within the building frame.

Lot Coverage

- Parcel coverage should be limited to 30% for the principal dwelling.
- Accessory buildings should have a small overall footprint with a maximum size of 25 m².



Building Materials and Colour

- A combination of two or more building materials is considered desirable. The use of vinyl siding should be avoided.
- Building style should reflect the established neighbourhood character and the dominant building form in the area.
- Colour schemes should be of a natural hue with low value or brightness.
- Complimentary accent colours are encouraged to emphasis architectural features.



Covered entries promote the observation and natural surveillance of the street.

Streetscape

- Buildings should recognize the public domain by including features such as porches, balconies, verandas, and/or covered entryways.
- Fencing and hedging in the front yard should be used to delineate private and public spaces. Hedging and fencing should be kept low. Fencing and landscaping in a front yard should not exceed 1 m in height.
- Landscaping should be used to define public and private spaces. The use of chain-link fencing for this purpose shall be avoided.
- Pedestrian linkages to the street are encouraged.



Landscaping and Fencing

- In conjunction with a development permit application for small lot residential development, submission of a landscape plan may be required.
- A minimum of 40% front yard landscaping in terms of both area and frontage is encouraged. The maximum driveway width should be 6 m.
- Shrubs and trees should be used as a mechanism to green the interface between the street and a dwelling.
- Trees and shrubs should be planted in conjunction with development to increase privacy.

Garages and Parking

- Parking should be accessed from a rear lane where the neighbourhood generally does not have access from the street.
- Where a neighbourhood is characterized by driveways from the street, garages must be recessed behind the front façade of the principal dwelling unit.
- Garages should not be the dominant housing feature visible from the street.
- The massing of garages should be secondary to the primary form of the home. The design and detailing of the garage must be consistent with the architectural style of the home, especially where doors are visible from the street.



1.19 IRON MASK WEST INDUSTRIAL DEVELOPMENT PERMIT AREA

(5-1-2608)

Application: Category (f) Form and character of commercial, industrial, or institutional development.

A Development Permit shall be required for any improvements to a property, including alterations, additions, site design, landscaping, new construction, building permit or subdivision, for all industrial development. Development Permits shall detail parking, access, and vehicular circulation; pedestrian circulation and access; landscaping; retaining walls; building elevations; site layout; and street enhancements.

Area: The Iron Mask West Industrial Development Permit Area is the area outlined on the attached map.

Justification: The Iron Mask West Special Industrial Development Area has been identified as an appropriate location for an industrial park, given its relative proximity to the core of Kamloops and access to the Trans Canada Highway. The area is appropriate for light, medium, and heavy industrial uses utilizing a range of parcel sizes. Given that Iron Mask West is highly visible from the Trans Canada and Coquihalla Highways, care is needed to ensure that a positive impression is communicated to the travelling public. Additionally, the integrity of Bowers Lake should be preserved. These guidelines are intended to encourage a high standard of built form, site design, and landscaping in this highly visible area.

Exemptions: Applications for the following shall not be required to apply for Development Permits:

- Internal renovations;
- External renovations, which do not affect the form and character of the building;
- Small storage sheds that are not visible from a public road; and
- Temporary buildings and structures permitted by a Temporary Commercial or Industrial Use Permit.

Delegation of Authority: In accordance with the Kamloops Development Permit Procedure Bylaw, Council approval is not required for the following Development Permit applications:

- Development Permits pertaining to only site planning or landscaping;
- Amendments to existing Development Permits; and
- Any new improvement to a property or building equalling \$250,000 or less in Building Permit value, where that development is in compliance with all applicable regulations and bylaws pertaining to the subject property.

Design Guidelines

a) Built Form

- Buildings shall be designed so as to create a positive visual impression from the Trans Canada and Coquihalla Highways, as well as Sugarloaf Road.
- Building components housing publicly accessible office and/or customer service areas shall be oriented toward the front of the building and incorporate architectural details such as windows, entry features, distinctive rooflines, and exterior finish materials that distinguish such areas from more utilitarian warehousing, manufacturing, and processing areas. The use of vinyl siding is discouraged in such publically accessible areas, while the use of natural wood, rock, and stone details is encouraged.
- Buildings shall be designed with architectural features and/or variation in treatment to avoid the appearance of long, homogenous façades and to reduce apparent building mass.
- Rooflines shall relate to adjacent building forms and styles. Varied rooflines shall be used to avoid the appearance of a long, flat building façade. Rooftop mechanical equipment must be screened from adjacent properties and streets by rooflines or well designed architectural details.
- Building exteriors shall use subdued rather than bright hues as the dominant colour tones and employ bold, complimentary accents. Secondary façades shall be treated in a colour tone that is the same as or complimentary to that of the principal façade.
- Large expanses of highly reflective treatments and glazing (i.e. mirror glass) shall be avoided on exterior walls so as to prevent heat and glare impacts on adjacent properties and roads.
- Buildings on corner sites or with double road frontage shall be designed to acknowledge the buildings' visibility from each street through continuity of design, colour, materials, exterior finish, and signage.
- The architectural style of buildings containing multiple tenants shall remain cohesive over the entire frontage. Each individual entry point shall be identifiable without detracting from the building's overall appearance.
- Fascia signs shall be integrated into the building's façade, compliment existing signage, and comply with the City of Kamloops Sign Regulations Bylaw.

b) Site Design, Parking, and Circulation

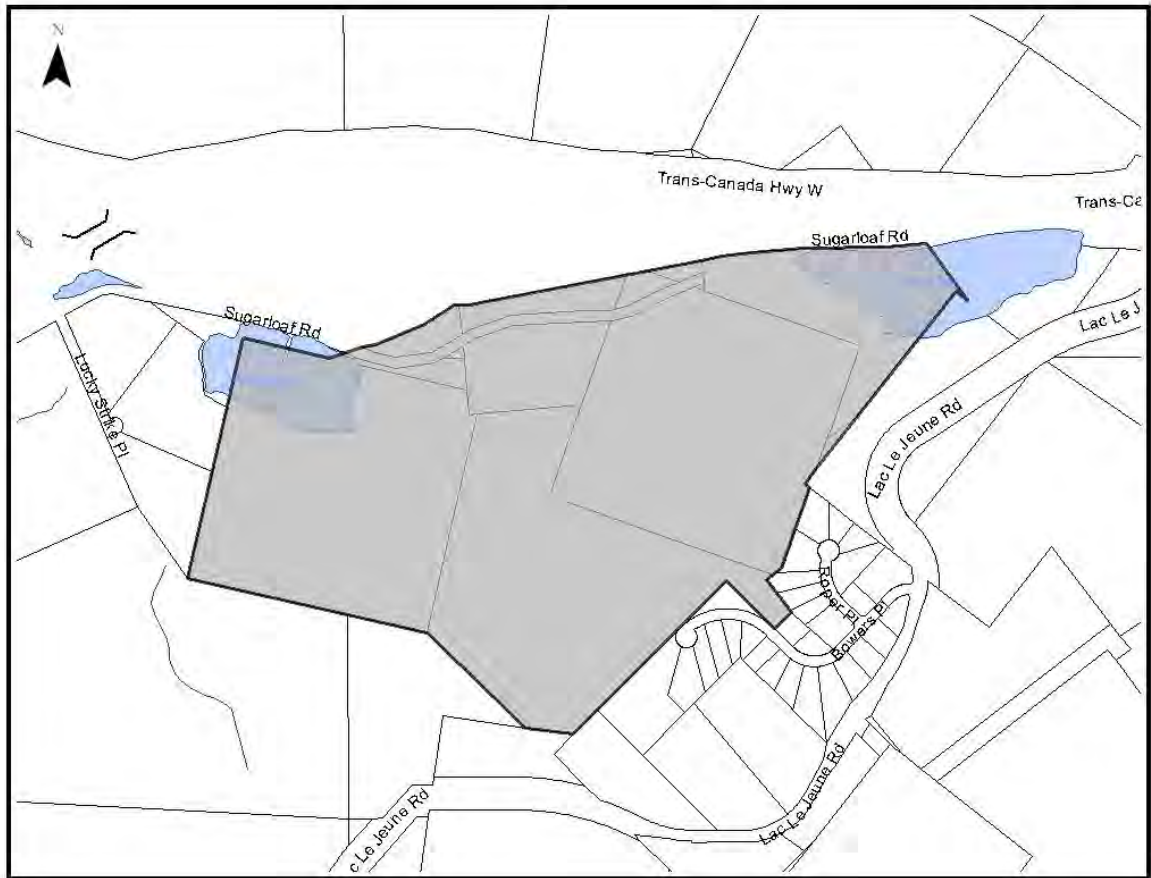
- Landscaping islands shall be used to separate large expanses of parking into smaller subsections appropriate to the use. Landscaping islands shall be located at each end of every parking aisle. In the case of longer parking aisles, landscape islands shall be provided in the middle of the aisle, with a maximum separation of 12 - 15 parking stalls between landscape islands. Landscaping islands shall have a minimum width 2.5 m and include at least one tree per parking aisle.

Schedule 1 - Development Permit Areas Iron Mask West Industrial Development Permit Area

- Raised curbs shall define entry driveways.
- Outdoor storage and processing areas, garbage bins, and loading/unloading areas shall be situated in a side or rear yard in order to minimize exposure of storage areas to public roadways and visually screened by landscaping from adjacent residential areas and streets.
- Site plans must demonstrate the capacity for safe and efficient movement of people and traffic on the site. To achieve this:
 - Site design must include pedestrian walkways along building frontages as well as continuous pedestrian connections between walkways along building frontages, street frontages, and on neighbouring sites;
 - Entrances to buildings shall be located to maximize pedestrian safety;
 - Pedestrian walkways shall be distinguished from driving surfaces by using varied treatments and by raising walkways to curb level; and
 - Bicycle racks shall be located near building entrances.
- Site layout shall incorporate Crime Prevention Through Environmental Design (CPTED) strategies such as encouraging the use of pedestrian thoroughfares to and from adjacent properties and avoiding the creation of dead ends.
- Corner lot site design and landscaping shall consider all street frontages equally and provide equal quality of design, orientation, and detail.
- Access driveways are encouraged to be shared in order to minimize the number of entry/exit points along the road and to provide maximum spacing between them.
- Circulation aisles shall use signs or speed humps to manage operating speeds every 100 m and where circulation aisles intersect.
- Except where the property fronts a highway, all free-standing signage shall be monument-style and pedestrian-oriented, with a maximum sign area of 4 m²; shall relate to the façade of the building; and shall comply with the City of Kamloops Sign Regulations Bylaw (the Sign Regulations Bylaw requires that the number of signs on site shall not exceed the lesser of one for every two businesses or one for each street frontage, provided that the total number of free-standing signs shall not exceed three).
- Detailed traffic analysis shall be required where the proposed development will generate in excess of 100 vehicle trips during peak hours or otherwise significantly impact traffic flows and transportation infrastructure, as determined by the Development and Engineering Services Department.
- Site design and landscape design shall respect and preserve the integrity of Bowers Lake. An environmental assessment will be required prior to site development to determine appropriate setbacks and/or other measures achieve this end.

c) Landscaping

- To create a positive impression from streets and highways, a continuous perimeter of landscaping shall be provided along road frontages and required landscaping shall be concentrated in the front yard. Landscaped areas should be irrigated and include a combination of trees, clustered shrubs and perennials, and ground covers.
- All areas not covered by buildings or structures and not required for parking, loading, storage, assembly, processing, or manufacturing shall be suitably landscaped. The provision of landscaping shall consider the use of xeriscaping and native landscaping materials.
- Outdoor storage, utility installations, and garbage and recycling areas shall be screened by a combination of fencing, shrubs, trees, perennials, and other plantings.
- Landscape design shall prioritize security and pedestrian safety. The use of CPTED principles such as enhancing lighting and minimizing dark areas, encouraging opportunities for natural surveillance, and using visual cues to control access to the site should be considered when designing landscaping.
- The preferred design for lighting within front yards is by ground mounted units located discreetly within landscaped areas. This approach limits the number of light standards in the streetscape zone.
- Lighting and light standards in all public areas, including parking lots, shall relate to the pedestrian, be limited to a maximum 6 m height, and be directed toward the ground so as to avoid light pollution on neighbouring properties. Lighting plans may be required.
- External lighting shall be incorporated into building and landscaping design.
- Landscape plans prepared and sealed by a landscape architect must be included in Development Permit applications; Development Permit approval will be contingent upon submission of security in the amount of 125% the estimated value of landscape work and British Columbia Society of Landscape Architects (BCSLA) Schedules of Assurance (Schedules L-1 and L-2).
- Retaining walls shall be of Gabion, Lock-Block, Redi-rock, or other split-faced styles. The use of non-textured concrete walls is discouraged.
- The use of hydroseeding is discouraged in highly visible areas such as front and side street yards. Where utilized, hydroseeding shall be carried out as per MMCD (Master Municipal Construction Document Association) guidelines, including grade preparation, seeding, and maintenance. Hydroseeded areas shall be well established and free of weeds prior to project completion.
- Installation of raised berms is encouraged where appropriate to enhance the effect of visual screening. Berms required for on-site drainage control shall be landscaped.



IRON MASK WEST INDUSTRIAL DEVELOPMENT PERMIT AREA